



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Meeting Agenda City Council

**THE FORT BRAGG CITY COUNCIL MEETS CONCURRENTLY  
AS THE FORT BRAGG MUNICIPAL IMPROVEMENT DISTRICT  
NO. 1 AND THE FORT BRAGG REDEVELOPMENT SUCCESSOR  
AGENCY**

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Monday, April 25, 2022

6:00 PM

Town Hall, 363 N. Main St. and  
Via Video Conference

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### CALL TO ORDER

### PLEDGE OF ALLEGIANCE

### ROLL CALL

### ZOOM WEBINAR INVITATION

*This meeting is being presented in a hybrid format, both in person at Town Hall and via Zoom.*

*Please click the link below to join the webinar:*

*<https://us06web.zoom.us/j/85348678053>*

*Or Telephone: +1 253 215 8782 or +1 346 248 7799 (\*6 mute/unmute; \*9 raise hand)*

*Webinar ID: 853 4867 8053*

*To speak during public comment portions of the agenda via Zoom, please join the meeting and use the raise hand feature when the Mayor or Acting Mayor calls for public comment on the item you wish to address.*

### AGENDA REVIEW

#### **1. MAYOR'S RECOGNITIONS AND ANNOUNCEMENTS**

- 1A. [22-200](#) Presentation of Proclamation Recognizing the 53rd Annual Professional Municipal Clerks Week, May 1 - May 7, 2022

**Attachments:** [06-Professional Municipal Clerks Week](#)

#### **2. PUBLIC COMMENTS ON: (1) NON-AGENDA, (2) CONSENT CALENDAR & (3) CLOSED SESSION ITEMS**

*MANNER OF ADDRESSING THE CITY COUNCIL: All remarks and questions shall be addressed to the City Council; no discussion or action will be taken pursuant to the Brown Act. No person shall speak without being recognized by the Mayor or Acting Mayor. Public comments are restricted to three (3) minutes per speaker.*

*TIME ALLOTMENT FOR PUBLIC COMMENT ON NON-AGENDA ITEMS: Thirty (30) minutes shall be allotted to receiving public comments. If necessary, the Mayor or Acting Mayor may allot an additional 30 minutes to public comments after Conduct of Business to allow those who have not yet spoken to do so. Any citizen, after being recognized by the Mayor or Acting Mayor, may speak on any topic that may be a proper subject for discussion before the City Council for such period of time as the Mayor or Acting Mayor may determine is appropriate under the*

circumstances of the particular meeting, including number of persons wishing to speak or the complexity of a particular topic. Time limitations shall be set without regard to a speaker's point of view or the content of the speech, as long as the speaker's comments are not disruptive of the meeting.

**BROWN ACT REQUIREMENTS:** The Brown Act does not allow action or discussion on items not on the agenda (subject to narrow exceptions). This will limit the Council's response to questions and requests made during this comment period.

**WRITTEN PUBLIC COMMENTS:** Any written public comments received after agenda publication will be forwarded to the Councilmembers as soon as possible after receipt and will be available for inspection at City Hall, 416 N. Franklin Street, Fort Bragg, California, during normal business hours. All comments will become a permanent part of the agenda packet on the day after the meeting or as soon thereafter as possible, except those written comments that are in an unrecognized file type or too large to be uploaded to the City's agenda software application. Public comments may be submitted to City Clerk June Lemos at [jlemos@fortbragg.com](mailto:jlemos@fortbragg.com).

### **3. STAFF COMMENTS**

### **4. MATTERS FROM COUNCILMEMBERS**

### **5. CONSENT CALENDAR**

All items under the Consent Calendar will be acted upon in one motion unless a Councilmember requests that an individual item be taken up under Conduct of Business.

- 5A. [22-188](#)** Adopt by Title Only and Waive the Second Reading of Ordinance 980-2022 Amending Section 18.42.110 (Mobile/Manufactured Homes and Mobile Home Parks) and Adding Section 18.42.175 (Tiny Homes) to Chapter 18.42 (Standards for Specific Land Uses) of Title 18 (Inland Land Use and Development Code) of the Fort Bragg Municipal Code

**Attachments:** [ORD 980 Tiny Homes](#)

- 5B. [22-189](#)** Adopt City Council Resolution Appointing Director to Represent and Vote on Behalf of the City of Fort Bragg on the California Intergovernmental Risk Authority Board of Directors

**Attachments:** [RESO CIRA Representatives](#)

- 5C. [22-190](#)** Adopt City Council Resolution Appointing Director to Represent and Vote on Behalf of the City of Fort Bragg on the Redwood Empire Municipal Insurance Fund Board of Directors

**Attachments:** [RESO REMIF Representatives](#)

- 5D. [22-192](#)** Adopt City Council Resolution Making the Legally Required Findings to Continue to Authorize the Conduct of Remote "Telephonic" Meetings During the State of Emergency

**Attachments:** [RESO Authorize Continuing Remote Meetings](#)

- 5E. [22-193](#)** Adopt City Council Resolution Confirming the Continued Existence of a Local Emergency in the City of Fort Bragg

**Attachments:** [RESO Declaring Continuing Local Emergency](#)  
[Public Comment 5E, 5G, and 8D](#)

- 5F. [22-194](#) Adopt City Council Resolution Adopting a List of Projects for Fiscal Year 2022-23 Funded by SB 1: The Road Repair and Accountability Act of 2017

Attachments: [RESO 2022-23 SB 1 Priority Project List](#)

- 5G. [22-198](#) Receive and File Written Report Pursuant to Government Code Section 65858(d) on Behalf of the City Council Describing the Measures Taken to Date to Alleviate the Condition Which Led to the Adoption of Urgency Ordinance 972-2021

Attachments: [04252022 Report on Cannabis Moratorium](#)  
[Public Comment 5E, 5G, and 8D](#)

- 5H. [22-186](#) Receive and File Minutes of the Public Works and Facilities Committee Meeting of February 10, 2022

Attachments: [02102022 PWF Meeting Minutes](#)

- 5I. [22-195](#) Approve Minutes of April 11, 2022

Attachments: [CCM2022-04-11](#)

## **6. DISCLOSURE OF EX PARTE COMMUNICATIONS ON AGENDA ITEMS**

## **7. PUBLIC HEARING**

*When a Public Hearing has been underway for a period of 60 minutes, the Council must vote on whether to continue with the hearing or to continue the hearing to another meeting.*

## **8. CONDUCT OF BUSINESS**

- 8A. [22-124](#) Receive and File Local Road Safety Plan Report from TJKM and Provide Recommendations for Select Highway Safety Improvement Program Grant Application

Attachments: [04252022 LRSP Council Presentation](#)  
[Fort Bragg Draft LRSP Report](#)  
[Appendix B](#)  
[Appendix C](#)  
[Appendix D](#)  
[Appendix E](#)

- 8B. [22-173](#) Receive Report and Consider Adoption of City Council Resolution Approving Professional Services Agreement with De Novo Planning Group to Prepare an Environmental Impact Report for a Proposed Grocery Outlet at 825, 845, and 851 S. Franklin St. and Authorizing City Manager to Execute Contract (Amount Not to Exceed \$56,013; Account No. 119-0000-2668)



- *Materials related to an item on this Agenda submitted to the Council/District/Agency after distribution of the agenda packet are available for public inspection upon making reasonable arrangements with the City Clerk for viewing same during normal business hours.*
- *Such documents are also available on the City of Fort Bragg's website at <https://city.fortbragg.com> subject to staff's ability to post the documents before the meeting.*

**ADA NOTICE AND HEARING IMPAIRED PROVISIONS:**

*It is the policy of the City of Fort Bragg to offer its public programs, services and meetings in a manner that is readily accessible to everyone, including those with disabilities. Upon request, this agenda will be made available in appropriate alternative formats to persons with disabilities.*

*If you need assistance to ensure your full participation, please contact the City Clerk at (707) 961-2823. Notification 48 hours in advance of any need for assistance will enable the City to make reasonable arrangements to ensure accessibility.*

*This notice is in compliance with the Americans with Disabilities Act (28 CFR, 35.102-35.104 ADA Title II).*



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-200

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Mayor's Office

**In Control:** City Council

**File Type:** Proclamation

**Agenda Number:** 1A.

Presentation of Proclamation Recognizing the 53rd Annual Professional Municipal Clerks Week,  
May 1 - May 7, 2022

***PROCLAMATION***  
**53rd ANNUAL PROFESSIONAL MUNICIPAL CLERKS WEEK**  
**May 1 - May 7, 2022**

**Whereas**, the Office of the Professional Municipal Clerk, a time honored and vital part of local government, exists throughout the world, and

**Whereas**, the Office of the Professional Municipal Clerk is the oldest among public servants, and

**Whereas**, the Office of the Professional Municipal Clerk provides the professional link between the citizens, the local governing bodies, and agencies of government at other levels, and

**Whereas**, Professional Municipal Clerks have pledged to be ever mindful of their neutrality and impartiality, rendering equal service to all, and

**Whereas**, the Professional Municipal Clerk serves as the information center on functions of local government and community, and

**Whereas**, Professional Municipal Clerks continually strive to improve the administration of the affairs of the Office of the Professional Municipal Clerk through participation in education programs, seminars, workshops and the annual meetings of their state, province, county and international professional organizations, and

**Whereas**, it is most appropriate that we recognize the accomplishments of the Office of the Professional Municipal Clerk.

**Now, Therefore, I, Bernie Norvell**, Mayor of the City of Fort Bragg, on behalf of the entire City Council, do hereby recognize the week of May 1 through May 7, 2022, as **Professional Municipal Clerks Week**, and further extend appreciation to our own Professional Municipal Clerk, **June Lemos**, and to all Professional Municipal Clerks for the vital services they perform and their exemplary dedication to the communities they represent.

Dated this 25th day of April, 2022

\_\_\_\_\_  
BERNIE NORVELL, Mayor

Attest:

\_\_\_\_\_  
Cristal Muñoz  
Acting City Clerk

No. 06-2022



# City of Fort Bragg

416 N Franklin Street  
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Phone: (707) 961-2823  
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## Text File

File Number: 22-188

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Ordinance

**Agenda Number:** 5A.

Adopt by Title Only and Waive the Second Reading of Ordinance 980-2022 Amending Section 18.42.110 (Mobile/Manufactured Homes and Mobile Home Parks) and Adding Section 18.42.175 (Tiny Homes) to Chapter 18.42 (Standards for Specific Land Uses) of Title 18 (Inland Land Use and Development Code) of the Fort Bragg Municipal Code

On April 25, 2022, the City Council of the City of Fort Bragg is to consider adoption of Ordinance 980-2022, said ordinance having been introduced for first reading (by title only and waiving further reading of the text) on April 11, 2022.

The proposed ordinance will amend Section 18.42.110 (Mobile/Manufactured Homes and Mobile Home Parks) and add Section 18.42.175 (Tiny Homes) to Chapter 18.42 (Standards for Specific Land Uses) of Title 18 (Inland Land Use and Development Code) of the Fort Bragg Municipal Code. This ordinance will regulate alternative housing models and contribute to addressing housing supply shortages by allowing movable tiny homes within the City of Fort Bragg.

If adopted, the ordinance will become effective on May 25, 2022.

**BEFORE THE CITY COUNCIL OF THE CITY OF FORT BRAGG**

**AN ORDINANCE AMENDING SECTION 18.42.110 (MOBILE/MANUFACTURED HOMES AND MOBILE HOME PARKS) AND ADDING SECTION 18.42.175 (TINY HOMES) TO CHAPTER 18.42 (STANDARDS FOR SPECIFIC LAND USES) OF TITLE 18 (INLAND LAND USE AND DEVELOPMENT CODE) OF THE FORT BRAGG MUNICIPAL CODE**

**ORDINANCE NO. 980-2022**

**WHEREAS**, the City of Fort Bragg (“City”) adopted an Inland General Plan and certified an Environmental Impact Report Addendum for the General Plan on December 2, 2012; and

**WHEREAS**, the adoption of an Inland Land Use and Development Code (ILUDC) is necessary to: 1) provide a regulatory framework for implementation of the Inland General Plan; 2) to implement new state planning and land use requirements; and 3) update zoning regulations in accordance with the City Council policy direction; and

**WHEREAS**, the City updated the Inland General Plan, Housing Element in 2019, the Housing Element encourages a variety of housing types for all income levels; and

**WHEREAS**, the City of Fort Bragg currently regulates alternative housing models that contribute to addressing housing supply shortages and affordability, such as accessory dwelling units (ADUs); and

**WHEREAS**, alternative housing models, such as movable tiny homes, can provide flexible housing options for a variety of households living at different income levels; and

**WHEREAS**, State law allows local agencies to adopt less restrictive requirements for the development of ADUs; and

**WHEREAS**, the City received grant funding through Senate Bill 2 to create a tiny home ordinance; and

**WHEREAS**, this Ordinance adds tiny houses as a separately regulated residential use and in mobile home parks; and

**WHEREAS**, the Planning Commission held a properly noticed public hearing on March 23, 2022, during which all interested persons were heard, and adopted Resolution PC04-2022 recommending City Council adopt the amendments to Inland Land Use and Development Code regarding regulations pertaining to tiny homes; and

**WHEREAS**, the City Council received Planning Commission’s recommendation and considered aforementioned amendments at a properly noticed public hearing on April 11, 2022; and

**WHEREAS**, the City Council did hear and consider all said reports, recommendations and testimony herein above set forth and used independent judgment to evaluate the project.

**NOW, THEREFORE, the City Council ordains as follows:**

**Section 1. Legislative Findings.** The City Council hereby finds as follows:

1. The foregoing recitals are true and correct and are made a part of this ordinance.
2. The proposed amendment is consistent with the General Plan and any applicable specific plan, because the proposed amendments are consistent with applicable land use designations and comply with State law. Furthermore, the City's Housing Element promotes a variety of housing types accessible to all income levels, including accessory dwelling units and multifamily developments, as illustrated in the following policies and programs:

Policy H-1.3 Secondary Dwelling Units. Continue to facilitate the construction of secondary dwelling units on residential properties.

Program H-1.3.2 No Development Impact Fees for Secondary Units. Continue to refrain from charging Capacity Fees for second units.

Program H-1.3.5 Allow Tiny Homes as Second Units: Consider revising the zoning ordinance so that people can park mobile residences (residences built under the vehicle code) as a second unit, so long as the residence looks like a house (e.g. external siding that is compatible with the residential neighborhood, skirted if the wheels would otherwise be visible from the public right of way, etc.).

Program H-1.3.6 Alternative Designs for Second Units: Explore options for allowing cutting edge construction techniques for second units including but not limited to: straw bale, rammed earth, prefabricated second units, etc.

Program H-1.7.10: Tiny Home Community. Consider adopting new zoning regulations to allow for small home subdivisions, with small individual parcel ownership, in all residential zoning districts. Consider changing the minimum lot size and minimum parcel dimensions of the ILUDC to accommodate tiny home communities as part of a planned unit development.

Policy H-1.7 Workforce Housing. Encourage multi-unit housing developments in order to encourage market rate rental housing, affordable housing and lower cost ownership opportunities such as townhomes and condominiums.

Program H-2.4.5 Prioritize City Services for Housing Developments. Continue to implement procedures to grant priority service for sewer and water services to residential developments.

Program H-5.2.1 Discourage Vacation Rentals: Continue to prohibit vacation rentals in all zoning districts except for the CBD. Undertake proactive undercover code enforcement activity on a regular basis against all illegal vacation rentals in Fort Bragg. Work with the County of Mendocino at all levels to reduce or eliminate further conversions of residential units into vacation rentals as this practice has greatly increased the magnitude of the housing crisis on the Mendocino coast and in the City of Fort Bragg.

3. The proposed amendment would not be detrimental to the public interest, health, safety, convenience, or welfare of the City because movable tiny homes can be an invaluable tool for providing much-needed affordable and/or available housing stock in our community. All existing and proposed residential units are constructed in compliance with City development standards.
4. The proposed ordinance is exempt from review under the California Environmental Quality Act under California Code of Regulations, Title 14, Section 15301 of the CEQA Guidelines and is also exempt from review because it does not meet the definition of a project under CEQA Guidelines section 15061, subdivision (b)(3) and section 15378, subdivision (a) and subdivision (b)(5). The proposed changes of allowing and adopting standards for moveable tiny houses as a new type of accessory dwelling unit as authorized by state law, has no potential for resulting in physical changes in the environment because it consists of changes in the standards governing issuance of ministerial permits for accessory dwelling units and does not directly or indirectly approve any applications for particular accessory dwelling units. As well, the proposed text amendments would not change the overall number of dwelling units allowed on any parcel.

**Section 2.** Based on the foregoing, the City Council hereby amends Table 2-1 of Article 2 (Zoning Districts and Allowable Land Uses) of Title 18 (Inland Land Use and Development Code) of the City of Fort Bragg Municipal Code as follows:

<b>TABLE 2-1 Allowed Land Uses and Permit Requirements for Residential Zoning Districts</b>	<b>P Permitted use, Zoning Clearance required</b>						
	<b>MUP Minor Use Permit required (see § 18.71.060)</b>						
	<b>UP Use Permit required (see § 18.71.060)</b>						
	<b>S Permit requirement set by Specific Use Regulations</b>						
	<b>— Use not allowed</b>						
<b>LAND USE (1)</b>	<b>PERMIT REQUIRED BY DISTRICT</b>						<b>Specific Use Regulations</b>
	<b>RR</b>	<b>RS</b>	<b>RL</b>	<b>RM</b>	<b>RH</b>	<b>RVH</b>	

RESIDENTIAL USES

Condominium conversion - 3 units maximum per parcel	–	–	–	P	UP	UP	
Home occupation	P	P	P	P	P	P	18.42.080
Mobile home park	UP	UP	UP	UP	UP	UP	18.42.110
Manufactured home	P	P	P	P	P	P	18.42.110
Multifamily housing, 3 units	–	–	–	P	P	P	18.42.120
Multifamily housing, 4 or more units	–	–	–	UP	UP	P	18.42.120
Co-housing, 4 or more units	–	–	–	UP	UP	P	18.42.120
Organizational housing/care facility (sorority, monastery, residential care, etc.) of more than 3,000 SF or 3 units	–	–	–	UP	UP	UP	
Residential accessory use or structure	P	P	P	P	P	P	18.42.160
Residential care facility for the elderly (RCFE)	–	–	–	UP	UP	UP	
Second unit – ADU/JADU	P	P	P	P	P	P	18.42.170
Tiny Homes	P	P	P	P	P	P	18.42.175
Single residential unit	P	P	P	P	P	P	

**Section 3.** Section 18.42.175 (Tiny Homes) is hereby added to Chapter 18.42 (Standards for Specific Land Uses), of Article 4 (Standards for Specific Land Uses) of Title 18 (Inland Land Use and Development Code) of the City of Fort Bragg Municipal Code and shall read as follows:

**18.42.175 –Tiny Homes**

- A. Applicability.** Where allowed by Article 2 (Zoning Districts and Allowable Land Uses), Tiny Homes shall comply with the standards of this section.
- B. Definitions.** A tiny home is a small towable residential unit that is not on a permanent foundation, and that meets the design and construction criteria listed in C below.
- C. Standards.** Tiny homes shall be allowed as a type of accessory dwelling unit subject to all of the following criteria:

**1. Limitation on location.**

- a. Tiny homes are allowed on any residentially zoned parcel (RR, RS, RL, RM, RH, and/or RVH).

**2. Development Standards.** A tiny home shall conform with the following requirements:

- a. **Height.** A tiny home shall have a maximum height of 13' 6" to comply with Department of Motor Vehicles (DMV) towing requirements.
- b. **Location.** A tiny home shall comply with standard front setbacks for the zoning district, tiny homes shall be located toward the rear of the property, and maintain 4' side and rear setbacks.
- c. **Size.** The minimum square footage of a tiny home shall be 150 square feet to comply with California Health & Safety Code. The maximum size shall be 400 square feet.
- d. **Number of Units Allowed.** Tiny homes are allowed on a parcel in the following configurations:
  - i. On a parcel with an existing primary unit, a maximum of two tiny homes are permitted. Tiny homes shall be considered a type of accessory dwelling unit for the purposes of density calculations.
  - ii. Tiny homes are permitted in mobile home parks, and the maximum allowed shall be determined in the use permit process.
- e. **Parking.** No additional parking is required for a tiny home.

**3. Design Standards.** A tiny home shall maintain a residential appearance through the following design standards.

- a. **Skirting.** The undercarriage (wheels, axles, tongue and hitch) shall be hidden from view with a solid wood, metal or concrete apron when parked.
- b. **Roof Pitch.** Roofs shall have a minimum of a 1:12 for greater than 50% of the roof area.
- c. **Foundation or Pad.** A paved parking pad shall be required and include bumper guards, curbs, or other installations adequate to prevent movement of the unit. Alternative paving methods may be permitted at the discretion of the Community Development Director.
- d. **Mechanical Equipment.** Mechanical equipment shall be incorporated into the structure and not be located on the roof (except for solar panels). Generators are prohibited except in emergencies.
- e. **Materials.** Materials for the exterior wall covering shall include wood, HardiePanel or equivalent material as determined by the Community Development Director. Single piece composite laminates, or interlocked metal sheathing is prohibited.
- f. **Windows.** Windows shall be double pane glass or better, labeled for building use, and be trimmed out.
- g. **Utility Connections.** A tiny home shall be connected to City water and sewer utilities through dedicated pipes. A tiny home may use on- or off-grid electricity. All tiny homes shall have a GFI shutoff breaker.

4. **Short Term Rentals.** Tiny homes shall not be used as short-term rentals as defined by section 18.42.190 – Vacation Rental Units.
5. **Applicable Codes.**
  - a. Tiny homes shall meet either the provisions of ANSI 119.5 or NFPA 1192. It shall be the burden of the applicant to show compliance with these standards.
  - b. Tiny homes shall be licensed and registered with the California Department of Motor Vehicles.
6. **Fire Inspection.** Tiny homes shall require a yearly inspection by the Fire Marshall.

**Section 4.** Section 18.42.110 (Mobile/Manufactured Homes and Mobile Home Parks) of Chapter 18.42 (Standards for Specific Land Uses) of Article 4 (Standards for Specific Land Uses), of Title 18 (Inland Land Use and Development Code) of the Fort Bragg Municipal Code is hereby amended to provide as follows:

**18.42.110 - Mobile/Manufactured Homes and Mobile Home Parks**

This Section provides requirements and development standards for the use of mobile homes and manufactured homes as single-family dwellings outside of mobile home parks, and for mobile home parks, where allowed by Article 2 (Zoning Districts and Allowable Land Uses).

**A. Mobile home outside of a mobile home park.**

1. **Site requirements.** The site, and the placement of the mobile home on the site, shall comply with all zoning, subdivision, and development standards applicable to a conventional single-family dwelling on the same parcel.
2. **Mobile home design and construction standards.** A mobile home outside of a mobile home park shall comply with the following design and construction standards:
  - a. The exterior siding, trim, and roof shall be of the same materials and treatment found in conventionally built residential structures in the surrounding area, and shall appear the same as the exterior materials on any garage or other accessory structure on the same site.
  - b. The roof shall have eave and gable overhangs of not less than 12 inches measured from the vertical side of the mobile home, and the roof pitch shall be no less than 3:12.
  - c. Tiny homes shall have a minimum roof pitch of 1:12.
  - d. The mobile home shall be placed on a foundation system or concrete pad, subject to the approval of the Building Official.
  - e. The mobile home shall be certified under the National Mobile Home Construction and Safety Standards Act of 1974 (42 USC Section 4401 et

seq.), American National Standards Institute 119.5, or National Fire Protection Agency 1192 and constructed after January 1, 1989.

**Section 5. Severability.** If any section, subsection, sentence, clause or phrase of this Ordinance is for any reason held by a court of competent jurisdiction to be invalid or unconstitutional, such decision shall not affect the validity of the remaining portions of the Ordinance. The City Council of the City of Fort Bragg hereby declares that it would have passed this Ordinance and each section, subsection, sentence, clause and phrase thereof irrespective of the fact that one or more sections, subsections, sentences, clauses or phrases may be held invalid or unconstitutional.

**Section 6. Effective Date and Publication.** This ordinance shall be and the same is hereby declared to be in full force and effect from and after thirty (30) days after the date of its passage. Within fifteen (15) days after the passage of this Ordinance, the City Clerk shall cause a summary of said Ordinance to be published as provided in Government Code §36933, in a newspaper of general circulation published and circulated in the City of Fort Bragg, along with the names of the City Council voting for and against its passage.

**The foregoing Ordinance was introduced by Councilmember Peters at a regular meeting of the City Council of the City of Fort Bragg held on April 11, 2022 and adopted at a regular meeting of the City of Fort Bragg held on April 25, 2022 by the following vote:**

**AYES:  
NOES:  
ABSENT:  
ABSTAIN:  
RECUSED:**

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**BERNIE NORVELL**  
Mayor

**ATTEST:**

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**June Lemos, MMC**  
City Clerk

**PUBLISH:** April 14, 2022 and May 5, 2022 (by summary).  
**EFFECTIVE DATE:** May 25, 2022.



# City of Fort Bragg

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## Text File

File Number: 22-189

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Resolution

**Agenda Number:** 5B.

Adopt City Council Resolution Appointing Director to Represent and Vote on Behalf of the City of Fort Bragg on the California Intergovernmental Risk Authority Board of Directors

**RESOLUTION NO. \_\_\_\_-2022**

**RESOLUTION OF THE FORT BRAGG CITY COUNCIL APPOINTING  
DIRECTOR TO REPRESENT AND VOTE ON BEHALF OF THE CITY OF  
FORT BRAGG ON THE CALIFORNIA INTERGOVERNMENTAL RISK  
AUTHORITY BOARD OF DIRECTORS**

**WHEREAS**, the City of Fort Bragg has been a member of the Redwood Empire Municipal Insurance Fund (REMIF) since December 31, 1978; and

**WHEREAS**, REMIF is a risk sharing pool of small to medium sized cities and towns (referred to as Members) which contribute to a shared fund that pays for liability and workers' compensation claims and provides risk management services to its Members;

**WHEREAS**, pools are empowered by the California Government Code to exist as joint powers authorities (JPA); and

**WHEREAS**, Resolution No. 4334-2020 approved an amendment to the Joint Powers Agreement and the bylaws of REMIF and approved the joint powers agreement and bylaws for Public Agency Risk Sharing Authority of California (PARSAC), also a public agency representing small to medium cities/towns and one fire district; and

**WHEREAS**, the REMIF and PARSAC Board of Directors voted to recommend that their respective Member agencies merge the two organizations by creating a new risk pool named the California Intergovernmental Risk Authority (CIRA or "The Authority"); and

**WHEREAS**, the self-insured health plan provided by REMIF will remain under the control of REMIF; and

**WHEREAS**, Article V of the CIRA bylaws states that the Board shall be comprised of one Director from each Member and an Alternate Director and the Alternate Director may only cast a vote in the absence of the Director; and

**WHEREAS**, Article V of the CIRA bylaws states that a Member may change any of its representatives to the Board only by written notification to CIRA from the Member's governing body; and

**WHEREAS**, by adoption of Resolution No. 4486-2021 on December 13, 2021, the City Council appointed the Assistant to the City Manager as the designated CIRA Board Member and the Human Resources Analyst as the designated Alternate Director; and

**WHEREAS**, due to an organizational change, the Human Resources Department now reports to the City Clerk, and the Assistant to the City Manager will no longer be responsible for HR or risk management; and

**WHEREAS**, it is necessary for the City Council to adopt a new resolution replacing the Assistant to the City Manager with the City Clerk as the City of Fort Bragg's designated CIRA Board Member;

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Fort Bragg does hereby designate the City Clerk as the CIRA Director for the City of Fort Bragg replacing the Assistant to the City Manager.

**The above and foregoing Resolution was introduced by Councilmember \_\_\_\_\_, seconded by Councilmember \_\_\_\_\_, and passed and adopted at a regular meeting of the City Council of the City of Fort Bragg held on the 25<sup>th</sup> day of April, 2022, by the following vote:**

- AYES:**
- NOES:**
- ABSENT:**
- ABSTAIN:**
- RECUSED:**

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**BERNIE NORVELL**  
**Mayor**

**ATTEST:**

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**June Lemos, MMC**  
**City Clerk**



# City of Fort Bragg

416 N Franklin Street  
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## Text File

File Number: 22-190

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Resolution

**Agenda Number:** 5C.

Adopt City Council Resolution Appointing Director to Represent and Vote on Behalf of the City of Fort Bragg on the Redwood Empire Municipal Insurance Fund Board of Directors

**RESOLUTION NO. \_\_\_\_-2022**

**RESOLUTION OF THE FORT BRAGG CITY COUNCIL APPOINTING  
DIRECTOR TO REPRESENT AND VOTE ON BEHALF OF THE CITY OF  
FORT BRAGG ON THE REDWOOD EMPIRE MUNICIPAL INSURANCE FUND  
BOARD OF DIRECTORS**

**WHEREAS**, the City of Fort Bragg has been a member of the Redwood Empire Municipal Insurance Fund (REMIF) since December 31, 1978; and

**WHEREAS**, REMIF is a risk sharing pool of small to medium sized cities and towns (referred to as Members) which contribute to a shared fund that pays for liability and workers' compensation claims and provides risk management services to its Members; and

**WHEREAS**, pools are empowered by the California Government Code to exist as joint powers authorities (JPA); and

**WHEREAS**, Resolution No. 4334-2020 approved an amendment to the Joint Powers Agreement and the bylaws of REMIF; and

**WHEREAS**, the REMIF and PARSAC Board of Directors voted to recommend that their respective Member agencies merge the two organizations by creating a new risk pool named the California Intergovernmental Risk Authority (CIRA or "The Authority"); and

**WHEREAS**, the self-insured health plan provided by REMIF remains under the control of REMIF; and

**WHEREAS**, REMIF continues to manage claims made prior to the existence of CIRA on July 1, 2021 for liability, property and workers compensation; and

**WHEREAS**, Article III of the REMIF bylaws states that the Board shall be comprised of one Director from each Member and an Alternate Director and the Alternate Director may only cast a vote in the absence of the Director; and

**WHEREAS**, by adoption of Resolution No. 4487-2021 on December 13, 2021, the City Council appointed the Assistant to the City Manager as the designated REMIF Board Member and the Human Resources Analyst as the designated Alternate Director; and

**WHEREAS**, due to an organizational change, the Human Resources Department now reports to the City Clerk, and the Assistant to the City Manager will no longer be responsible for HR or risk management; and

**WHEREAS**, it is necessary for the City Council to adopt a new resolution replacing the Assistant to the City Manager with the City Clerk as the City of Fort Bragg's designated REMIF Board Member;

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Fort Bragg does hereby designate the City Clerk as the REMIF Director for the City of Fort Bragg replacing the Assistant to the City Manager.

**The above and foregoing Resolution was introduced by Councilmember \_\_\_\_\_, seconded by Councilmember \_\_\_\_\_, and passed and adopted at a regular meeting of**

the City Council of the City of Fort Bragg held on the 25<sup>th</sup> day of April, 2022, by the following vote:

- AYES:**
- NOES:**
- ABSENT:**
- ABSTAIN:**
- RECUSED:**

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**BERNIE NORVELL**  
Mayor

**ATTEST:**

---

**June Lemos, MMC**  
City Clerk



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-192

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Resolution

**Agenda Number:** 5D.

Adopt City Council Resolution Making the Legally Required Findings to Continue to Authorize the Conduct of Remote "Telephonic" Meetings During the State of Emergency

**RESOLUTION NO. \_\_\_\_-2022**

**RESOLUTION OF THE FORT BRAGG CITY COUNCIL  
MAKING THE LEGALLY REQUIRED FINDINGS TO CONTINUE TO  
AUTHORIZE THE CONDUCT OF REMOTE “TELEPHONIC”  
MEETINGS DURING THE STATE OF EMERGENCY**

**WHEREAS**, on March 4, 2020, pursuant to California Gov. Code Section 8625, the Governor declared a state of emergency; and

**WHEREAS**, on September 17, 2021, Governor Newsom signed AB 361, which bill went into immediate effect as urgency legislation; and

**WHEREAS**, AB 361 added subsection (e) to Gov. Code Section 54953 to authorize legislative bodies to conduct remote meetings provided the legislative body makes specified findings; and

**WHEREAS**, as of April 18, 2022, the COVID-19 pandemic has killed more than 89,752 Californians; and

**WHEREAS**, social distancing measures decrease the chance of spread of COVID-19; and

**WHEREAS**, this legislative body previously adopted a resolution to authorize this legislative body to conduct remote “telephonic” meetings; and

**WHEREAS**, Government Code 54953(e)(3) authorizes this legislative body to continue to conduct remote “telephonic” meetings provided that it has timely made the findings specified therein;

**NOW, THEREFORE, IT IS RESOLVED** by the City Council of the City of Fort Bragg as follows:

1. This legislative body declares that it has reconsidered the circumstances of the state of emergency declared by the Governor and at least one of the following is true: (a) the state of emergency continues to directly impact the ability of the members of this legislative body to meet safely in person; and/or (2) state or local officials continue to impose or recommend measures to promote social distancing.

**The above and foregoing Resolution was introduced by Councilmember \_\_\_\_\_, seconded by Councilmember \_\_\_\_\_, and passed and adopted at a regular meeting of the City Council of the City of Fort Bragg held on the 25<sup>th</sup> day of April 2022, by the following vote:**

**AYES:  
NOES:  
ABSENT:  
ABSTAIN:  
RECUSED:**

---

**BERNIE NORVELL**  
Mayor

**ATTEST:**

---

**June Lemos, MMC**  
City Clerk



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-193

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Resolution

**Agenda Number:** 5E.

Adopt City Council Resolution Confirming the Continued Existence of a Local Emergency in the City of Fort Bragg

At a special meeting on March 24, 2020, the Fort Bragg City Council ratified the City Manager's Proclamation declaring a local emergency due to COVID-19 in its Resolution No. 4242-2020. Since that date, the Council has adopted 47 resolutions reconfirming the existence of a local emergency. The City is required to reconfirm the existence of a local emergency every 21 days pursuant to Fort Bragg Municipal Code Section 2.24.040.

**RESOLUTION NO. \_\_\_\_-2022**

**RESOLUTION OF THE FORT BRAGG CITY COUNCIL  
CONFIRMING THE CONTINUED EXISTENCE OF A LOCAL  
EMERGENCY IN THE CITY OF FORT BRAGG**

**WHEREAS**, California Government Code section 8630 empowers the Fort Bragg City Council to proclaim the existence of a local emergency when the City is threatened or likely to be threatened by the conditions of extreme peril to the safety of persons and property that are or are likely to be beyond the control of the services, personnel, equipment, and facilities of this City; and

**WHEREAS**, California Government Code section 8558(c) states that a “local emergency” means the duly proclaimed existence of conditions of disaster or extreme peril to the safety of persons and property within the territorial limits of a city; and

**WHEREAS**, COVID-19, a novel coronavirus causing infectious disease, was first detected in China in December 2019 and has spread across the world and to the United States. Symptoms of COVID-19 include fever, cough, and shortness of breath; outcomes have ranged from mild to severe illness, and, in some cases, death. The Center for Disease Control and Prevention (CDC) has indicated the virus is a tremendous public health threat; and

**WHEREAS**, on March 13, 2020, the President of the United States issued a proclamation declaring the COVID-19 outbreak in the United States as a national emergency, beginning March 1, 2020; and

**WHEREAS**, the Governor of the State of California and the Public Health Officer of the County of Mendocino have both issued Shelter-in-Place orders to combat the spread of COVID-19; and

**WHEREAS**, on March 17, 2020 the City Manager, as the City’s Director of Emergency Services, issued Proclamation No. CM-2020-01 declaring a local emergency as authorized by Government Code section 8630 and Fort Bragg Municipal Code section 2.24.040(B); and

**WHEREAS**, at a special meeting on March 24, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4242-2020, ratifying the City Manager’s Proclamation declaring the existence of a local emergency; and

**WHEREAS**, at a special meeting on April 6, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4245-2020 by which it continued the local emergency; and

**WHEREAS**, at a special meeting on April 20, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4247-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on May 11, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4250-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on May 26, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4253-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on June 8, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4266-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on June 22, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4270-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on July 13, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4284-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on July 27, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4289-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on August 10, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4294-2020 by which it continued the local emergency; and

**WHEREAS**, at a special meeting on August 31, 2020, the City Council of the City of Fort Bragg adopted Resolution No. 4300-2020 by which it continued the local emergency; and

**WHEREAS**, at a special meeting on September 21, 2020, the City Council of the City of Fort Bragg adopted Resolution 4304-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on October 13, 2020, the City Council of the City of Fort Bragg adopted Resolution 4317-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on October 26, 2020, the City Council of the City of Fort Bragg adopted Resolution 4319-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on November 9, 2020, the City Council of the City of Fort Bragg adopted Resolution 4323-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on November 23, 2020, the City Council of the City of Fort Bragg adopted Resolution 4329-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on December 14, 2020, the City Council of the City of Fort Bragg adopted Resolution 4333-2020 by which it continued the local emergency; and

**WHEREAS**, at a special meeting on December 22, 2020, the City Council of the City of Fort Bragg adopted Resolution 4340-2020 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on January 11, 2021, the City Council of the City of Fort Bragg adopted Resolution 4343-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on January 25, 2021, the City Council of the City of Fort Bragg adopted Resolution 4347-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on February 8, 2021, the City Council of the City of Fort Bragg adopted Resolution 4351-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on February 22, 2021, the City Council of the City of Fort Bragg adopted Resolution 4358-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on March 8, 2021, the City Council of the City of Fort Bragg adopted Resolution 4363-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on March 22, 2021, the City Council of the City of Fort Bragg adopted Resolution 4366-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on April 12, 2021, the City Council of the City of Fort Bragg adopted Resolution 4376-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on April 26, 2021, the City Council of the City of Fort Bragg adopted Resolution 4381-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on May 10, 2021, the City Council of the City of Fort Bragg adopted Resolution 4385-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on May 24, 2021, the City Council of the City of Fort Bragg adopted Resolution 4391-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on June 14, 2021, the City Council of the City of Fort Bragg adopted Resolution 4396-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on June 28, 2021, the City Council of the City of Fort Bragg adopted Resolution 4405-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on July 12, 2021, the City Council of the City of Fort Bragg adopted Resolution 4418-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on July 26, 2021, the City Council of the City of Fort Bragg adopted Resolution 4422-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on August 9, 2021, the City Council of the City of Fort Bragg adopted Resolution 4427-2021 by which it continued the local emergency; and

**WHEREAS**, at a special meeting on August 30, 2021, the City Council of the City of Fort Bragg adopted Resolution 4434-2021 by which it continued the local emergency; and

**WHEREAS**, at a special meeting on September 20, 2021, the City Council of the City of Fort Bragg adopted Resolution 4447-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on October 12, 2021, the City Council of the City of Fort Bragg adopted Resolution 4451-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on October 25, 2021, the City Council of the City of Fort Bragg adopted Resolution 4460-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on November 8, 2021, the City Council of the City of Fort Bragg adopted Resolution 4463-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on November 22, 2021, the City Council of the City of Fort Bragg adopted Resolution 4473-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on December 13, 2021, the City Council of the City of Fort Bragg adopted Resolution 4480-2021 by which it continued the local emergency; and

**WHEREAS**, at a special meeting on December 27, 2021, the City Council of the City of Fort Bragg adopted Resolution 4491-2021 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on January 10, 2022, the City Council of the City of Fort Bragg adopted Resolution 4497-2022 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on January 24, 2022, the City Council of the City of Fort Bragg adopted Resolution 4504-2022 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on February 14, 2022, the City Council of the City of Fort Bragg adopted Resolution 4509-2022 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on February 28, 2022, the City Council of the City of Fort Bragg adopted Resolution 4513-2022 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on March 14, 2022, the City Council of the City of Fort Bragg adopted Resolution 4518-2022 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on March 28, 2022, the City Council of the City of Fort Bragg adopted Resolution 4525-2022 by which it continued the local emergency; and

**WHEREAS**, at a regular meeting on April 11, 2022, the City Council of the City of Fort Bragg adopted Resolution 4529-2022 by which it continued the local emergency

**NOW, THEREFORE, BE IT RESOLVED AND PROCLAIMED** by the City Council of the City of Fort Bragg that for reasons set forth herein, said local emergency shall be deemed to continue to exist until the City Council of the City of Fort Bragg, State of California, proclaims its termination; and

**BE IT FURTHER RESOLVED** that the City Council of the City of Fort Bragg will review the need for continuing the local emergency at least once every 21 days until the City Council terminates the local emergency; and

**BE IT FURTHER RESOLVED** that this resolution confirming the continued existence of a local emergency shall be forwarded to the Director of the Governor's Office of Emergency Services and the Governor of the State of California, as well as the Mendocino County Office of Emergency Services.

**The above and foregoing Resolution was introduced by Councilmember \_\_\_\_\_, seconded by Councilmember \_\_\_\_\_, and passed and adopted at a regular meeting of the City Council of the City of Fort Bragg held on the 25th day of April, 2022 by the following vote:**

- AYES:**
- NOES:**
- ABSENT:**
- ABSTAIN:**
- RECUSED:**

---

**BERNIE NORVELL**  
**Mayor**

**ATTEST:**

---

**June Lemos, MMC  
City Clerk**

**From:** [Jacob Patterson](#)  
**To:** [Lemos, June](#)  
**Cc:** [Spaur, David](#); [sarah mccormick](#); [Smith, John](#)  
**Subject:** Public Comment -- 4/25/22 CC mtg., Item Nos. 5E, 5G, and 8D  
**Date:** Monday, April 25, 2022 1:05:39 PM

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City Council,

It might be a little odd connecting consent calendar Items 5E and 5G for the purposes of public comment but I am doing so because the City may be focusing on the wrong things in how we are allocating scarce staff time and financial resources compared to the actual pressing problems facing the community. The Covid-19 pandemic is serious but I think the more pressing "emergency" facing the community is your lack of collective judgment and prioritization of efforts that we can actually address on a local level.

Please see the below excerpt from yesterday's Mendocino County Today in the AVA, which sums up the situation nicely, IMO. As a City, we appear to be wasting a lot of time, money, and energy on relatively low-impact and low-utility efforts that cater to local special interests and fun pet projects of particular councilmembers, staff (or former staff) rather than tackling many of the real problems facing our community, some of which are discussed in the AVA excerpt below. I recognize that some of the listed issues are not clearly within the direct control of the City or City Council but we are spending a lot of time and money on relatively unimportant projects like investing in public money to develop the ocean water infrastructure for the Noyo Center's potential--emphasis on potential--future aquarium and research center and allegedly for a blue economy business park that is not even permitted by the current zoning (except aquaculture) and isn't likely to ever come to fruition despite all of the best intentions of the supporters of such concepts. Why are we investing so many local public resources with a single well-connected local non-profit but effectively ignoring so many other more pressing community needs and services that have a greater likelihood of tangible economic benefits or much-needed local services? Where is the urgency dealing with facilitating future healthcare services on the coast as we continue to face diminishing access to such services? What is happening with the Community Land Trust and how is it actually moving forward to developing more local housing opportunities? What are we doing to address the numerous housing-related programs we committed to consider when you adopted the current housing element? Where is the pilot safe parking program for mobile homeless living in vehicles and RVs that won't be covered by the tiny home ordinance--all the more pressing because County code enforcement efforts are apparently leading to eviction notices for at least some of the long-term residents of Wildwood Campground? (Where do you think those people are likely to end up along with their trailers and RVs?...)

To address the matters at hand on the agenda tonight, why are we spending so much time on cannabis-related issues when the legal cannabis industry is in such decline and we likely missed the boat on getting our regulations in order? There are rapidly diminishing returns at this point and facilitating more commercial cannabis activity in town is less likely to generate meaningful economic development and the free-for-all that is being pushed by the Council majority for the CBD is likely to harm our other economic development opportunities for our downtown core; meanwhile we are effectively ignoring the industrial areas north of town where such development has already been proposed and is the most likely area for future economic development related to commercial cannabis, particularly with the pending end of the sunset period for the Mitchell Creek and Simpson Lane County neighborhood in June 2022. What is the purpose of even adopting a new moratorium for pending applications in the

CBD when the only applications we have in the CBD are not approvable under the current code so we don't need to worry about them getting approved prior to the new regulations potentially going into effect mid-July?

On a more granular note, why was the report in 5G not prepared and presented to the City Council prior to the expiration of the prior moratorium as is required? Can we do nothing correctly according to legally-required procedures? When will you recognize that there is a problem with how the City is doing things and try to make some changes to get us on the right procedural track? When will the City actually hire qualified people to perform these functions rather than sometimes hiring apparently under-qualified people who may function as relatively expensive interns who we then pay to develop many of the basic qualifications for their jobs they likely should have already possessed in order to be hired in the first place and using public funds to do so? What is going on and why are none of you doing enough to right the ship (at least in the opinion of people like me who pay attention to details and City projects and activities overall rather than just lookign at each project and effort in a silo without taking a more broad perspective)? I wouldn't be surprised if the new city manager gets to town and doesn't even unpack before she/he/they go running for the hills because of all of the issues and dysfunction apparent in how the City is being run not the least of which appears to be an illogical organizational structure and inefficient allocation of staff positions within City Hall that appears to be structure to cater to the needs and wants of the current staff rather than what actually makes sense to serve and provide services to the community and the public-at-large. Please consider these topics as you decide how to proceed with the agenda items tonight and in general as the City moves forward with budget development and various projects and efforts.

Please note that I have no idea who wrote into the AVA under "name withheld" and it wasn't me; I first became a local resident by being born at the hospital a decade prior to the author's arrival.

Regards,

--Jacob



## FORT BRAGG'S DIMINISHING CORE SERVICES

Editor,

Over the past 30+ years...since I moved to Fort Bragg in 1986. I've loved this town and all that it represented (past tense).

However, as of late, what seems like almost overnight, Fort Bragg has become a town in crisis!

All the institutions that we came to depend on and respect are for all intents and purposes GONE! I raised my son here in great schools, acquired my AA at the local JC, received caring and supportive medical/emergency services for my rescues, was on the receiving end of dedicated and devoted medical health professionals and more.

But, these and too many other institutions that I've come to depend on are no longer available to the residents of this community.

The Junior College is a ghost town, the hospital struggles to entice doctors to the Coast. Emergency and even regular veterinary care services are all but non-existent! We are forced to seek services elsewhere, off the Coast, and for those with limited resources, they do without.

Fort Bragg City Council members, I ask you: What is it, exactly, that you are doing to address the absence of essential, core services of this community, services you were elected to provide?

What exactly have or are you currently doing/planning to address the absence of the most basic needs of this community?

What are you doing or plan to do to put an end to the ever-diminishing services that once were available to this community?

It's a long time since 1986, but I would ask you, Council members, are we, here on the Coast, here in Fort Bragg, better off now, than we were 10 years ago? 20 years ago?

That answer is abundantly clear: NO! Not even close!

And now, you talk about dissolving the Mendocino Coast Health Care District? Why? Because you've already accomplished so much in providing for the needs of this community? My guess? Pure arrogance? Another agenda? Your inbox is overflowing. Start doing what you were elected to do!

Name Withheld

Fort Bragg





# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-194

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Resolution

**Agenda Number:** 5F.

Adopt City Council Resolution Adopting a List of Projects for Fiscal Year 2022-23 Funded by SB  
1: The Road Repair and Accountability Act of 2017

**RESOLUTION NO. \_\_\_\_-2022**

**RESOLUTION OF THE FORT BRAGG CITY COUNCIL ADOPTING A LIST OF PROJECTS FOR FISCAL YEAR 2022-23 FUNDED BY SB 1: THE ROAD REPAIR AND ACCOUNTABILITY ACT OF 2017**

**WHEREAS**, Senate Bill 1 (SB 1), the Road Repair and Accountability Act of 2017 (Chapter 5, Statutes of 2017), was passed by the Legislature and signed into law by the Governor in April 2017 to address the significant multi-modal transportation funding shortfalls statewide; and

**WHEREAS**, SB 1 includes accountability and transparency provisions that will ensure the residents of our City are aware of the projects proposed for funding in our community and which projects have been completed each fiscal year; and

**WHEREAS**, the City must adopt by resolution a list of projects proposed to receive fiscal year funding from the Road Maintenance and Rehabilitation Account (RMRA), created by SB 1, which must include a description and the location of each proposed project, a proposed schedule for the project's completion, and the estimated useful life of the improvement; and

**WHEREAS**, the City will receive an estimated \$168,886 in RMRA funding in Fiscal Year 2022-23 from SB 1; and

**WHEREAS**, this is the sixth year in which the City is receiving SB 1 funding which will enable the City to continue essential road maintenance and rehabilitation projects, safety improvements, repairing and replacing aging bridges, and increasing access and mobility options for the traveling public that would not have otherwise been possible without SB 1; and

**WHEREAS**, the City uses the Traffic Modification Requests and service requests submitted through the "Report-a-Streets" problem portal of the website received from citizens and approved by the Traffic Committee throughout the year to develop the SB 1 project locations to ensure revenues are being used on the projects that are high priority for our community; and

**WHEREAS**, the funding from SB 1 will help the City focus on the general maintenance and rehabilitation needs citywide; and

**WHEREAS**, the 2018 California Statewide Local Streets and Roads Needs Assessment found that the City’s streets and roads are in fair condition and this revenue will help us sustain the overall fair quality of our road system over the next decade; and

**WHEREAS**, the SB 1 project list and overall investment in our local streets and roads infrastructure with a focus on basic maintenance and safety will have significant positive impact on the City’s ability to procure equipment and materials for road maintenance work performed by Public Works staff.

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Fort Bragg does hereby identify the following projects to be funded with Road Maintenance and Rehabilitation Account revenues for the fiscal year 2022-23:

1. The foregoing recitals are true and correct.
2. The following list of newly proposed projects will be funded in part or solely with Fiscal Year 2022-23 Road Maintenance and Rehabilitation Account revenues:

**Project Title: PP01-2022. Street and Alley Pavement Maintenance**

**Project Description:** Procurement of RMRA Eligible Public Works Equipment, Road Maintenance and Rehab Materials, and Public Works Staffing to perform Small Scale pavement repair and patching. The goal of this project is to prevent street and alley pavement on local/residential streets with low Pavement Condition Index (PCI) ratings from deteriorating to an unusable state, as these street categories often go unselected for rehabilitation due to the algorithm used in the Pavement Management Program.

**Project Location:** Citywide

**Estimated Project Schedule:** Start (07/22) – Completion (06/23)

**Estimated Project Useful Life:** Depending on Materials used and existing pavement conditions, project benefits will last two to ten years.

**Project Title: PP02-2022. Sub-Surface Storm Drainage Maintenance and Repairs**

**Project Description:** Procurement of RMRA Eligible Public Works Equipment, Stormdrain Maintenance and Rehab Materials, and Public Works Staffing to perform sub-surface drainage facilities and repair overlying pavement. The goal of this project is to repair structural damage caused by storms or failures or damaged drainage facilities including curb and gutter, valley gutters, pipes, inlets, and outlets, which convey stormwater off and away from the roadways.

**Project Location:** Citywide

**Estimated Project Schedule:** Start (07/22) – Completion (06/23)

**Estimated Project Useful Life:** Drainage facility improvement will last 10-30 years.

The above and foregoing Resolution was introduced by Councilmember \_\_\_\_\_, seconded by Councilmember \_\_\_\_\_, and passed and adopted at a regular meeting of the City Council of the City of Fort Bragg held on the 25<sup>th</sup> day of April, 2022, by the following vote:

AYES:  
NOES:  
ABSENT:  
ABSTAIN:  
RECUSED:

---

**BERNIE NORVELL**  
Mayor

**ATTEST:**

---

**June Lemos, MMC**  
City Clerk



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-198

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Staff Report

**Agenda Number:** 5G.

Receive and File Written Report Pursuant to Government Code Section 65858(d) on Behalf of the City Council Describing the Measures Taken to Date to Alleviate the Condition Which Led to the Adoption of Urgency Ordinance 972-2021



AGENCY: City Council  
MEETING DATE: April 25, 2022  
DEPARTMENT: City Manager  
PREPARED BY: David Spaur  
EMAIL ADDRESS: [Dspaur@fortbragg.com](mailto:Dspaur@fortbragg.com)

## AGENDA ITEM SUMMARY

**TITLE:**

**Receive and File Written Report Pursuant to Government Code Section 65858(d) on Behalf of the City Council Describing the Measures Taken to Date to Alleviate the Condition Which Led to the Adoption of Urgency Ordinance 972-2021**

**ISSUE:**

Pursuant to Government Code Section 65858(d), this report was due ten days prior to the expiration of the City's last moratorium on approval of applications and permits for cannabis dispensaries.

**ANALYSIS:**

The following measures have taken place since the adoption of the original moratorium ordinance (Urgency Ordinance 972-2021): (1) The Planning Commission has made significant revisions to the original ordinance; and (2) The City Council has directed staff to make changes to the Planning Commission's recommended cannabis ordinance.

The Planning Commission, at their March 23, 2022 meeting, made a recommendation to the City Council to accept revisions made by the Planning Commission and to consider adoption of the Cannabis Ordinance.

The City Council, at their March 28, 2022 meeting, directed staff to make changes to the proposed cannabis ordinance to include regulations on location requirement, limit on cannabis businesses in the Central Business District, buffer around sensitive uses, fire sprinkler requirement for microbusiness, green energy requirement, definition and quantification of accessory uses, minor use permit requirement, microenterprise regulations, and hours of operation.

These revisions will be incorporated into a new proposed ordinance scheduled for a public hearing and introduction on May 23, 2022, with adoption on June 13, 2022.

Staff considered the expiration of the 45-day moratorium and the creation of a window between the sunset date of the moratorium on April 14, 2022 and the adoption of Ordinance 979-2022 possibly on June 13, 2022. After careful consideration, it was decided to recommend a new 45-day urgency ordinance until adoption of Ordinance 979-2022. That proposed urgency ordinance, No. 981-2022, is Item 8D on tonight's agenda.

**RECOMMENDED ACTION:**

Receive and file this report.

**ALTERNATIVE ACTION(S):**

Do not accept this report and provide staff further direction.

**FISCAL IMPACT:**

There is no identifiable fiscal impact at this time.

**CONSISTENCY:**

The report is consistent with the current process and will remain consist with the process for adoption going forward.

**GREENHOUSE GAS EMISSIONS IMPACT:**

N/A.

**IMPLEMENTATION/TIMEFRAMES:**

N/A

**ATTACHMENTS:**

N/A

**NOTIFICATION:**

N/A

**From:** [Jacob Patterson](#)  
**To:** [Lemos, June](#)  
**Cc:** [Spaur, David](#); [sarah mccormick](#); [Smith, John](#)  
**Subject:** Public Comment -- 4/25/22 CC mtg., Item Nos. 5E, 5G, and 8D  
**Date:** Monday, April 25, 2022 1:05:39 PM

---

City Council,

It might be a little odd connecting consent calendar Items 5E and 5G for the purposes of public comment but I am doing so because the City may be focusing on the wrong things in how we are allocating scarce staff time and financial resources compared to the actual pressing problems facing the community. The Covid-19 pandemic is serious but I think the more pressing "emergency" facing the community is your lack of collective judgment and prioritization of efforts that we can actually address on a local level.

Please see the below excerpt from yesterday's Mendocino County Today in the AVA, which sums up the situation nicely, IMO. As a City, we appear to be wasting a lot of time, money, and energy on relatively low-impact and low-utility efforts that cater to local special interests and fun pet projects of particular councilmembers, staff (or former staff) rather than tackling many of the real problems facing our community, some of which are discussed in the AVA excerpt below. I recognize that some of the listed issues are not clearly within the direct control of the City or City Council but we are spending a lot of time and money on relatively unimportant projects like investing in public money to develop the ocean water infrastructure for the Noyo Center's potential--emphasis on potential--future aquarium and research center and allegedly for a blue economy business park that is not even permitted by the current zoning (except aquaculture) and isn't likely to ever come to fruition despite all of the best intentions of the supporters of such concepts. Why are we investing so many local public resources with a single well-connected local non-profit but effectively ignoring so many other more pressing community needs and services that have a greater likelihood of tangible economic benefits or much-needed local services? Where is the urgency dealing with facilitating future healthcare services on the coast as we continue to face diminishing access to such services? What is happening with the Community Land Trust and how is it actually moving forward to developing more local housing opportunities? What are we doing to address the numerous housing-related programs we committed to consider when you adopted the current housing element? Where is the pilot safe parking program for mobile homeless living in vehicles and RVs that won't be covered by the tiny home ordinance--all the more pressing because County code enforcement efforts are apparently leading to eviction notices for at least some of the long-term residents of Wildwood Campground? (Where do you think those people are likely to end up along with their trailers and RVs?...)

To address the matters at hand on the agenda tonight, why are we spending so much time on cannabis-related issues when the legal cannabis industry is in such decline and we likely missed the boat on getting our regulations in order? There are rapidly diminishing returns at this point and facilitating more commercial cannabis activity in town is less likely to generate meaningful economic development and the free-for-all that is being pushed by the Council majority for the CBD is likely to harm our other economic development opportunities for our downtown core; meanwhile we are effectively ignoring the industrial areas north of town where such development has already been proposed and is the most likely area for future economic development related to commercial cannabis, particularly with the pending end of the sunset period for the Mitchell Creek and Simpson Lane County neighborhood in June 2022. What is the purpose of even adopting a new moratorium for pending applications in the

CBD when the only applications we have in the CBD are not approvable under the current code so we don't need to worry about them getting approved prior to the new regulations potentially going into effect mid-July?

On a more granular note, why was the report in 5G not prepared and presented to the City Council prior to the expiration of the prior moratorium as is required? Can we do nothing correctly according to legally-required procedures? When will you recognize that there is a problem with how the City is doing things and try to make some changes to get us on the right procedural track? When will the City actually hire qualified people to perform these functions rather than sometimes hiring apparently under-qualified people who may function as relatively expensive interns who we then pay to develop many of the basic qualifications for their jobs they likely should have already possessed in order to be hired in the first place and using public funds to do so? What is going on and why are none of you doing enough to right the ship (at least in the opinion of people like me who pay attention to details and City projects and activities overall rather than just lookign at each project and effort in a silo without taking a more broad perspective)? I wouldn't be surprised if the new city manager gets to town and doesn't even unpack before she/he/they go running for the hills because of all of the issues and dysfunction apparent in how the City is being run not the least of which appears to be an illogical organizational structure and inefficient allocation of staff positions within City Hall that appears to be structure to cater to the needs and wants of the current staff rather than what actually makes sense to serve and provide services to the community and the public-at-large. Please consider these topics as you decide how to proceed with the agenda items tonight and in general as the City moves forward with budget development and various projects and efforts.

Please note that I have no idea who wrote into the AVA under "name withheld" and it wasn't me; I first became a local resident by being born at the hospital a decade prior to the author's arrival.

Regards,

--Jacob



## FORT BRAGG'S DIMINISHING CORE SERVICES

Editor,

Over the past 30+ years...since I moved to Fort Bragg in 1986. I've loved this town and all that it represented (past tense).

However, as of late, what seems like almost overnight, Fort Bragg has become a town in crisis!

All the institutions that we came to depend on and respect are for all intents and purposes GONE! I raised my son here in great schools, acquired my AA at the local JC, received caring and supportive medical/emergency services for my rescues, was on the receiving end of dedicated and devoted medical health professionals and more.

But, these and too many other institutions that I've come to depend on are no longer available to the residents of this community.

The Junior College is a ghost town, the hospital struggles to entice doctors to the Coast. Emergency and even regular veterinary care services are all but non-existent! We are forced to seek services elsewhere, off the Coast, and for those with limited resources, they do without.

Fort Bragg City Council members, I ask you: What is it, exactly, that you are doing to address the absence of essential, core services of this community, services you were elected to provide?

What exactly have or are you currently doing/planning to address the absence of the most basic needs of this community?

What are you doing or plan to do to put an end to the ever-diminishing services that once were available to this community?

It's a long time since 1986, but I would ask you, Council members, are we, here on the Coast, here in Fort Bragg, better off now, than we were 10 years ago? 20 years ago?

That answer is abundantly clear: NO! Not even close!

And now, you talk about dissolving the Mendocino Coast Health Care District? Why? Because you've already accomplished so much in providing for the needs of this community? My guess? Pure arrogance? Another agenda? Your inbox is overflowing. Start doing what you were elected to do!

Name Withheld

Fort Bragg





# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-186

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Committee Minutes

**Agenda Number:** 5H.

Receive and File Minutes of the Public Works and Facilities Committee Meeting of February 10, 2022



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Meeting Minutes Public Works and Facilities Committee

---

Thursday, February 10, 2022

3:00 PM

Via Video Conference

---

### MEETING CALLED TO ORDER

Committee Chair Peters called meeting to order at 3:00 pm.

### ROLL CALL

All committee members present as well as Director Smith, Assistant City Engineer Chantell O'Neal, temporary Committee Clerk Sarah Peters and Diana Sanchez.

**Present:** 2 - Lindy Peters and Tess Albin-Smith

### PLEASE TAKE NOTICE

Clerk Peters read the "Please take notice clause".

### 1. APPROVAL OF MINUTES

1A. [22-064](#) Approve Minutes of December 9, 2021

This Committee Minutes was approved for Council review

### 2. PUBLIC COMMENTS ON NON-AGENDA ITEMS

One non-agenda item public comment received from:  
Kathy Silva

### 3. CONDUCT OF BUSINESS

3A. [21-638](#) Public Works Director Oral Report on Departmental Updates and Items of Interest

Director Smith addressed committee members on departmental items of interest, provided responses to inquiries and project status' on:

- Todd's Point parking situation; this is County jurisdiction. Director to request signage and take to Traffic Safety Committee.

- One way alley behind Starbuck's; dangerous with low visibility of oncoming traffic. Director recommended taking this item to the next Traffic Safety Committee and include Starbuck's owners as well as concerned members of the public.

- Consider a tree ordinance; research tree's. Look into funding options and condition of existing city trees. Committee member Albin-Smith offered to begin preparations. Committee suggested

combining efforts of tree planting and a maintenance program. Bring back to Public Works Committee as an agenda item for future discussion and public input.

Director provided updates on:

- Current water conditions; no rain, low flows, looking forward to another drought year. Begin water emergency preparations.
- Water tank project; in design. Working on CDBG funding.
- Raw water line project; anticipating construction to begin in late summer.
- Water Treatment Plant Project; working on funding. Project in design, looking at late summer for construction.
- Water Meter Replacement Project; preparing for preconstruction meeting. A few months for meter delivery and installation.
- Pudding Creek Water and Sewer Line Project: Caltrans will be managing project, currently reviewing plans.
- Biosolids Dryer; looking at receiving it fully soon, due to transportation delays.
- 2021 CIPP Project; underway, waiting for equipment delivery.
- USDA Grant applications for equipment; under review. Expecting matching funds to come in for previous applications soon.
- Solid Waste SB 1383; staff working on \$20,000 grant application.
- Artificial turf soccer fields and new playground status; to be discussed at next CDD meeting.
- Facility repairs/project in process; Fire Station, Town Hall Bathrooms, Guest House Museum, PD, Corp Yard Building.
- 2022 PSPS; anticipate preparations.

#### **4. MATTERS FROM COMMITTEE / STAFF**

Chair Peters recognized the large amount of work in addition to ongoing projects with limited staff. John Smith reported two maintenance workers to begin soon.

Albin-Smith requested status of Dig Once policy; possibility to incorporate the policy to the upcoming Streets Rehabilitation Project and Broadband. Assistant City Engineer O'Neal explained the policy effects and challenges. Albin-Smith suggested looking into other City's who have experienced this implementation and working with design engineers with experience.

Chair Peters requested signage on the Coastal Trail spelling out rules and regulations; what is allowed and not allowed and citing Municipal Codes. Chair Peters to forward a list to Director Smith with suggestions.

## ADJOURNMENT

Chair Peters adjourned meeting at 3:39 pm.

**This was adjourned**



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-195

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Consent Agenda

**In Control:** City Council

**File Type:** Minutes

**Agenda Number:** 5I.

Approve Minutes of April 11, 2022



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Meeting Minutes City Council

*THE FORT BRAGG CITY COUNCIL MEETS CONCURRENTLY  
AS THE FORT BRAGG MUNICIPAL IMPROVEMENT DISTRICT  
NO. 1 AND THE FORT BRAGG REDEVELOPMENT  
SUCCESSOR AGENCY*

Monday, April 11, 2022

6:00 PM

Town Hall, 363 N. Main St. and  
Via Video Conference

### CALL TO ORDER

Acting Mayor Morsell-Haye called the meeting to order at 6:00 PM.

### PLEDGE OF ALLEGIANCE

### ROLL CALL

- Present:** 4 - Vice Mayor Jessica Morsell-Haye, Councilmember Tess Albin-Smith, Councilmember Lindy Peters and Councilmember Marcia Rafanan
- Absent:** 1 - Mayor Bernie Norvell

### AGENDA REVIEW

#### 1. MAYOR'S RECOGNITIONS AND ANNOUNCEMENTS

- 1A. [22-157](#)** Presentation of Proclamation Supporting and Standing in Solidarity with the People of Ukraine

Acting Mayor Morsell-Haye read a Proclamation supporting and standing in solidarity with Ukraine. Mary Rose Kaczorowski accepted the Proclamation, made remarks regarding the fight for freedom and an independent Ukraine, and presented the Council with a Ukrainian flag and posters. Mariya Tuzyk from southwestern Ukraine appeared at the meeting via Zoom and spoke about the current situation in Ukraine and thanked the Council for the Proclamation.

- 1B. [22-165](#)** Presentation of Proclamation Declaring April as Sexual Assault Awareness Month

Acting Mayor Morsell-Haye read a Proclamation declaring April as sexual assault awareness month. Makayla Chacon, Director of Services for Project Sanctuary, accepted the Proclamation.

#### 2. PUBLIC COMMENTS ON: (1) NON-AGENDA, (2) CONSENT CALENDAR & (3) CLOSED SESSION ITEMS

- (1) Jay McMartin-Rosenquist.
- (2) None.
- (3) None.

#### 3. STAFF COMMENTS

City Manager Spaur reported that interviews for a new City Manager will be held on April 20 and 21 and an announcement via press release will be made two weeks after a decision has been made. Chief Naulty gave an update on Police Department personnel and staffing and reported that the winter shelter was extended for two weeks. He noted that the recruitment for Police Chief opened today through Peckham & McKenney and will close on May 20. Interviews will be held the week of June 13.

#### **4. MATTERS FROM COUNCILMEMBERS**

Councilmember Peters reported that the City of Fort Bragg will be applying for the League of California Cities' Helen Putnam Award this year in the Public Works category for the desalination-reverse osmosis plant project. Councilmember Peters remarked on the powerful presentation by Ms. Tuzyk from Ukraine and condemned Vladimir Putin for war crimes.

#### **5. CONSENT CALENDAR**

##### **Approval of the Consent Calendar**

**A motion was made by Councilmember Peters, seconded by Councilmember Albin-Smith, to approve the Consent Calendar. The motion carried by the following vote:**

**Aye:** 4 - Vice Mayor Morsell-Haye, Councilmember Albin-Smith, Councilmember Peters and Councilmember Rafanan

**Absent:** 1 - Mayor Norvell

**5A. [22-162](#)** Adopt by Title Only and Waive the Second Reading of Ordinance No. 978-2022 Adding Chapter 6.09 (Organic Waste Disposal Reduction) to Title 6 (Health and Sanitation) of the Fort Bragg Municipal Code in Compliance with SB 1383

**This Ordinance was adopted on the Consent Calendar.**

Enactment No: ORD 978-2022

**5B. [22-163](#)** Adopt City Council Resolution Making the Legally Required Findings to Continue to Authorize the Conduct of Remote "Telephonic" Meetings During the State of Emergency

**This Resolution was adopted on the Consent Calendar.**

Enactment No: RES 4528-2022

**5C. [22-164](#)** Adopt City Council Resolution Confirming the Continued Existence of a Local Emergency in the City of Fort Bragg

**This Resolution was adopted on the Consent Calendar.**

Enactment No: RES 4529-2022

**5D. [22-167](#)** Readopt Master Traffic Resolution

**This Resolution was adopted on the Consent Calendar.**

Enactment No: RES 1271-2022/A

- 5E. [22-168](#) Adopt City Council Resolution Authorizing a Budget Amendment to the FY 2021-22 (Budget Amendment No. 2022-21) Adding Funds to Police Department Salary (Account No. 110-4200-0101), Creating Social Services Liaison (Account No. 110-4200-0379) and a Grant Reimbursement Account (Account No. 110-4200-3318) to Fund the Social Services Liaison Program Through a Fully Reimbursable Grant to Include the Purchase of a Vehicle and the Hiring of Two Employees

**This Resolution was adopted on the Consent Calendar.**

Enactment No: RES 4530-2022

- 5F. [22-169](#) Adopt City Council Resolution Establishing a City of Fort Bragg Master Salary Rate Compensation Plan Adding a New Classification for Social Service Liaison - Crisis Worker

**This Resolution was adopted on the Consent Calendar.**

Enactment No: RES 4531-2022

- 5G. [22-154](#) Approve Minutes of March 28, 2022

**These Minutes were approved on the Consent Calendar.**

- 5H. [22-170](#) Approve Minutes of Special Closed Session of April 5, 2022

**These Minutes were approved on the Consent Calendar.**

## **6. DISCLOSURE OF EX PARTE COMMUNICATIONS ON AGENDA ITEMS**

None.

## **7. PUBLIC HEARING**

- 7A. [22-166](#) Open Public Hearing and Immediately Continue Public Hearing to Date, Time and Place Certain - May 23, 2022 at 6:00 PM at Town Hall, 363 N. Main Street - to Consider Introduction of Ordinance 979-2022 Relating to Cannabis Regulations

**Acting Mayor Morsell-Haye opened the public hearing at 6:34 PM and continued the hearing to May 23, 2022 at 6:00 PM.**

**The public hearing on this Ordinance was continued to May 23, 2022.**

- 7B. [22-155](#) Receive Report, Receive Planning Commission Recommendation, Conduct Public Hearing, and Consider Introducing by Title Only and Waiving the First Reading of Ordinance 980-2022 Amending Section 18.42.110 (Mobile/Manufactured Homes and Mobile Home Parks) and Adding Section 18.42.175 (Tiny Homes) to Chapter 18.42 (Standards For Specific Land Uses) of Title 18 (Inland Land Use And Development Code) of the Fort Bragg

## Municipal Code

**Acting Mayor Morsell-Haye opened the public hearing at 6:35 PM.**

Assistant Planner Locke presented the staff report on this agenda item.

Public Comment was received from:

- Shelley Green spoke in support of converting existing garages to dwelling units.
- Pearl Martin commented that setbacks should be open to discussion to allow more freedom, and noted that the cost of a concrete foundation pad can be expensive.
- Walter Stillman thanked the Council for bringing the ordinance forward and spoke in support of its adoption.
- Jay McMartin-Rosenquist stated that the ordinance does not describe the size of parcels where tiny homes are allowed.
- Jacob Patterson suggested deleting the concrete pad requirement and encouraged Council to adopt the ordinance.
- Johanna Jensen commented that the Tiny Home ordinance was not only good for people who need housing, but also for people who would like additional rental income.

**Acting Mayor Morsell-Haye closed the public hearing at 7:11 PM.**

Discussion was held regarding the concrete pad requirement, foundation system, and earthquake safety. It was noted that the ordinance does not require only concrete pads, but allows alternative foundation methods to be permitted at the discretion of the Community Development Director.

**A motion was made by Councilmember Peters, seconded by Councilmember Albin-Smith, that this Ordinance be introduced by title only, waiving the reading of the text. The motion carried by the following vote:**

**Aye:** 4 - Vice Mayor Morsell-Haye, Councilmember Albin-Smith, Councilmember Peters and Councilmember Rafanan

**Absent:** 1 - Mayor Norvell

**8. CONDUCT OF BUSINESS****9. CLOSED SESSION**

**Acting Mayor Morsell-Haye recessed the meeting at 7:24 PM; the meeting reconvened to Closed Session at 7:36 PM.**

**9A. [22-171](#)**

CONFERENCE WITH LEGAL COUNSEL - EXISTING LITIGATION, Pursuant to Paragraph (1) of Subdivision (d) of Government Code Section 54956.9; Name of Case: City of Fort Bragg vs. Mendocino Railway and Does 1-10, Case No.: 21CV00850, Superior Court of the State of California, County of Mendocino

**Acting Mayor Morsell-Haye reconvened the meeting to Open Session at 7:51 PM and reported that the Court has said the Judge will issue an opinion on the case on or before May 19, 2022.**

**ADJOURNMENT**

**Acting Mayor Morsell-Haye adjourned the meeting at 7:51 PM.**

\_\_\_\_\_  
BERNIE NORVELL, MAYOR

\_\_\_\_\_  
June Lemos, MMC, City Clerk

IMAGED (\_\_\_\_\_)



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-124

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**Agenda Date:** 4/25/2022

**Version:** 2

**Status:** Business

**In Control:** City Council

**File Type:** Report

**Agenda Number:** 8A.

Receive and File Local Road Safety Plan Report from TJKM and Provide Recommendations for Select Highway Safety Improvement Program Grant Application



# Local Road Safety Plan

Fort Bragg City Council Meeting

April 25, 2022



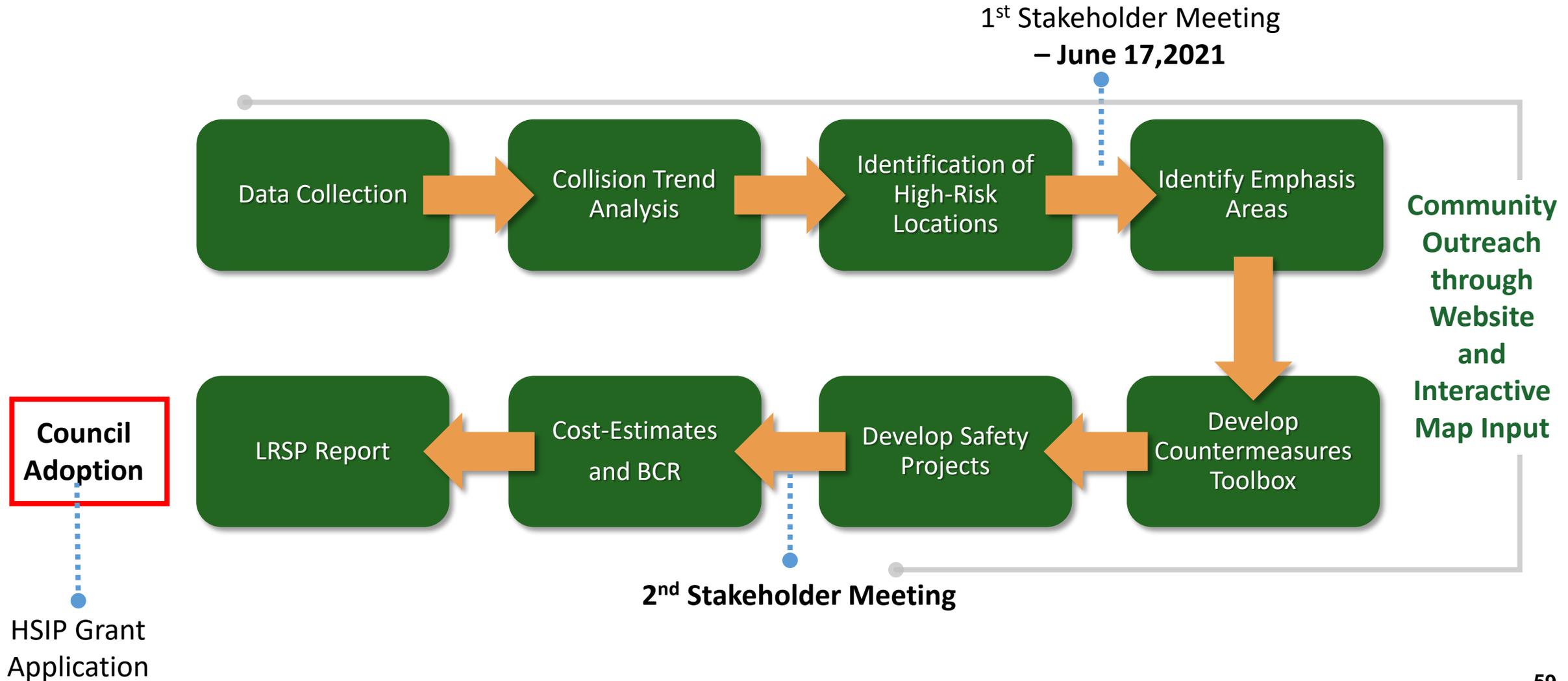
# Purpose of this meeting

- Present Local Roadway Safety Plan
- Seek Council comments and feedback
- Seek Council approval

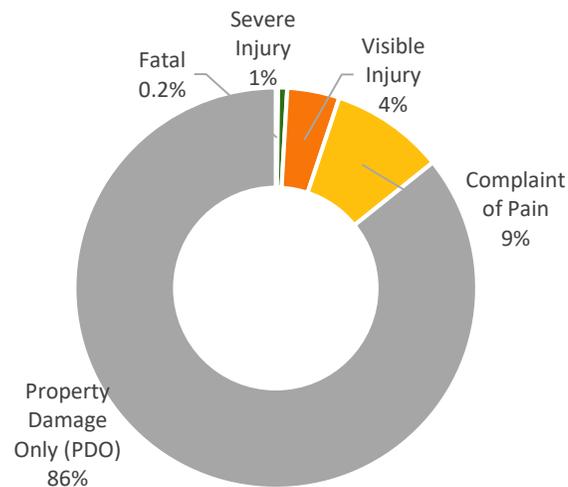
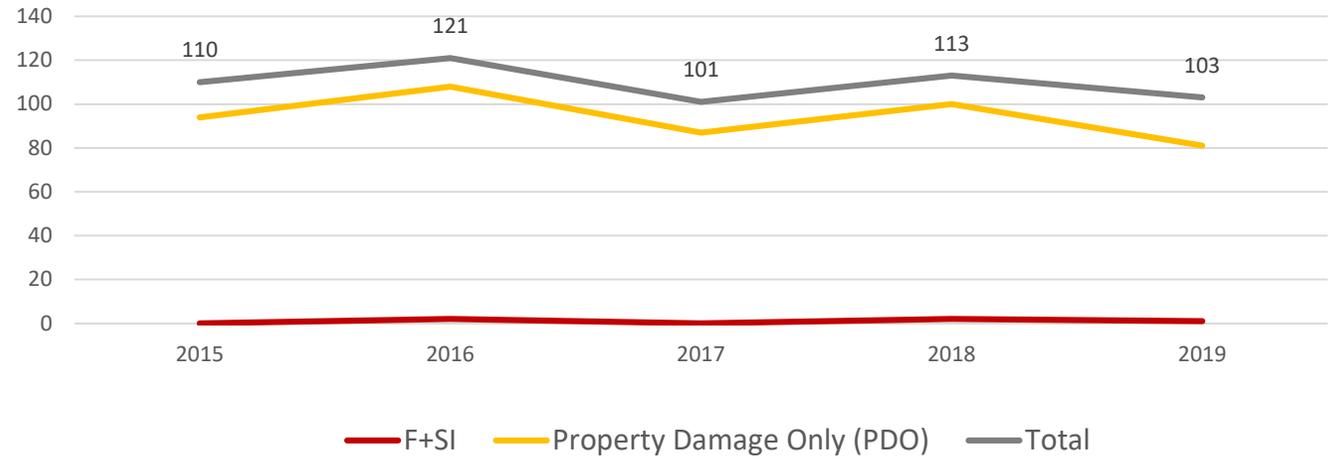
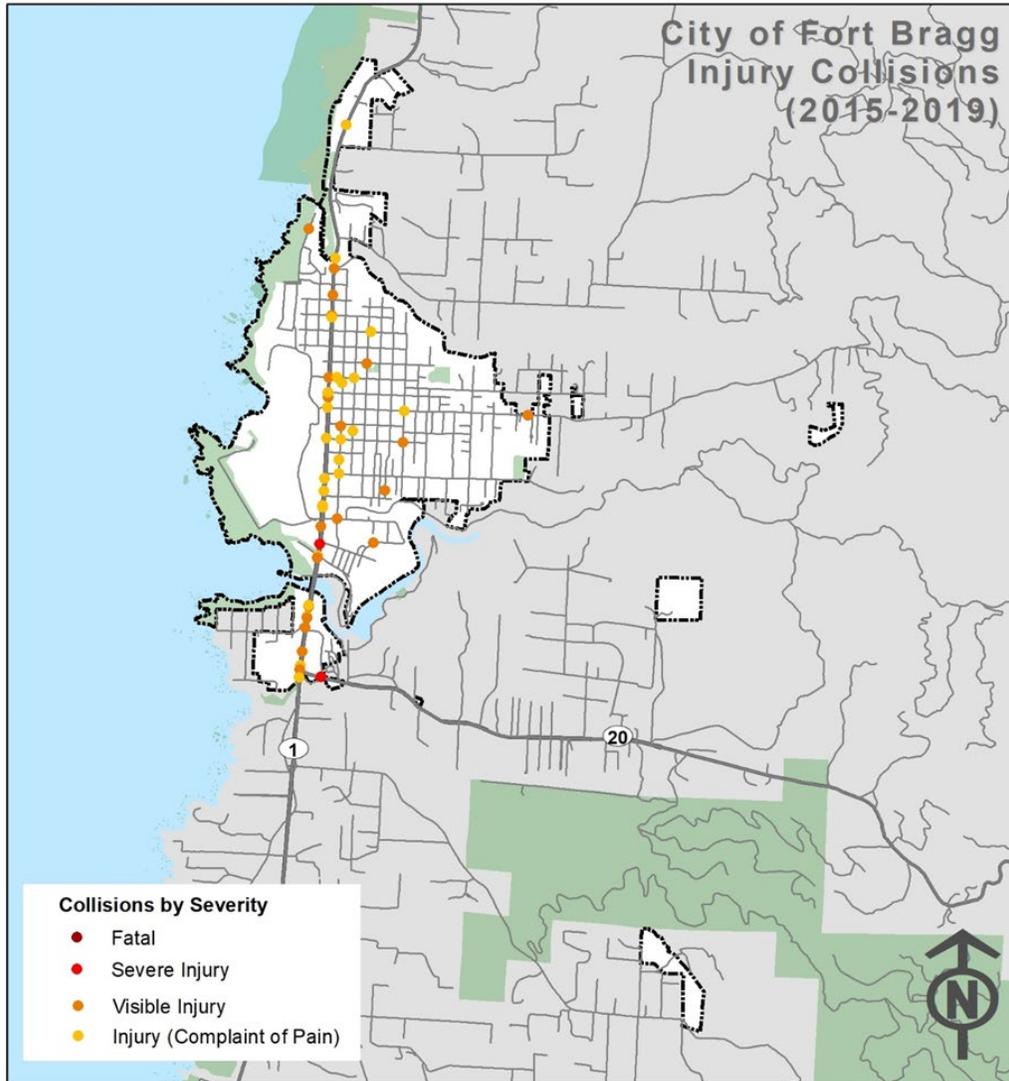
# What is Local Roadway Safety Plan ?

- **Overarching Goals:**
  - To reduce fatalities and severe injuries (F+SI) on the City's roadways and intersections
  - To identify, analyze and prioritize safety improvements on local roads
  - A required document to be eligible for the Highway Safety Improvement Program (HSIP) grant funding
- **Considers Engineering and Non-engineering Strategies**
  - 4 E's of Traffic Safety: Education, Enforcement, Engineering and Emergency Medical Services

# Project Status and Milestones



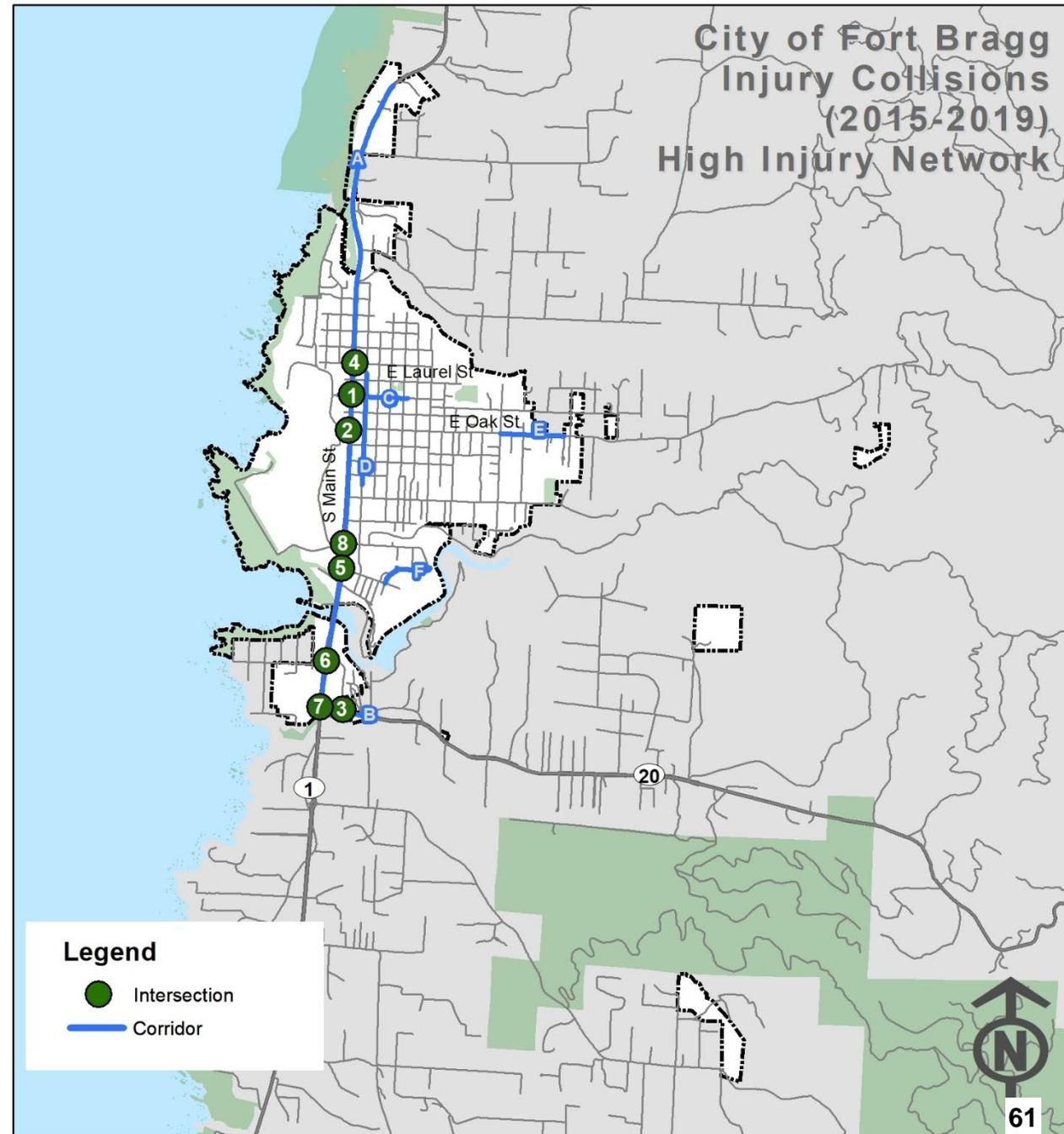
# Analysis Findings



- 548 collisions between 2015-2019
- 5 fatal and severe injury (F+SI collisions)
- 1 killed and 78 injured

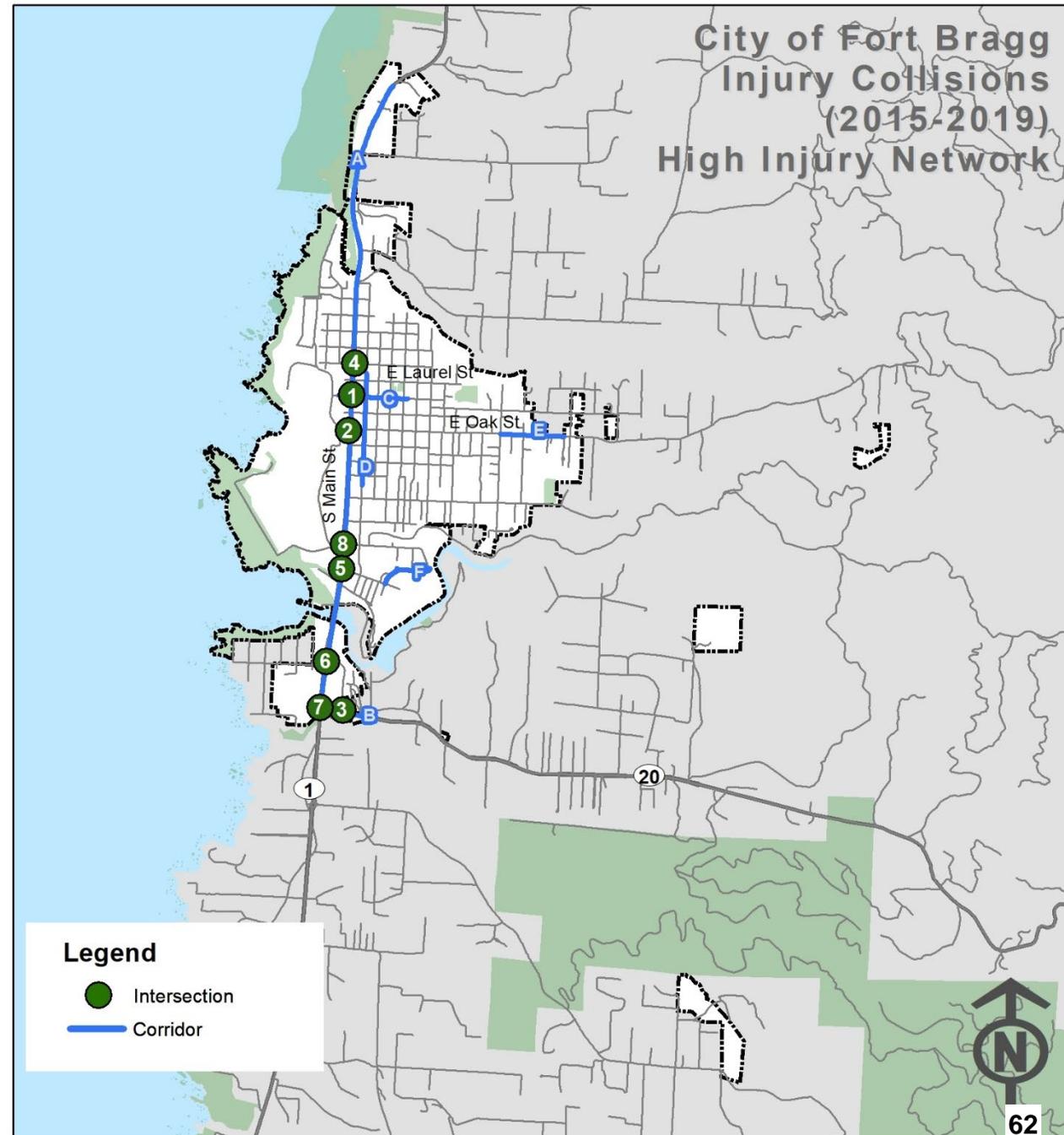
# High-Injury Intersections

ID	Intersection	Total	F+SI	EPDO Score
		Collisions		
1	Redwood Ave and Route 1/S Main St	5	1	171
2	Oak St and Route 1/S Main St	2	1	165
3	Boat Yard Dr and Route 20	1	1	165
4	Pine St and Route 1/ S Main St	1	1	165
5	South St and Route 1/S Main St	1	1	165
6	Boat Yard Drive and Route 1/ S Main St	3	0	28
7	Route 1 and Route 20	3	0	23
8	Cypress St and Route 1/ S Main St	3	0	18



# High-Injury Corridors

ID	Corridors	Total	F+SI	Length (miles)	EPDO Score
		Collisions			
A	Main St/Route 1: Jane Ln to Highway 20/ Fort Bragg Willits Rd	29	1	3.6	383
B	Highway 20/ Fort Bragg Willits Rd: Route 1 to South Harbor Dr	1	1	0.1	165
C	Redwood Ave: West Terminus to North Whipple St	5	0	0.3	35
D	Franklin St: Laurel St to E Chestnut St	3	0	0.6	23
E	Fort Bragg Sherrwood Rd: California Way to Dana St	1	0	0.2	11
F	River Dr/ Kempe Way: South St to Cypress St	1	0	0.3	11

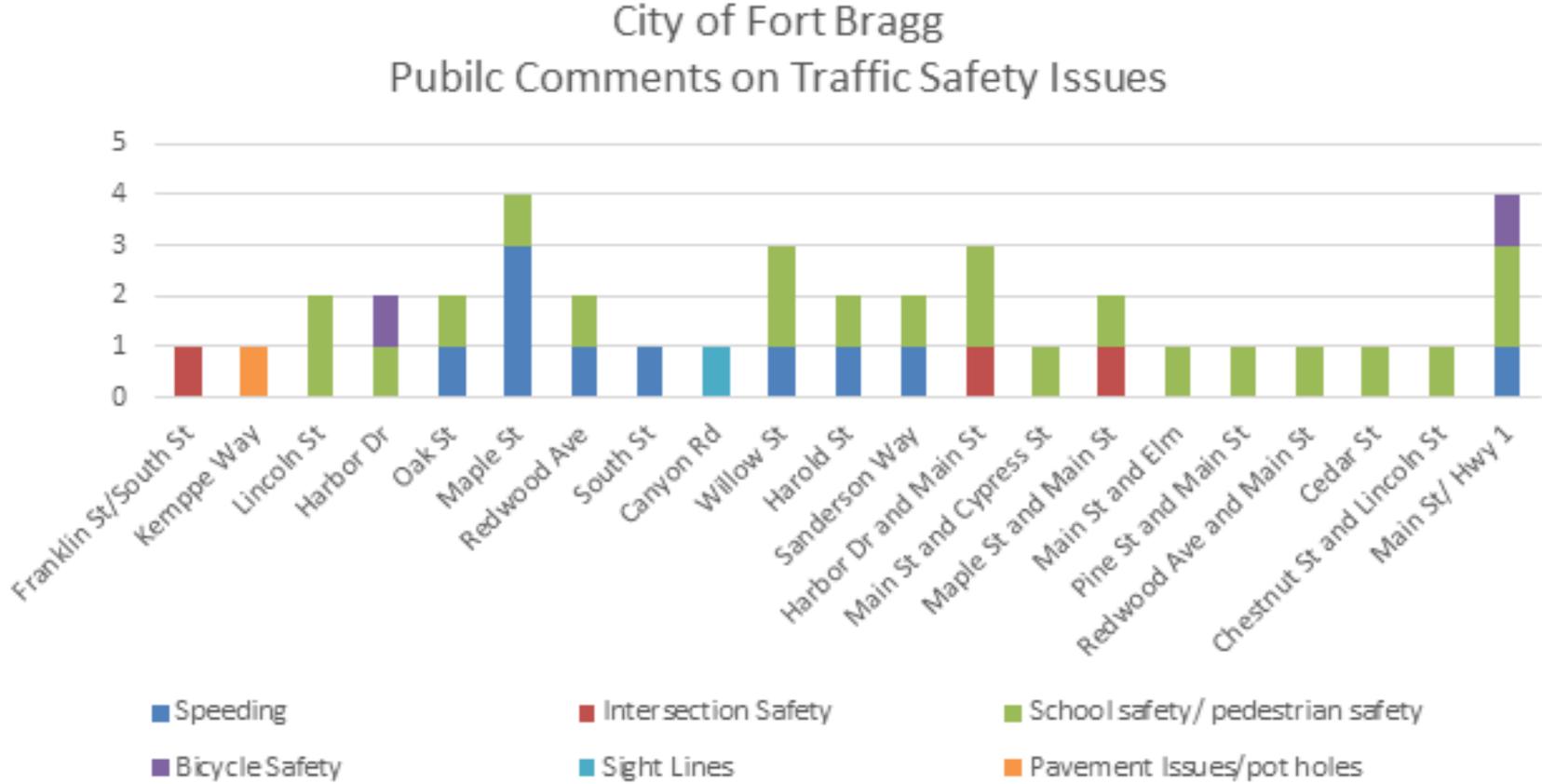


# Outreach Effort

- Map Input Responses

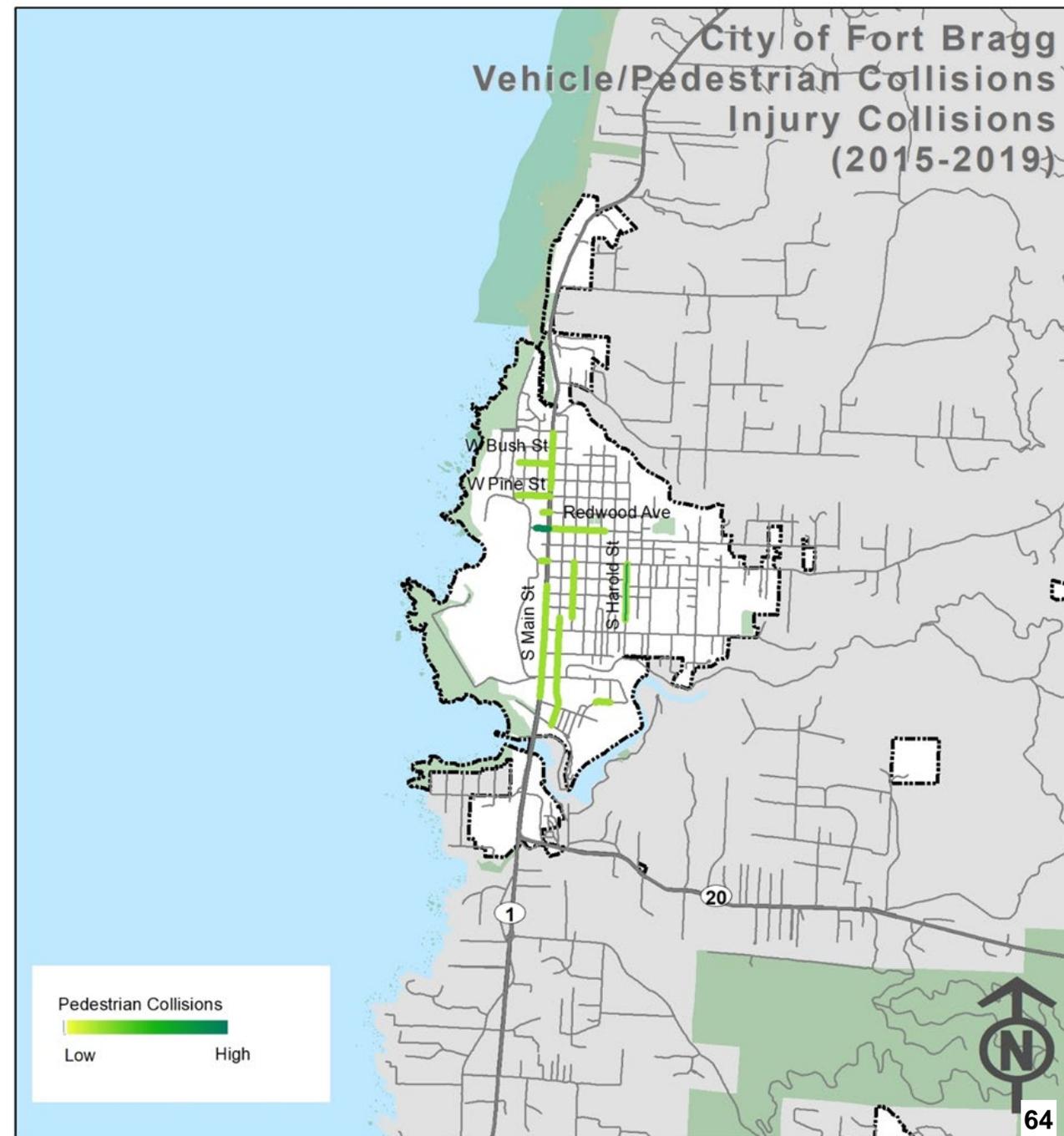
21 comments

- Survey Period – March 2021 – September 31, 2021

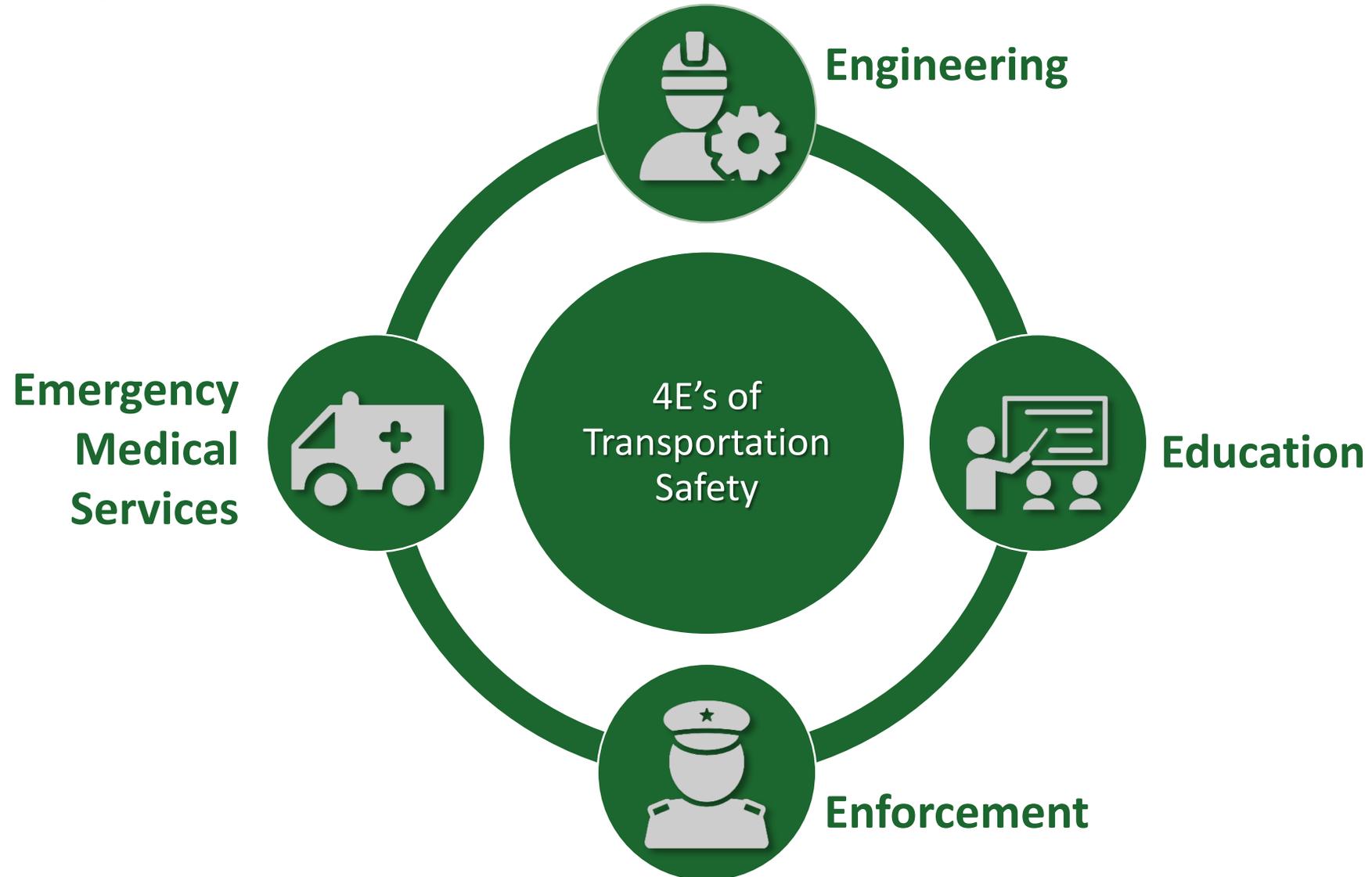


# Top Emphasis Areas

- Intersection Safety
  - Collisions within 250 feet of intersections
- Pedestrian Safety
  - Vehicle-Pedestrian collisions
  - Pedestrian right-of-way violations
  - Pedestrian violations
- Improper Turning Collisions
- Route 1 Collisions
- Alley Way Collisions



# E-Strategies



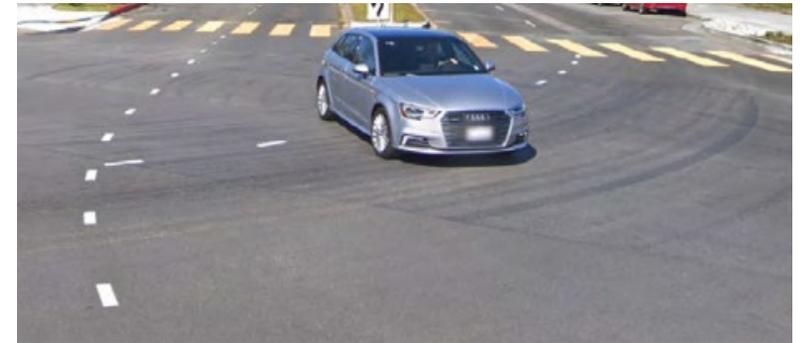
# Signalized Intersection Improvements



Improve Signal Hardware



Convert Signal to Mast Arm



Install raised pavement markers



Install raised median on approaches



Install Pedestrian Countdown



Modify signal phasing to implement a Leading Pedestrian Interval (LPI)

# Unsignalized Intersection Improvements



Install Signals



Install/upgrade larger or additional stop signs or other intersection warning/regulatory signs



Install right turn lane



Upgrade intersection pavement markings



Install/upgrade pedestrian crossing at uncontrolled locations



Install Rectangular Rapid Flashing Beacon (RRFB)

# Roadway Segment Improvements



Install/Upgrade signs with new fluorescent sheeting (regulatory or warning)



Install dynamic/variable speed warning signs



Install sidewalk/pathway



Install/upgrade pedestrian crossing (with enhanced safety features)



Install raised pedestrian crossing

# Non Engineering Strategies

- Education

- Conduct public information and education campaign for intersection safety laws, unsafe speeds, distracted driving, improper turning and driving under the influence.
- Conduct bicycle and pedestrian safety campaigns and outreach to raise their awareness of bicycle and pedestrian safety needs through media outlets, social media and Bike and Walk Mendocino. Update pamphlet for crosswalk safety and bicycle safety for Fort Bragg every 3-5 years

- Enforcement

- Targeted enforcement at high-risk locations.
- Increase the number of personnel who have completed Advanced Roadside impaired Driving Enforcement (ARIDE) training

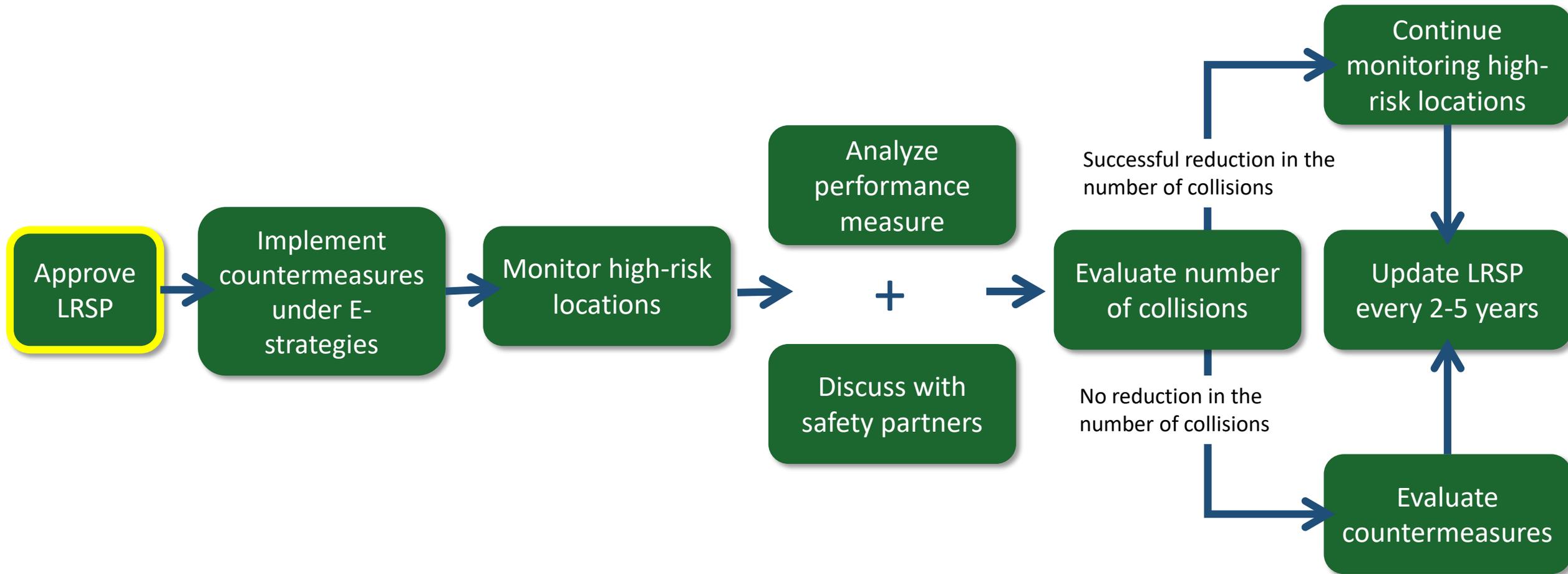
- EMS

- Install emergency vehicle pre-emption systems
- Increase the number of EMS/fire control personnel taking Traffic Incident Management Training

# Viable Safety Projects

- **Project #1:** Systemic Improvements at Signalized Intersections including improve signal timing, raised pavement markers and striping and addition of Leading Pedestrian Interval (LPI)
- **Project #2:** Pedestrian Improvements at Unsignalized Intersections, including addition of warning and regulatory signs, enhanced safety features, and RRFB's
- **Project #3:** Roadway Segment Improvements including upgrading signs with new fluorescent sheeting, dynamic/variable speed warning signs and enhanced pedestrian safety features.
- **Project #4:** Segment lighting and installation of sidewalks
- **Project #5:** Pedestrian set-aside to upgrade pedestrian crossing with enhanced safety features

# Implementation





THANK YOU



**City of Fort Bragg**

# **Local Roadway Safety Plan**

**04/15/2022**

**Draft Report**

**City of Fort Bragg**  
**Local Roadway Safety Plan**

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**City of Fort Bragg**  
**Local Roadway Safety Plan**

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- Appendix A: Matrix of Planning Goals, Policies, and Projects
- Appendix B: Consolidated Collision Database
- Appendix C: HSIP Eligible Countermeasures
- Appendix D: Countermeasure Toolbox
- Appendix E: B/C Ratio Calculation - LRSM (2020)

## **Executive Summary**

The City of Fort Bragg's Local Roadway Safety Plan (LRSP) is a comprehensive plan that creates a framework to systematically identify and analyze traffic safety related issues and recommend projects and countermeasures. The LRSP aims to reduce fatal and severe injury collisions through a prioritized list of improvements that can enhance safety on local roadways.

The LRSP takes a proactive approach to addressing safety needs. It is viewed as a guidance document that can be a source of information and ideas. It can also be a living document, one that is routinely reviewed and updated by City staff and their safety partners to reflect evolving collision trends and community needs and priorities. With the LRSP as a guide, the City will be able to ready to apply for grant funds, such as the federal Highway Safety Improvement Program (HSIP).

### **Chapter 1 – Introduction**

The Introduction presents the project, describes how this report is organized, summaries the vision and goals, the study area for the LRSP, details how the report is organized and introduces the safety partners.

### **Chapter 2 – Existing Planning Efforts**

This chapter summarizes existing City and regional planning documents and projects that are relevant to the LRSP. It ensures that the recommendations of the LRSP are in line with existing goals, objectives, policies, or projects. This chapter summarized the following documents: City of Fort Bragg Costal General Plan (2008), Inland General Plan (2012), Fort Bragg Bicycle Master Plan (2009), 2018 Street Safety Plan, South Main Street Access and Beautification Plan (2011), City of Trails: Trails Feasibility Study (2016), City of Trails: Supplemental Trail Feasibility Studies (2017), City of Fort Bragg FY 2020-2021 Budget, Mill Site Specific Plan (2012), Mendocino County Regional Active Transportation Plan (2017), Mendocino County Safe Routes to School Plan (2014), Mendocino County Pedestrian Facility Needs Inventory and Engineered Feasibility Study (2019), and Mendocino Council of Governments 2020 Regional Transportation Improvement Program (2019)

### **Chapter 3 – Collision Data Collection and Analysis**

Collision data was obtained and analyzed for a five-year period from 2015 to 2019 from the California Highway Patrol's Statewide Integrated Traffic Records System (SWITRS) and the University of California at Berkeley SafeTREC's Transportation Injury Mapping Service (TIMS).

## **City of Fort Bragg**

### **Local Roadway Safety Plan**

The collision analysis identified general trends of collisions in the City of Fort Bragg. There were a total of 548 collisions reported City-wide from 2015 to 2019. Out of these 470 collisions (86 percent) were property damage only (PDO) collisions, 50 collisions (9 percent) led to complaint of pain injury and 23 collisions (4 percent) led to a visible injury. There were 5 F+SI (fatal and severe injury) collisions, 4 collisions (1 percent) led to a severe injury and 1 collisions led to a fatality.

For fatal and severe injury (F+SI) collisions, 60 percent of collisions involved pedestrian. This calls for evaluating pedestrian conditions along this intersection and also throughout the City at locations with similar characteristics that are potentially unsafe for pedestrians. Improvements at these locations can include reducing pedestrian crossing distances, installing high visibility crosswalks, installing pedestrian refuge/ median islands, and installing bulb outs. The South Main Street Access and Beautification Plan contains similar proposed pedestrian improvements for South Main Street which were identified as high injury corridors. The pedestrian safety improvements identified in this plan may be used to provide the basis for a Highway Safety Improvements (HSIP) grant.

Gateway treatments and roundabouts are also identified in the South Main Street Access and Beautification Plan which can reduce speeds and provide traffic calming benefits to all road users. Automobile right of way, unsafe speed collisions and broadside collisions can be reduced with roundabouts which were also identified as being a main factor in fatal and severe injuries in Fort Bragg.

#### **Chapter 4 - Emphasis Areas**

Emphasis areas are a focus of the LRSP that are identified through the various collision types and factors resulting in fatal and severe injury collisions within the City of Fort Bragg. The nine emphasis areas for Fort Bragg are:

- Intersection Safety
  - a. Collisions within 250 feet of intersections
- Pedestrian Safety
- Improper Turning Collisions
- Route 1 Collisions
- Alley Ways Collisions
- Older Adult (Party at Fault) Collisions

## **City of Fort Bragg**

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#### **Chapter 5 – Countermeasure Identification**

Engineering countermeasures were selected for each of the high-risk locations and for the emphasis areas. These were based off of approved countermeasures from the Caltrans Local Roadway Safety Manual (LRSM) used in HSIP grant calls for projects. The intention is to give the City potential countermeasures for each location that can be implemented either in future HSIP calls for projects, or using other funding sources, such as the City’s Capital Improvement Program. Non-engineering countermeasures were also selected using the 4 E’s strategies, and are included with the emphasis areas.

#### **Chapter 6 – Safety Projects**

A set of four safety projects were created for high-risk intersections and roadway segments, using HSIP approved countermeasures. These safety projects are:

Project 1: Systemic Improvements at Unsignalized Intersections

Project 2: Pedestrian Improvements at Unsignalized Intersections

Project 3: Systemic Roadway Segment Improvements

Project 4: Pedestrian and Other Roadway Segment Improvements

Project 5: Pedestrian Set Aside

#### **Chapter 7 – Evaluation and Implementation**

The LRSP is a guidance document that is recommended to be updated every two to five years in coordination with the safety partners. The LRSP document provides engineering, education, enforcement, and emergency medical service related countermeasures that can be implemented throughout the City to reduce fatal and severe injury collisions. After implementing countermeasures, the performance measures for each emphasis area should be evaluated annually. The most important measure of success of the LRSP should be reducing fatal and severe injury collisions throughout the City. If the number of fatal and severe injury collisions does not decrease over time, then the emphasis areas and countermeasures should be re-evaluated.

## 1. Introduction

### What is a LRSP?

The Local Roadway Safety Plan (LRSP) is a localized data-driven traffic safety plan that provides opportunities to address unique highway safety needs and reduce the number of fatal and severe injury collisions. The LRSP creates a framework to systematically identify and analyze traffic safety-related issues, and recommend safety projects and countermeasures. The LRSP facilitates the development of local agency partnerships and collaboration, resulting in the development of a prioritized list of improvements that can qualify for Highway Safety Improvement Program (HSIP) funding.

The LRSP is a proactive approach to addressing safety needs and is viewed as a living document that can be constantly reviewed and revised to reflect evolving trends, and community needs and priorities.

### Vision and Goals of the LRSP

- Goal #1: Systematically identify and analyze roadway safety problems and recommend improvements
- Goal #2: Improve the safety of all road users by using proven effective countermeasures
- Goal #3: Ensure coordination and response of key stakeholders to implement roadway safety improvements within Fort Bragg
- Goal #4: Serve as a resource for staff who continually seek funding for safety improvements
- Goal #5: Recommend how safety improvements can be made in a manner that is fair and equitable for all Fort Bragg residents

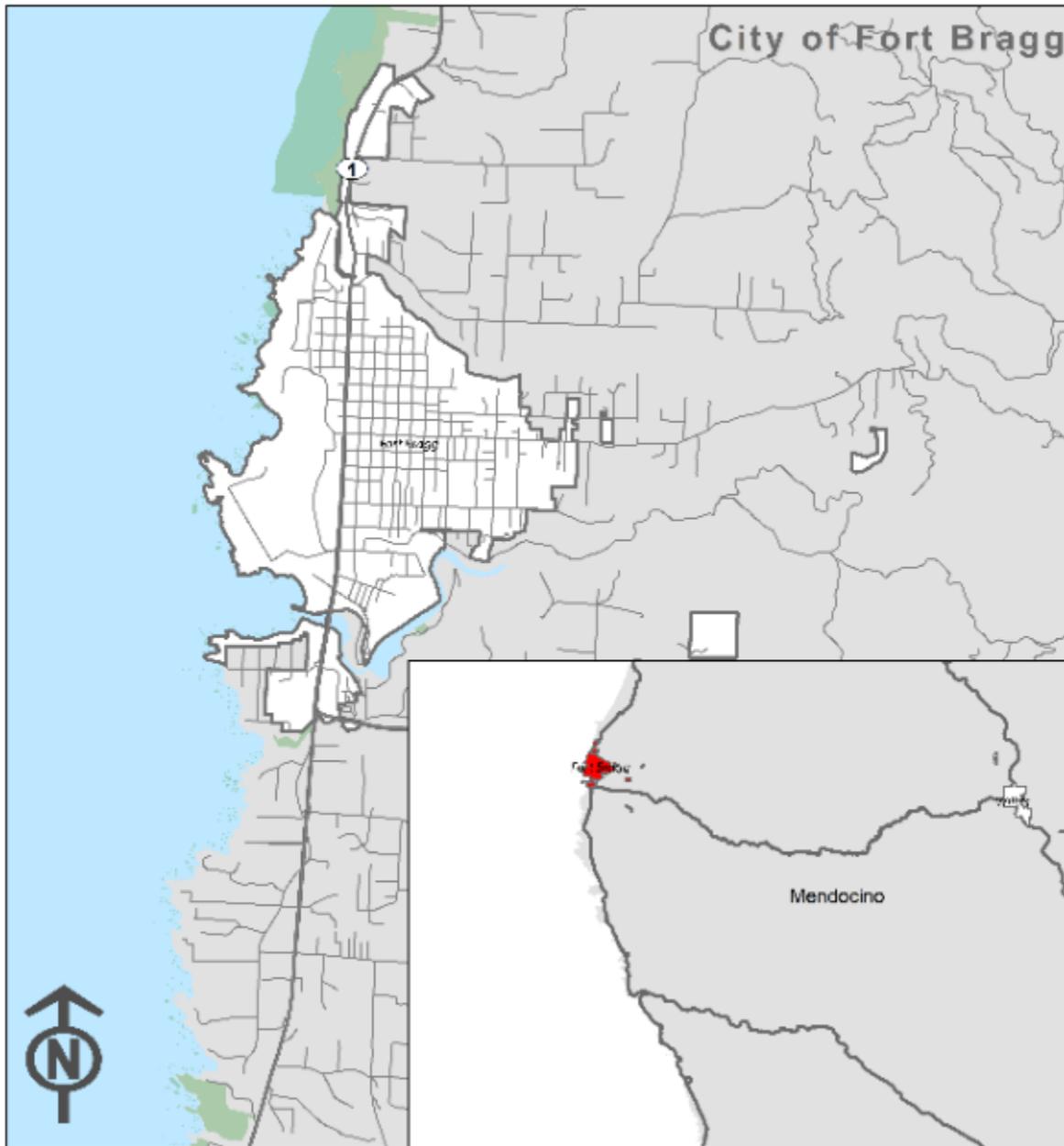
### Study Area

The City of Fort Bragg is located in Mendocino County, California, covering a total area of about 2.931 square miles. It is located on the coast, 24 miles west of the City of Willits at an elevation of 85 feet.

The City's estimated population is 7,302 (ACS 2019 1-year estimate). The City is accessible via CA-1 corridor. **Figure 1** shows the study area.

**City of Fort Bragg**  
**Local Roadway Safety Plan**

Figure 1. Study Area: City of Fort Bragg



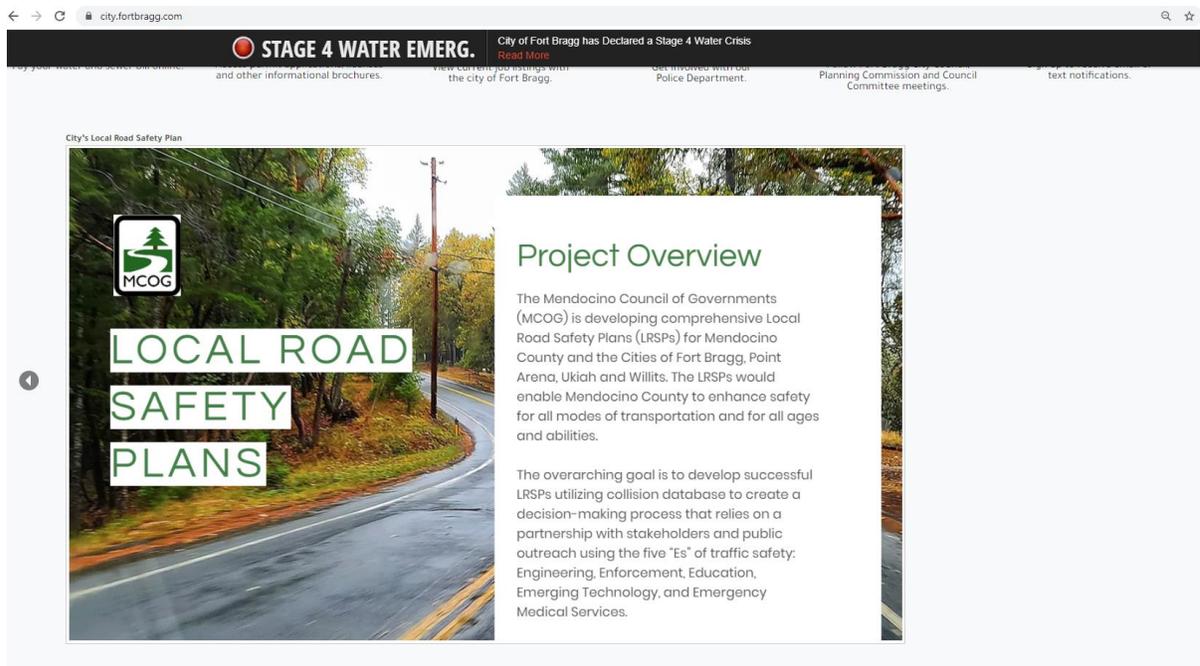
# City of Fort Bragg Local Roadway Safety Plan

## Safety Partners

Safety partners are vital to the development and implementation of an LRSP. For the City of Fort Bragg, these include representatives from Public Works, Fire Department, School District, Mendocino Coast Hospital Ambulance Service, Community Development Department, Police Department, County Supervisor, Walk and Bike Mendocino, CHP, Cal Fire, Mendocino Transit Authority, Mendocino County Sheriff and Caltrans District 1. Two stakeholder meetings among these departments/agencies were conducted to review project goals and findings, and to solicit feedback from the group during the project timeline.

This stakeholder outreach was supplemented by a project website ([mendocinosaferoads.com](http://mendocinosaferoads.com)), with an interactive map input platform. Project related info was also published on the City's website. As part of the Mendocino County Local Road Safety Plan, a public input platform called mapptionaire was published online and advertised on social media to solicit input public comments regarding traffic safety. The mapptionaire tool was open for public comments starting March 5th, 2021 and closed on September 31, 2021. During this period 324 comments were submitted, out of which 21 comments were for the City of Fort Bragg.

Figure 2. City's website and social media posting



**City of Fort Bragg**  
**Local Roadway Safety Plan**

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The City of Fort Bragg with the Mendocino Council of Governments (MCOG) is developing a citywide Local Road Safety Plan geared towards improving traffic safety for pedestrians, bicyclists, transit users and drivers. To learn more about the project, please visit the project website.

Participate in a virtual workshop to tell us about your traffic-related safety concerns on the city streets!

Visit <https://www.mendocinosaferoads.com/> for more!

La Ciudad de Fort Bragg con el Consejo de Gobiernos de Mendocino (MCOG) está desarrollando un Plan de Seguridad Vial Local para toda la ciudad orientado a mejorar la seguridad del tráfico para peatones, ciclistas, usuarios de tránsito y conductores. Para obtener más información sobre el proyecto, visite el sitio web del proyecto.

¡Participe en un taller virtual para contarnos sus preocupaciones de seguridad relacionadas con el tráfico en las calles de la ciudad!

Visite la pagina <https://www.mendocinosaferoads.com/>



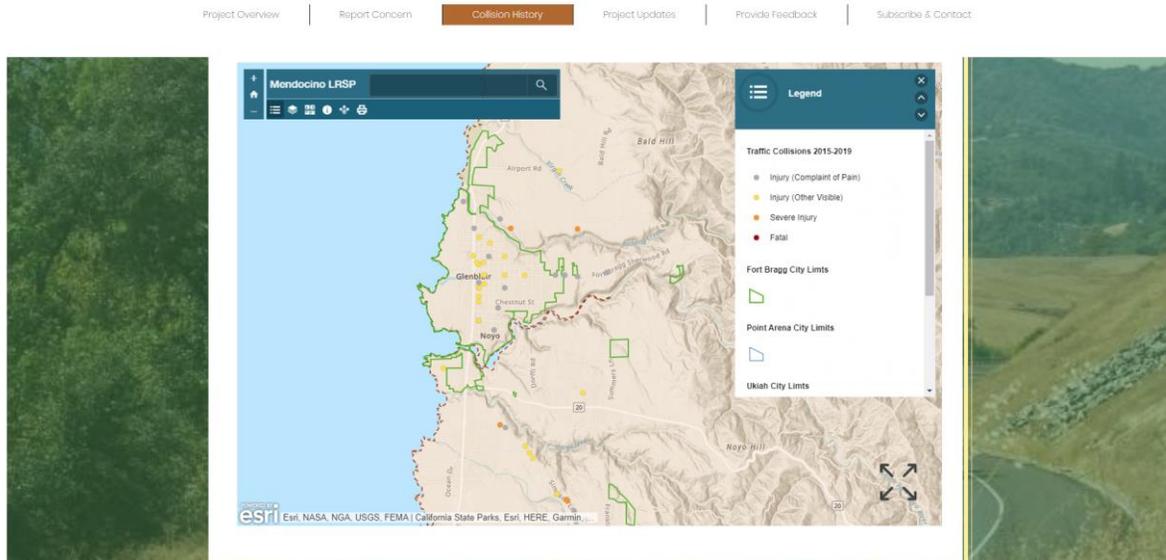
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**Mendocino Safe Roads**

The Mendocino Council of Governments (MCOG) is developing co...

## City of Fort Bragg Local Roadway Safety Plan

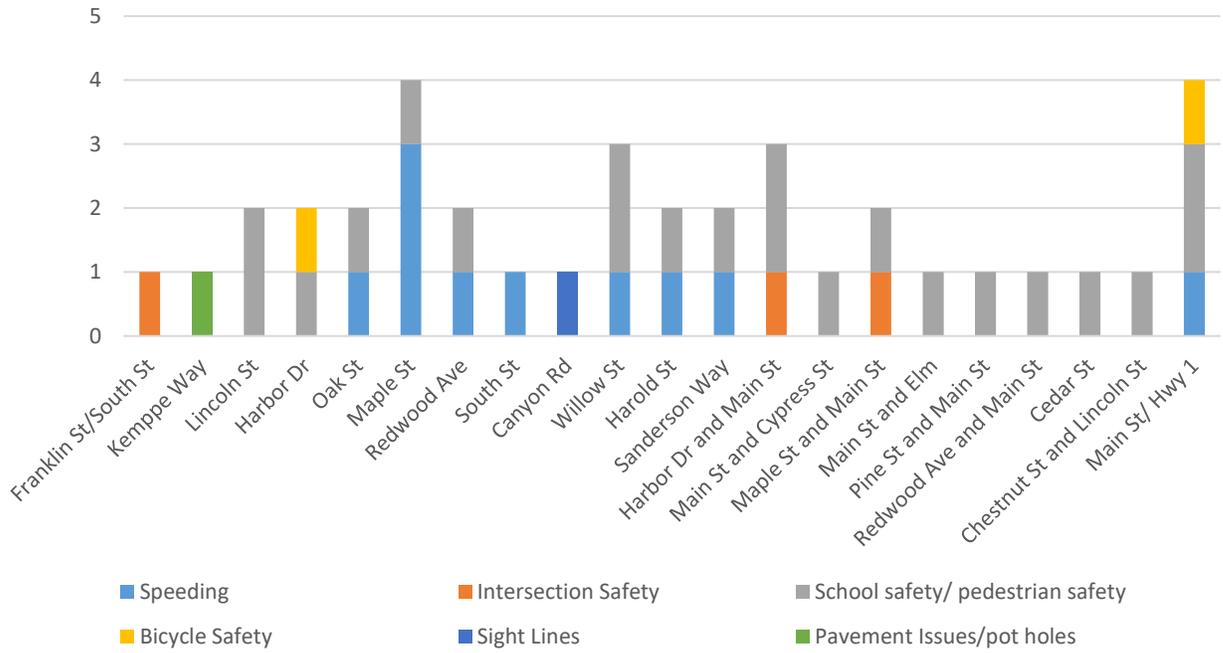
Figure 3. Project Website: [www.mendocinosaferoads.com](http://www.mendocinosaferoads.com)



The most common commented on traffic safety issue pedestrian/school safety, with 14 comments. Main Street/Hwy 1 was the most commented on street with pedestrian safety issues, followed by Willow Street and Lincoln Street. Maple Street was referred to the most as a street with speeding issues, with 3 comments. Other streets were South Street, Redwood Avenue, Willow Street, Oak Street, Harold Street and Sanderson Street.

# City of Fort Bragg Local Roadway Safety Plan

Figure 4. City of Fort Bragg - Public Comments



## 2. Existing Planning Efforts

This chapter summarizes the planning documents, projects underway, and studies reviewed for the City of Fort Bragg Local Road Safety Plan (LRSP) being developed as a part the Mendocino Council of Governments’ LRSPs for local agencies. The purpose of this review is to ensure the LRSP vision, goals, and E’s strategies are aligned with prior planning efforts, planned transportation projects and non-infrastructure programs. The documents reviewed are listed below:

- City of Fort Bragg Coastal General Plan (2008)
- Inland General Plan (2012)
- Fort Bragg Bicycle Master Plan (2009)
- 2018 Street Safety Plan
- South Main Street Access and Beautification Plan (2011)
- City of Trails: Trails Feasibility Study (2016)
- City of Trails: Supplemental Trail Feasibility Studies (2017)
- City of Fort Bragg FY 2020-2021 Budget
- Mill Site Specific Plan (2012)
- Mendocino County Regional Active Transportation Plan (2017)
- Mendocino County Safe Routes to School Plan (2014)
- Mendocino County Pedestrian Facility Needs Inventory and Engineered Feasibility Study (2019)
- Mendocino Council of Governments 2020 Regional Transportation Improvement Program (2019)

The following sections include brief descriptions of these documents and how they inform the development of the LRSP. A summary of each document is provided in **Table 1**. A more detailed list of relevant policies and programs is provided in **Appendix A**.

**Table 1 Document Review Summary**

Document	Highlights
<b>City of Fort Bragg Coastal General Plan (2008)</b>	Circulation element of the coastal General Plan details long range plans for the City of Fort Bragg including bicycle, pedestrian, vehicle and transit policies.
<b>Inland General Plan (2012)</b>	This general plan regulates land use for inland properties that are within City limits but not in the Coastal Zone.
<b>Fort Bragg Bicycle Master Plan (2009)</b>	This plan establishes goals and policies, analyzes existing conditions, proposes recommended standards and identifies potential projects for guiding the improvement of the City’s bicycle facilities.

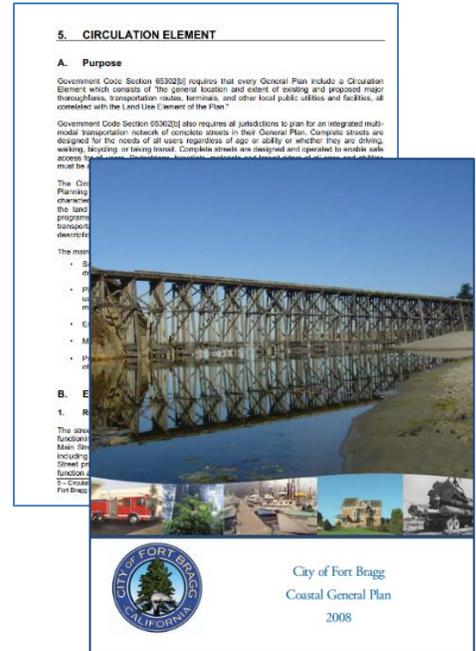
**City of Fort Bragg**  
**Local Roadway Safety Plan**

Document	Highlights
<b>2018 Street Safety Plan</b>	This plan recommends infrastructure improvements that will enhance the safety of pedestrians, bicyclists and motorists on residential neighborhoods and commercial streets in Fort Bragg.
<b>South Main Street Access and Beautification Plan (2011)</b>	This project enhances pedestrian crossings of Highway 1, with curb extensions, high visibility striping, stop bars, pedestrian signage and strategically placed median refuge islands. It also improves safety by reducing vehicle speeds, as well as beautifies the streetscape with trees and landscape strips.
<b>City of Trails: Trails Feasibility Study (2016)</b>	This City of Trails Feasibility Study evaluates three potential new priority trails which could be developed to expand the existing trail network in Fort Bragg.
<b>City of Trails: Supplemental Trail Feasibility Studies (2017)</b>	The City of Trails supplement evaluates engineering and geotechnical challenges associated with implementation of two segments of costal trails.
<b>City of Fort Bragg FY 2020-2021 Budget</b>	The City of Fort Bragg’s fiscal year 2020 – 2021 Budget outlines the funds the city has allocated to various departments and project include street and road maintenance and improvements.
<b>Mill Site Specific Plan (2012)</b>	The Mill Site Specific Plan is a community-based vision for the redevelopment of the old mill site in Fort Bragg.
<b>Mendocino County Regional Active Transportation Plan (2017)</b>	Details bicycle and pedestrian improvements on County significant corridors. Includes many detailed priority bike and pedestrian projects.
<b>Mendocino County Safe Routes to School Plan (2014)</b>	Safe Routes to School (SRTS) is a program with a simple goal: helping more children get to school by walking and bicycling including the schools Westport Village Community School and Three Rivers Learning Center.
<b>Mendocino County Pedestrian Facility Needs Inventory and Engineered Feasibility Study (2019)</b>	The project’s goal is to improve sidewalks, paths, and safe crossings in Mendocino County so it’s easier to walk where you need to travel.
<b>Mendocino Council of Governments 2020 Regional Transportation Improvement Program (2019)</b>	The Regional Transportation Improvement Program (RTIP) is a program of highway, local road, transit and active transportation projects that a region plans to fund with State and Federal revenue.

# City of Fort Bragg Local Roadway Safety Plan

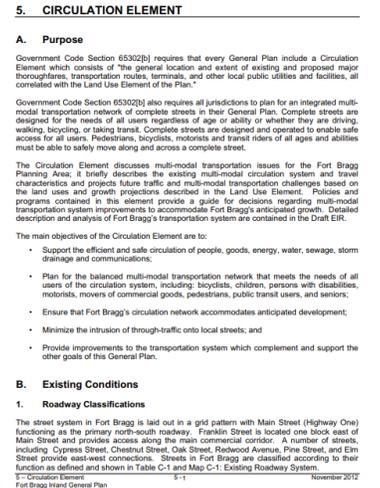
## City of Fort Bragg Coastal General Plan (2008)

The General Plan presents a consolidated framework of decisions for guiding where and how development should occur in Fort Bragg. The Coastal General Plan applied to all projects in the Coastal Zone. Circulation Element discusses transportation issues for the Fort Bragg Planning Area; it briefly describes the existing circulation system and travel characteristics and projects future traffic based on the land uses and growth projections described in the Land Use Element. The Circulation Element ensure that Fort Bragg's circulation network is sufficient to accommodate anticipated development.



## Inland General Plan (2012)

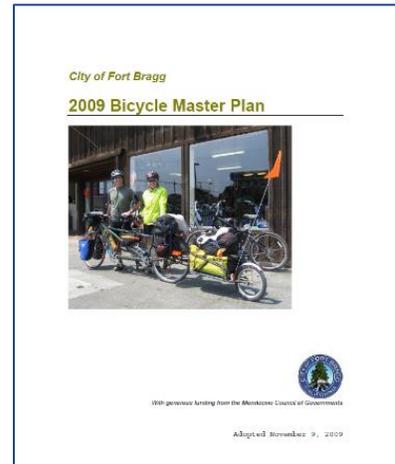
This General Plan regulates land use for inland properties that are in city limits, but not in the Coastal Zone. The policy framework of the Inland General Plan has a long range perspective and is intended to address development concerns for the next ten years (2022). The Circulation Element contains policies for public transit, bicycle facilities, parking and transportation for the mobility impaired, taking into account the relationship between land use and transportation needs of the community.



## City of Fort Bragg Local Roadway Safety Plan

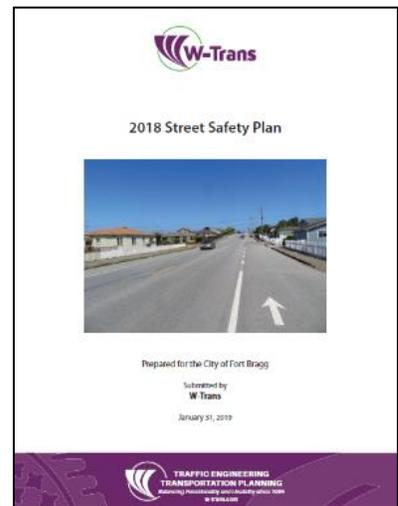
### City of Fort Bragg 2009 Bicycle Master Plan

In 2009, the City adopted a Bicycle Master Plan that incorporated the development of bike paths, bike lanes, and bike routes throughout the City. The City's Bicycle Master Plan builds on the existing Bicycle Circulation Plan. Bicycling is an important transportation option that offers many benefits to the Fort Bragg community. The Bicycle Master Plan was prepared to direct the City's efforts to improve the cycling environment in Fort Bragg.



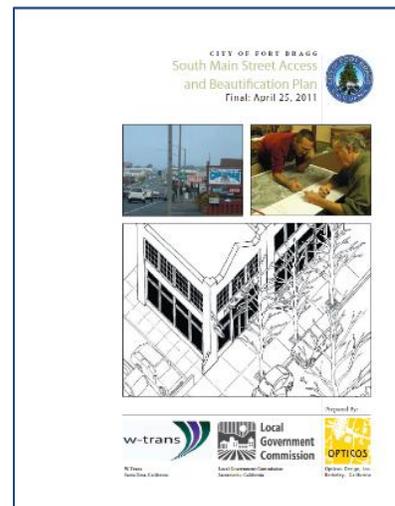
### 2018 Street Safety Plan

The City of Fort Bragg completed a Residential Streets Safety Plan (RSSP) in 2005. The 2018 Street Safety Plan is an expansion and update of the 2011 Residential Streets Safety Plan that also address commercial street safety. The roadways evaluated in this study are Maple Street, Elm Street, Pine Street, Main Street (SR 1), Fir Street and Harold Street. This plan will provide guidance on countermeasures selected for the LRSP.



### South Main Street Access and Beautification Plan (2011)

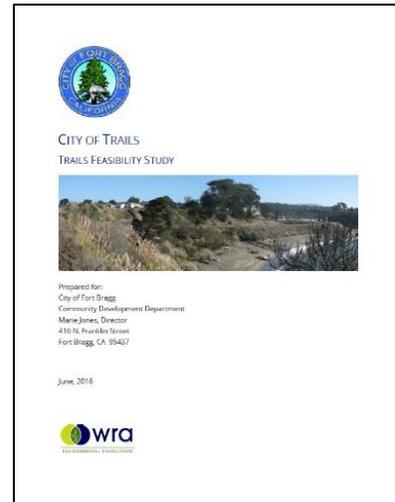
This plan's primary focus is to improve safety, mobility, and access between central Fort Bragg and its southern business, recreational, and residential areas and to improve the aesthetic qualities of the South Main corridor through design recommendations that positively impact the overall urban design of the project area.



## City of Fort Bragg Local Roadway Safety Plan

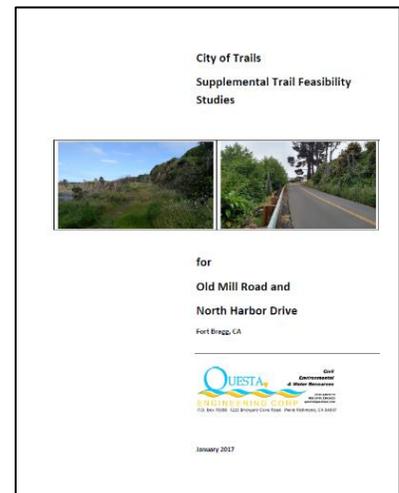
### City of Trails: Trails Feasibility Study (2016)

The City of Trails Feasibility Study evaluates three potential new priority trails which could be developed to expand the existing trail network in Fort Bragg. The purpose of the City of Trails Feasibility Study is to identify trail opportunities that are beneficial and of interest to the community; provide detailed feasibility and development cost information for the selected priority trails; and identify permitting requirements. Trails benefit communities by providing healthy opportunities to walk and ride to daily destinations and recreational activities. The availability of trails can, over time, reduce a community's dependence on cars, total vehicle miles traveled (VMTs) and greenhouse gas emissions. This plan will provide guidance on countermeasures selected for the LRSP.



### City of Trails: Supplemental Trail Feasibility Studies (2017)

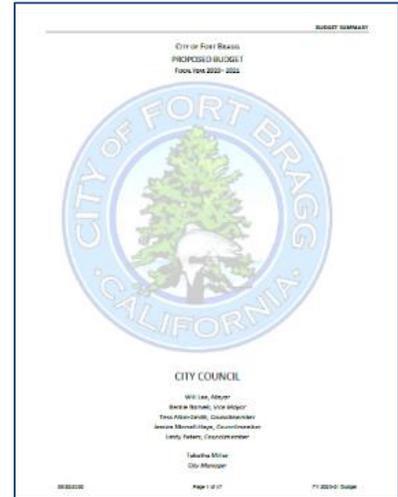
The City of Trails Supplemental Trail Feasibility Study evaluates engineering and geotechnical challenges associated with implementation of two segments of a coastal trail to connect the existing Coastal Trail-South Segment, with Noyo Harbor. This document builds on the City of Trails Feasibility Study, in which the Old Mill Road Multi-use Trail to North Noyo Harbor is described as a priority trail, in addition, this Study also addresses the feasibility of placing either a Class I or II bicycle trail parallel to North Harbor Drive. This plan will provide guidance on countermeasures selected for the LRSP.



## City of Fort Bragg Local Roadway Safety Plan

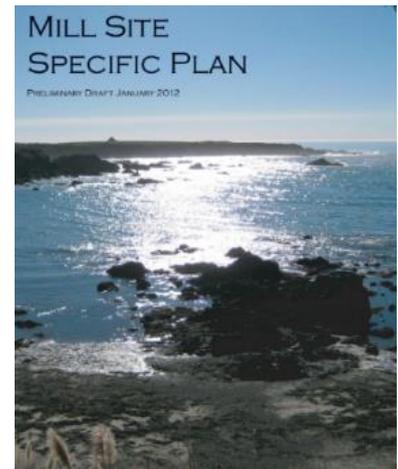
### City of Fort Bragg FY 2020-2021 Budget

The City of Fort Bragg’s fiscal year 2020 – 2021 Budget outlines the funds the city has allocated to various departments and project include street and road maintenance and improvements. Street maintenance, along with traffic and safety improvement cost along with their funding sources have been listed under the FY 2021-2025 Capital Improvement Program.



### Mill Site Specific Plan (2012)

The Mill Site Specific Plan is the result of a community-based vision for the redevelopment of the old mill site in Fort Bragg that defines the framework for future redevelopment. The Mill Site Specific Plan Study Area includes the Plan Area and the adjacent 82-acre coastal trail and parkland area to the west. The central elements to the Specific Plan’s central vision are the coastline, walkability, public spaces, a central business district extension, opens space and habitat restoration.



### Mendocino County Regional Active Transportation Plan (2017)

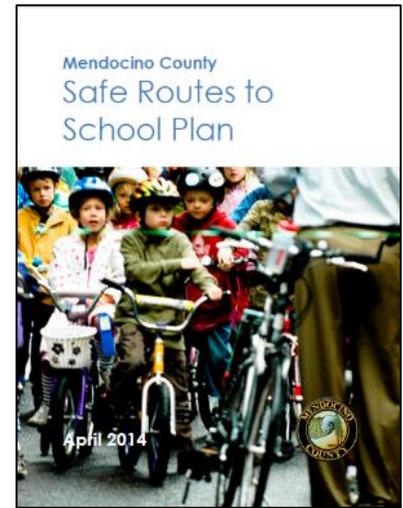
This Plan is intended to identify priority bicycle and pedestrian improvements within all jurisdictions of Mendocino County, which include the Cities of Ukiah, Willits, Fort Bragg and Point Arena and the unincorporated areas of the County of Mendocino.



**City of Fort Bragg**  
**Local Roadway Safety Plan**

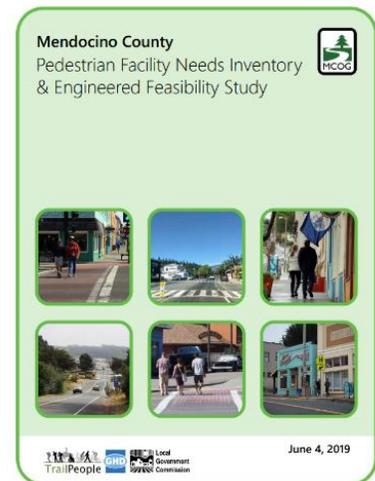
**Mendocino County Safe Routes to School Plan (2014)**

Safe Routes to School (SRTS) is a program with a simple goal: helping more children get to school by walking and bicycling. The plan envisions active kids using safe streets, helped by engaged adults (from teachers to parents, engineers, planners and police officers), surrounded by responsible drivers. The plan is the first area-wide Safe Routes to School plan in Mendocino County, designed to serve schools in the unincorporated areas of the county. The plan includes recommendations for a Safe Routes to School program that will strive to enhance children's health and well-being, ease traffic congestion near the school to improve safety, increase the number of students getting regular physical activity and improve air quality around schools.



**Mendocino County Pedestrian Facility Needs Inventory and Engineered Feasibility Study (2019)**

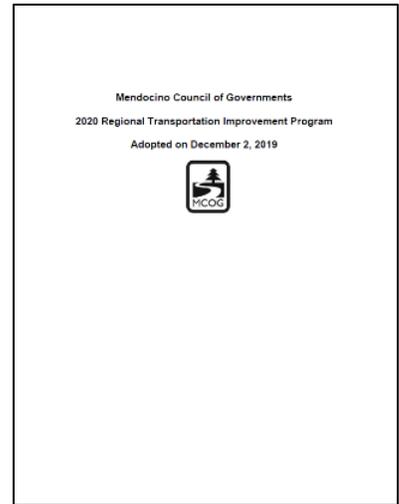
The Mendocino County Pedestrian Facility Needs Inventory and Engineered Feasibility Study has a simple goal: to improve sidewalks, paths, and safe crossings in Mendocino County so it's easier to walk where you need to. This study covers all of Mendocino County; a vast amount of territory and many communities from large to tiny. This report describes all the potential pedestrian access improvement projects identified through the review of past studies, the inventory and analysis of existing conditions for pedestrian access, agency staff input, and the public input from workshops, meetings and on-line surveys



**City of Fort Bragg**  
**Local Roadway Safety Plan**

**Mendocino Council of Governments 2020 Regional  
Transportation Improvement Program (2019)**

The Regional Transportation Improvement Program (RTIP) is a program of highway, local road, transit and active transportation projects that a region plans to fund with State and Federal revenue programmed by the California Transportation Commission in the State Transportation Improvement Program (STIP).



### **3. Collision Data Collection and Analysis**

This chapter summarizes the results of a citywide collision analysis for collisions that have occurred in the City of Fort Bragg between January 2015 and December 2019. A five-year city-wide collision data set was retrieved from Transportation Injury Mapping System (TIMS) and Statewide Integrated Traffic Records System (SWITRS).

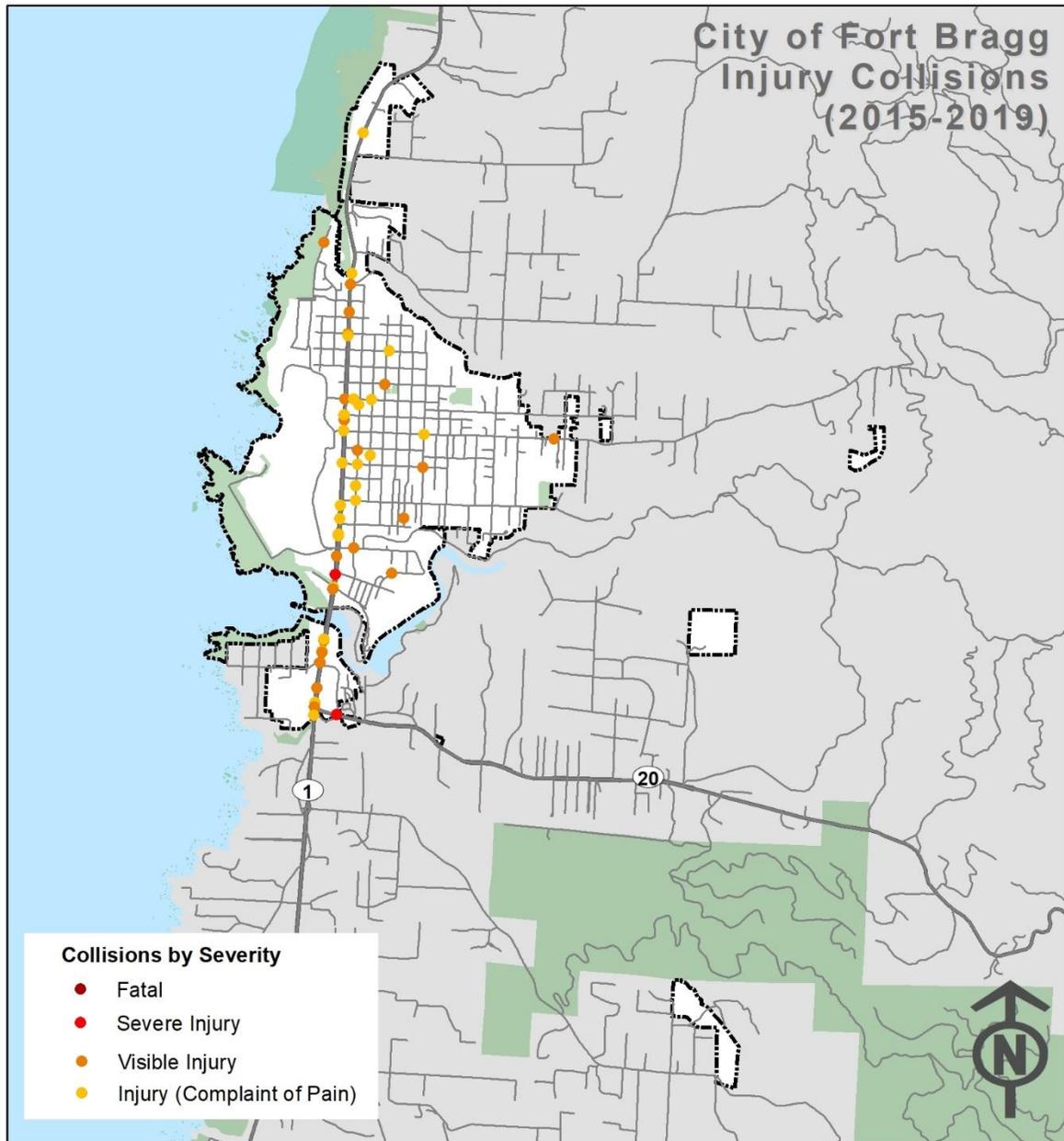
The LRSP focuses on systemically identifying and analyzing traffic safety issues to recommend appropriate safety strategies and improvements. This chapter starts with an analysis of citywide collisions of all severity, including Property Damage Only (PDO) collisions, retrieved from Transportation Injury Mapping System (TIMS) and Statewide Integrated Traffic Records System (SWITRS). Further on, a detailed analysis was conducted for high-injury collisions, including fatal and severe injury (F+SI) collisions that have occurred on Fort Bragg's roadways. After this data was separated, a comprehensive evaluation was conducted based on factors such as collision severity, type of collision, primary collision factor, lighting, weather and time of the day. The following is a brief overview of the sections:

- Demographic and Jurisdiction Information
- Data Collection
- Collision Data Analysis
- Fatal and Severe Injury Collision Analysis
- Geographic Collision Analysis
- High Injury Network
- Summary

Error! Reference source not found. illustrates all the injury collisions that have occurred in Fort Bragg from January 2015 to December 2019.

**City of Fort Bragg**  
**Local Roadway Safety Plan**

Figure 5. All Injury Collisions on Fort Bragg Roadways (2015 – 2019)



**City of Fort Bragg**  
**Local Roadway Safety Plan**

**Demographic and Jurisdiction Characteristics**

This section provides an understanding of the demographics of the City of Fort Bragg and Mendocino County, including characteristics like the population, centerline miles of roadway and commute to work. The data was collected from the United States Census Bureau<sup>1</sup>.

**Population**

According to the 2015 - 2019 American Community Service (ACS) 5-year Estimate data, the population of Fort Bragg is 7,302, which is 8.4 percent of the county population. The population as well as the centerline miles are listed in **Table 2**.

**Table 2. Fort Bragg and Mendocino Population and Centerline Miles**

	Population	Percent of County Population	Centerline Miles	Percent of County Centerline Miles
<b>Point Arena</b>	421	0.5%	2.3	0.2%
<b>Willits</b>	4,893	5.6%	20.5	1.8%
<b>Fort Bragg</b>	7,302	8.4%	28.1	2.5%
<b>Ukiah</b>	15,943	18.4%	58.9	5.3%
<b>Unincorporated</b>	58,190	67.1%	1,009.9	90.2%
<b>Total</b>	86,749		1,119.7	

**Commute to Work**

In the City of Fort Bragg, approximately 78 percent of residents travel by cars or vans to work, out of which 64 percent drive alone and 14 percent carpool. About 14 percent of residents walk to work and 1 percent of residents take transit. The different modes of transportation used to commute to work for the City are shown in **Table 3**.

**Table 3. City of Fort Bragg Commute to Work Census Data**

Commute to Work	Fort Bragg	Commute to Work	Fort Bragg
<b>Drive alone</b>	64%	Bicycle	0%
<b>Carpool</b>	14%	Work from Home	8%
<b>Public Transportation</b>	1%	Other	0%
<b>Walked</b>	14%		

**Jurisdiction Rankings**

Between 2015 and 2019, Mendocino County had 112 fatal traffic collisions, with 2 occurring in Fort Bragg, with a traffic fatality rate per 100,000 population of 25.82 for the County as a whole, and 5.28 for Fort Bragg. These rates are less than the California average and the country

<sup>1</sup> United States Census Bureau. (2021). 2015-2019 American Community Service ACS 5-year Estimate <https://data.census.gov>

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average with 8.95 and 10.28, respectively. **Table 4** shows the comparison of traffic fatality rates and population.

**Table 4. Jurisdiction Ranking**

Jurisdiction	Fatal Traffic Collisions (2015-2019)	Population	5 year Fatality Rate per 100,000
<b>Fort Bragg</b>	2	7,302	5.48
<b>Mendocino County</b>	112*	86,749	25.82
<b>California</b>	17,684	39,512,223	8.95
<b>United States</b>	168,742	328,239,523	10.28
<b>*Note: These numbers include all state route collisions fatalities</b>			
<b>Source: TIMS, Census, NHTSA</b>			

### Office of Traffic Safety Rankings

Additional information on collisions in the City of Fort Bragg is provided by the California Office of Traffic Safety (OTS). OTS is designated by the Governor to receive federal traffic safety funds for coordinating California's highway safety programs. OTS rankings from 2018, the latest available year, indicate that the City of Fort Bragg ranks in the top, meaning higher collisions rates in alcohol involved collisions (3 out of 75 similarly sized cities), pedestrian collisions (15 out of 75 similarly sized cities) and speed related collisions (16 out of 75 similarly sized cities). These rankings take into account fatal and injury crashes per population and per vehicle miles traveled (VMT). Overall Fort Bragg ranks 46 out 102 similarly sized cities in California in fatal and injury collisions. **Table 5** provides a summary of the 2018 rankings<sup>2</sup>.

**Table 5. Office of Traffic Safety Ratings 2018**

OTS 2018 Ranking	Fort Bragg	OTS 2018 Ranking	Fort Bragg	OTS 2018 Ranking	Fort Bragg
<b>Total Fatality and Injury</b>	13/75	Pedestrian	15/75	Speed Related	16/75
<b>Alcohol Involved</b>	3/75	Bicycle	36/75	Nighttime	28/75

<sup>2</sup> California Office of Traffic Safety. (2018). Office of Traffic Safety Rankings 2018. [https://www.ots.ca.gov/media-and-research/crash-rankings-results/?wpv-wpcf-year=2018&wpv-wpcf-city\\_county=Fort+Bragg&wpv\\_filter\\_submit=Submit](https://www.ots.ca.gov/media-and-research/crash-rankings-results/?wpv-wpcf-year=2018&wpv-wpcf-city_county=Fort+Bragg&wpv_filter_submit=Submit)

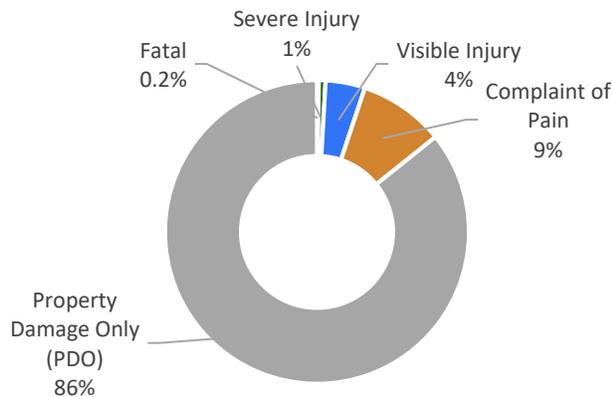
## Collision Data Collection

Collision data helps understand different factors that might be influencing collision patterns and various factors leading to collisions in a given area. For the purpose of this analysis, a five-year jurisdiction-wide collision data, from 2015 to 2019 was retrieved from Transportation Injury Mapping System (TIMS) and Statewide Integrated Traffic Records System (SWITRS)<sup>3</sup>. State route roadways in Fort Bragg were included in this analysis. The collision data was analyzed and plotted in ArcMap to identify high-risk intersections and roadways segments.

## Collision Data Analysis

There were a total of 548 collisions reported City-wide from 2015 to 2019. Out of these 470 collisions (86 percent) were property damage only (PDO) collisions, 50 collisions (9 percent) led to complaint of pain injury and 23 collisions (4 percent) led to a visible injury. There were 5 F+SI (fatal and severe injury) collisions, 4 collisions (1 percent) led to a severe injury and 1 collisions led to a fatality. **Figure 6** illustrates the classification of all collisions based on severity.

**Figure 6. Collisions by Severity (2015-2019)**



The analysis first includes a comparative evaluation between all collisions and F+SI collisions, based on various factors including but not limited to the collision trend, primary collision factor, collision type,

facility type, motor vehicle involved with, weather, lighting, and time of the day. Further on, a comprehensive analysis is conducted for only F+SI collisions. F+SI collisions cause the most damage to those affected, infrastructure and the aftermath of these collisions lead to great expenses for jurisdiction administration. The LRSP process thus focuses on these collision locations to proactively identify and counter their respective safety issues.

<sup>3</sup> California Highway Patrol. (2021). Statewide Integrated Traffic Records System. <https://www.chp.ca.gov/programs-services/services-information/switrs-internet-statewide-integrated-traffic-records-system>

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The collision data was segregated by facility type, i.e. based on collisions occurring on intersections and roadway segments. For the purposes of the analysis, a collision was said to have occurred at an intersection if it occurred within 250 feet of it. The reported collisions categorized by facility type and collision severity are presented in **Table 6**.

**Table 6. Collisions by Severity and Facility Type**

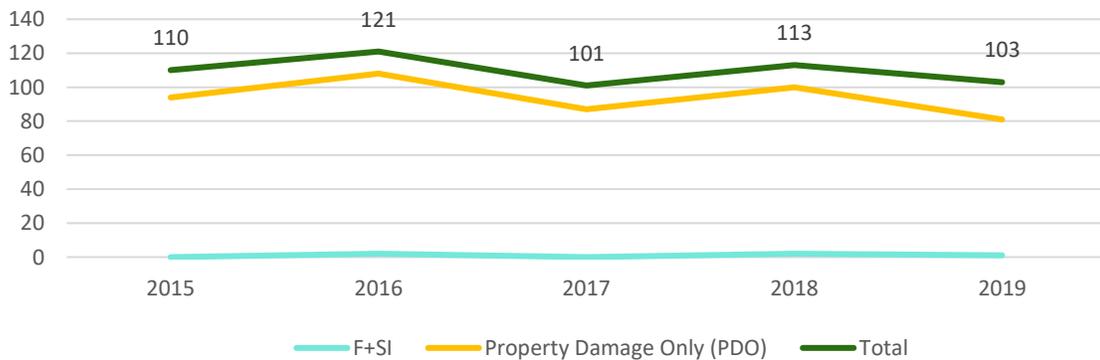
Collision Severity	Roadway Segment	Intersection	Total
<b>Fatal</b>	0	1	1
<b>Severe Injury</b>	0	4	4
<b>Visible Injury</b>	7	16	23
<b>Complaint of Pain</b>	5	45	50
<b>Property Damage Only (PDO)</b>	43	427	470
Total	<b>55</b>	<b>493</b>	<b>548</b>

**Preliminary Analysis**

**Collision Severity by Year**

For all collisions, the number of collisions decreased from 2015 to 2019. The highest number of collisions (121 collisions) were observed in 2016 and the lowest number of collisions (101) were observed in 2017. A total of 5 F+SI collisions occurred in the City of Fort Bragg during the study period. No F+SI collisions occurred in 2015 and 2017. Overall, F+SI collisions were observed to rise from 2017 to 2019, with the highest number of F+SI collisions (2 collisions) occurring in the years 2016 and 2018. **Figure 7** the five-year collision trend for all collisions, F+SI collisions and also PDO collisions.

**Figure 7. Five Year Collision Trend**

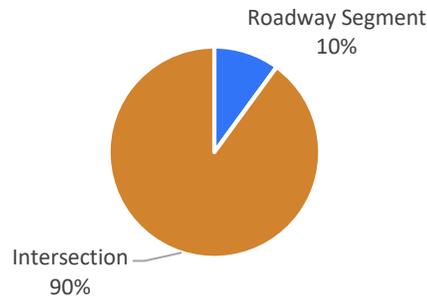


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**Intersection vs. Roadway Collisions**

When evaluating roadways vs intersections, it was observed that the majority of collisions occurred at intersections. In the City of Fort Bragg, 90% of all collisions (493 collisions) occurred at intersections whereas 10% (55 collisions) occurred on roadway segments. This classification by facility type can be observed in **Figure 8**.

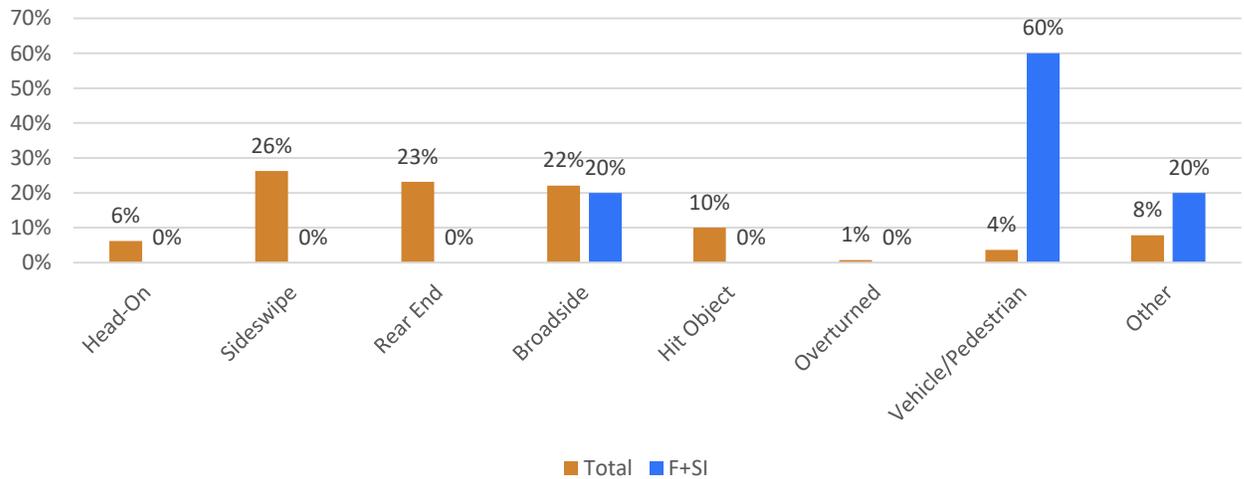
**Figure 8. Intersection vs. Roadway Collisions - All Collisions**



**Collision Type**

Considering collisions of all severity the most commonly occurring collision type was sideswipe collisions (26 percent) and rear end collisions (23 percent). The collision type for F+SI collisions are noticeably different. For F+SI collisions, the most commonly occurring collision type was vehicle/pedestrian collisions (60 percent) and the second most common was broadside collisions (20%). **Figure 9** illustrates the collision type for all collisions as well as F+SI collisions.

**Figure 9. Collision Type – All Collisions vs. F+SI Collisions**



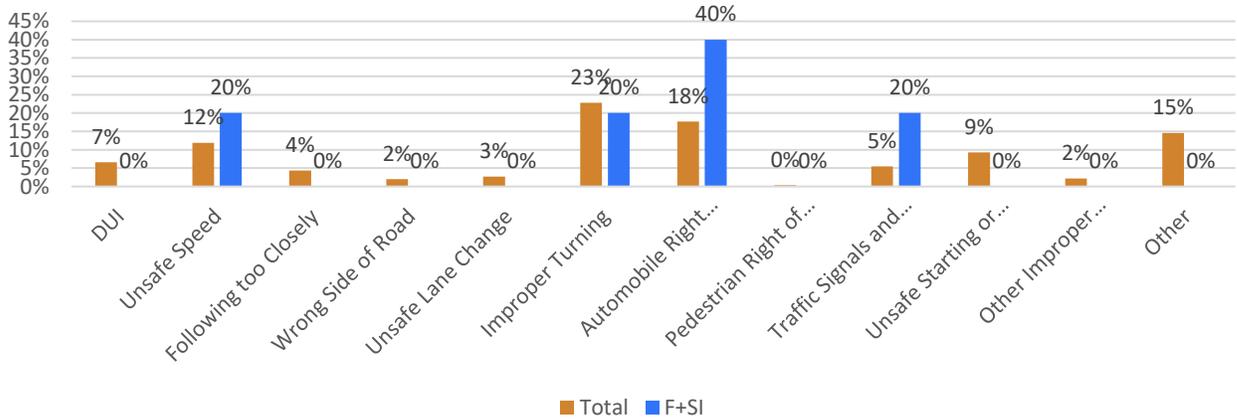
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**Violation Category**

Considering collisions of all severity, the most common violation category was observed to be improper turning (23 percent) and automobile right of way (18 percent). For F+SI collisions, automobile right of way (40 percent) was also observed to be the main violation categories.

**Figure 10** illustrates the violation category for all collisions and F+SI collisions.

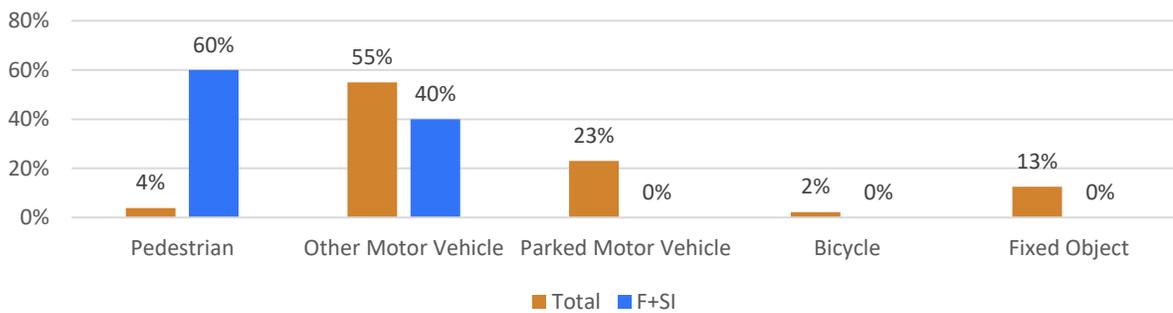
**Figure 10. Violation Category: All Collisions vs. F+SI Collisions**



**Motor Vehicle Involved With**

Considering all collisions, 55 percent of the collisions are motor vehicle involved with another motor vehicle. The remaining collisions include motor vehicle involved with parked vehicles (23 percent) and motor vehicle involved with fixed object (13 percent). The trends for F+SI collisions are noticeably different. For F+SI collisions, 60 percent of the collisions involved a pedestrian and 40 percent involved another vehicle, indicating these collision types are more likely to result in a fatal or severe collision. **Figure 11** illustrates the percentage for all collisions as well as F+SI collisions.

**Figure 11. Motor Vehicle Involved with: All Collisions vs. F+SI Collisions**

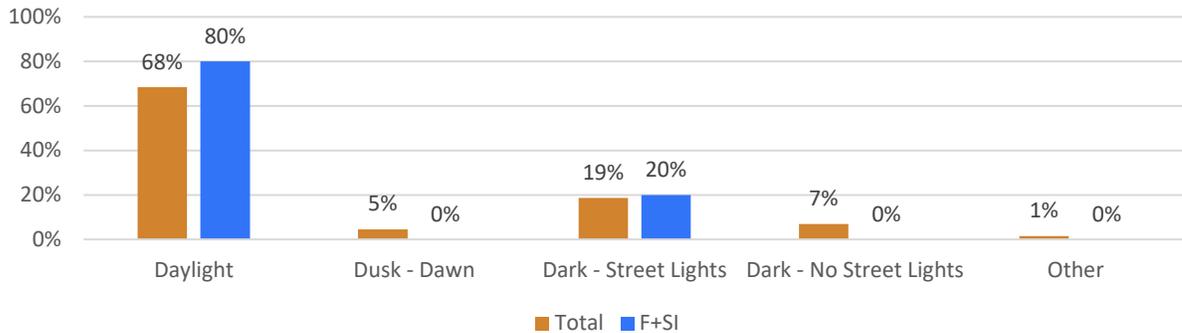


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**Lighting**

For collisions of all severity, 68 percent of collisions have occurred in daylight and 19 percent of collisions have occurred in the dark on streets with street lights. For F+SI collisions, 80 percent of collisions have occurred in daylight and 20 percent of collisions occurred in the dark on streets with street lights. **Figure 12** illustrates the lighting condition for all collisions and F+SI collisions.

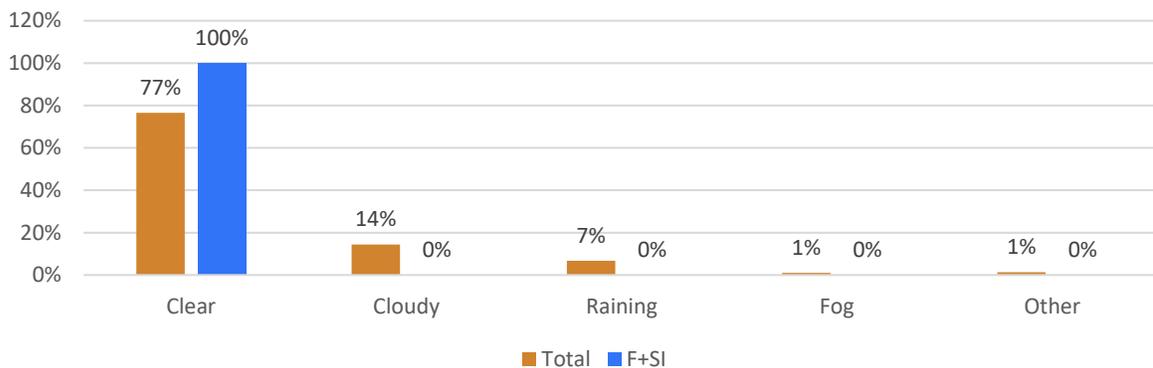
**Figure 12. Lighting Conditions: All Collisions vs. F+SI Collisions**



**Weather**

For all collisions, 77 percent of the collisions have occurred during clear weather conditions and 14 percent collisions have observed to occur during cloudy weather conditions. For F+SI collisions, 100 percent of the collisions have occurred during clear weather conditions. **Figure 13** illustrates the percentage distribution of weather conditions during occurrence of collisions of all severity as well as F+SI collisions.

**Figure 13. Weather Conditions: All Collisions vs. F+SI Collisions**

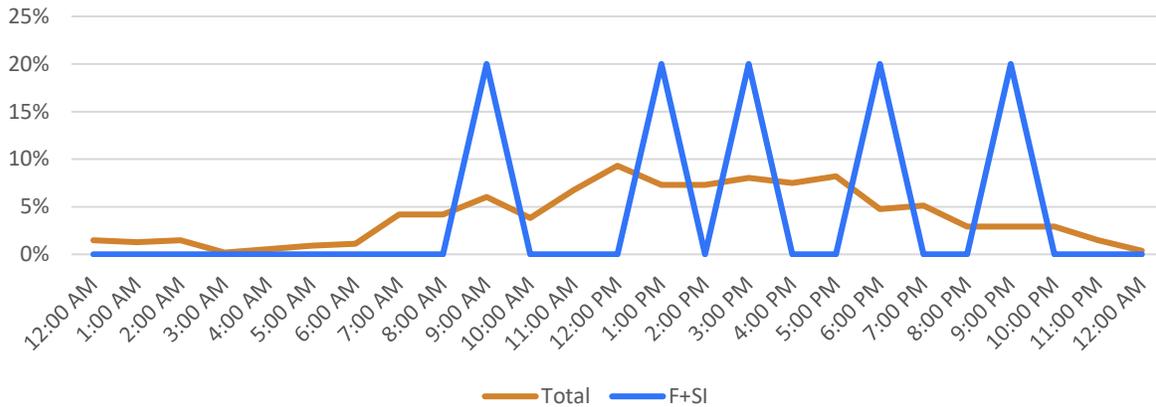


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**Time of the Day**

For collisions of all severity, maximum number of collisions have occurred between 12:00 p.m. to 1:00 p.m. (9 percent) and the minimum number of collisions have occurred between 3:00 a.m. to 6:00 a.m. (0 percent). For all F+SI collisions, the collisions occurred throughout the day. **Figure 14** illustrates the percentage of collisions occurring during the day for all severity collisions as well as F+SI collisions.

**Figure 14. Time of the Day: All Collisions vs. F+SI Collisions**



### **Fatal and Severe Injury Collision Analysis**

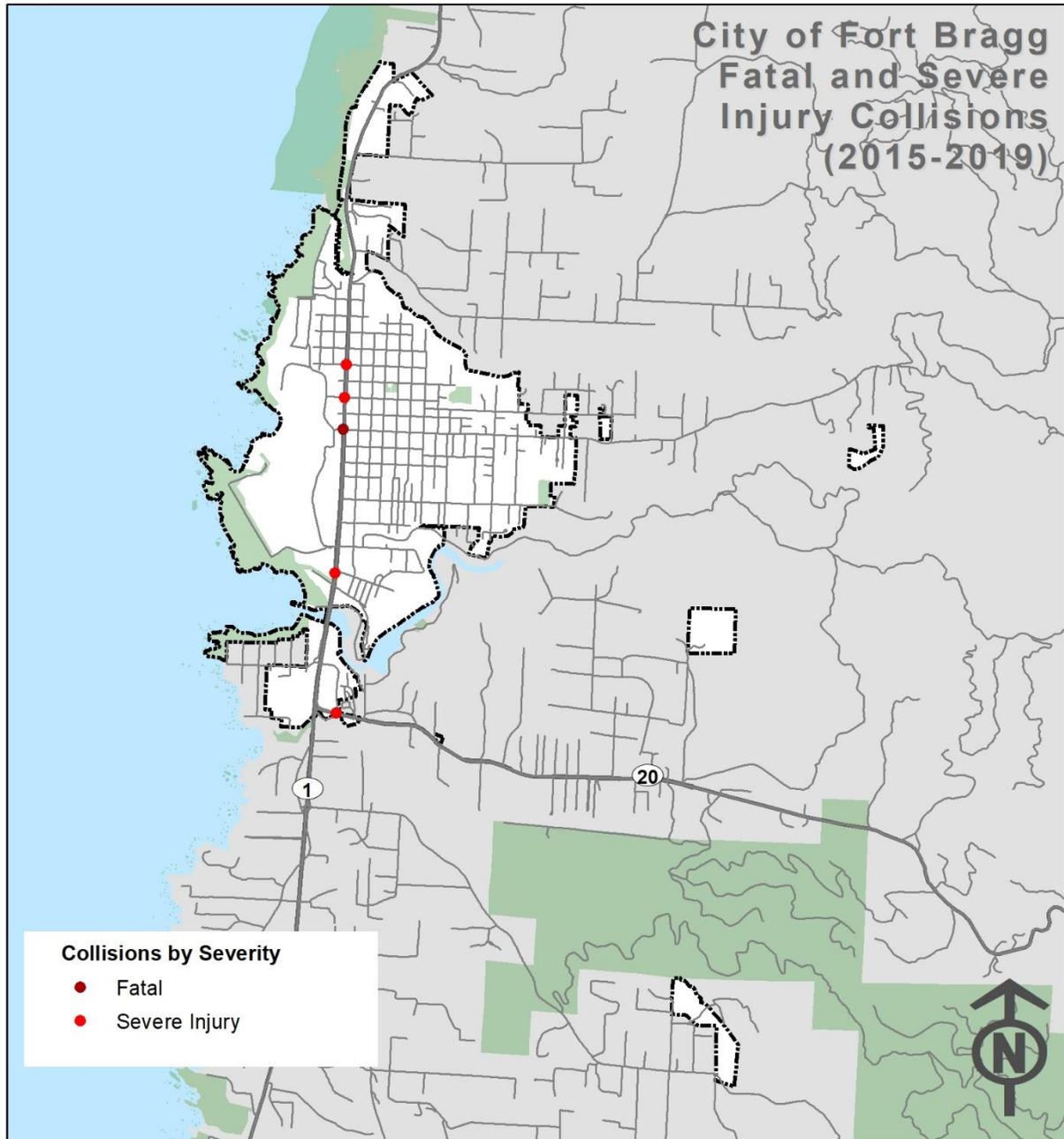
The detailed collision analysis is effective for identifying high-risk locations by evaluating collisions that have led to a fatality or a severe injury. Collisions have been further analyzed taking into account the following collision attributes:

- Location
- Violation Category
- Collision Type vs. Violation Category
- Collision Type vs. Motor Vehicle Involved With
- Motor Vehicle Involved With vs. Violation Category
- Collision Type vs. Lighting Conditions
- Collision Types vs. Time of Day
- Gender vs. Age

**Figure 15** illustrates all the location of the fatal and severe injury collisions that have occurred in the City from 1/1/2015 to 12/31/2019.

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Figure 15. Fort Bragg F+SI Collisions (2015-2019)

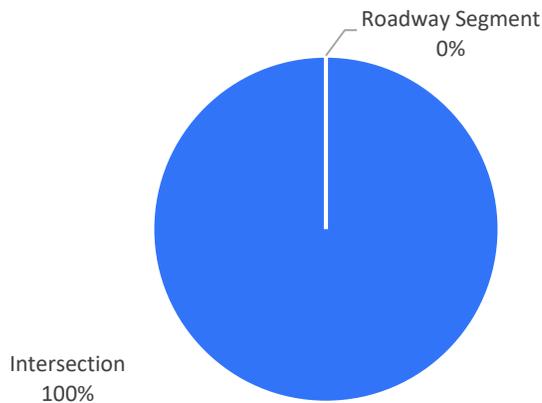


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### Facility Type

Of the total 5 F+SI collisions that occurred in Fort Bragg, 5 collisions (100 percent) occurred at intersections (within 250 feet of an intersection) and none occurred on roadway segment or at mid-block locations. This distribution is illustrated in **Figure 16**.

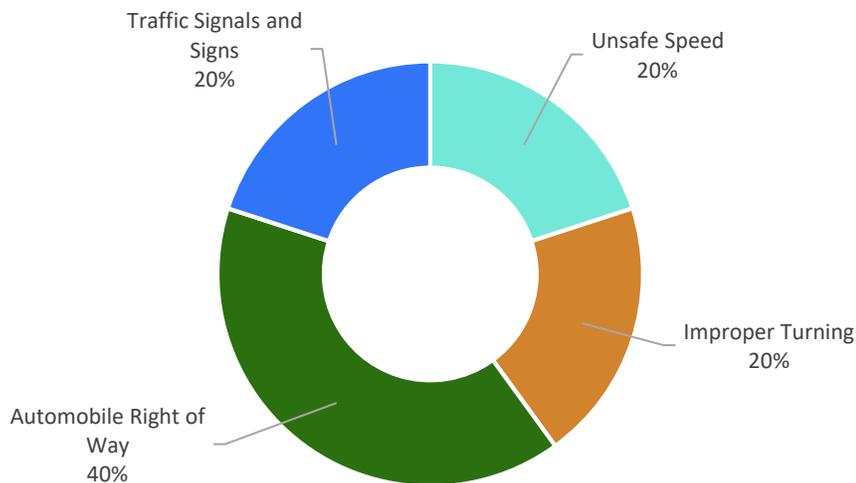
Figure 16. F+SI Collisions: Roadway Segments and Intersections



### Violation Category

For F+SI collisions, automobile right of way (40 percent) was observed to be major violation categories. **Figure 17** illustrates the violation category for F+SI collisions.

Figure 17. F+SI Collisions: Violation Category

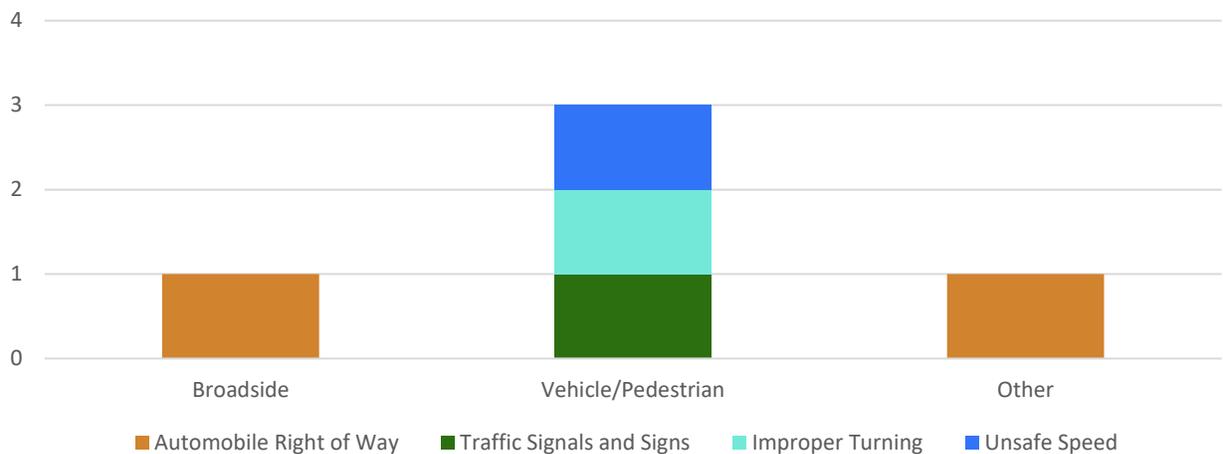


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**Collision Type and Violation Category**

For all collisions that led to a fatality or severe injury, the most common violation types were automobile right of way. **Figure 18** illustrates the type of collision as well as the violation category for F+SI collisions.

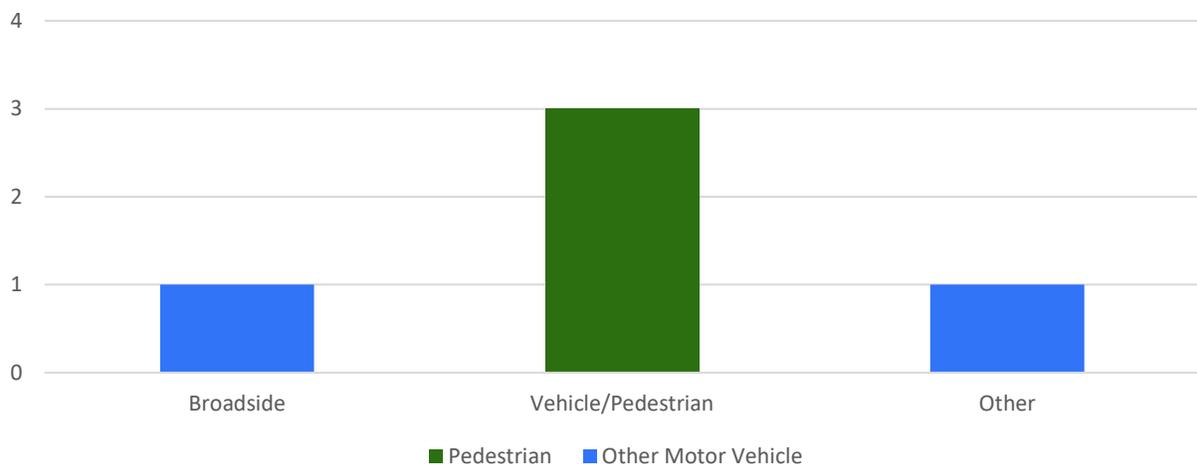
**Figure 18. F+SI Collisions: Collision Type Vs Violation Category (2015-2019)**



**Collision Type and Motor Vehicle Involved With**

For all F+SI collisions, the most common collision types were vehicle/pedestrian collisions and broadside collisions that occurred between two motor vehicles. **Figure 19** illustrates the type of collision as well as the motor vehicle involved with for F+SI collisions.

**Figure 19. F+SI Collisions: Type and Motor Vehicle Involved with**

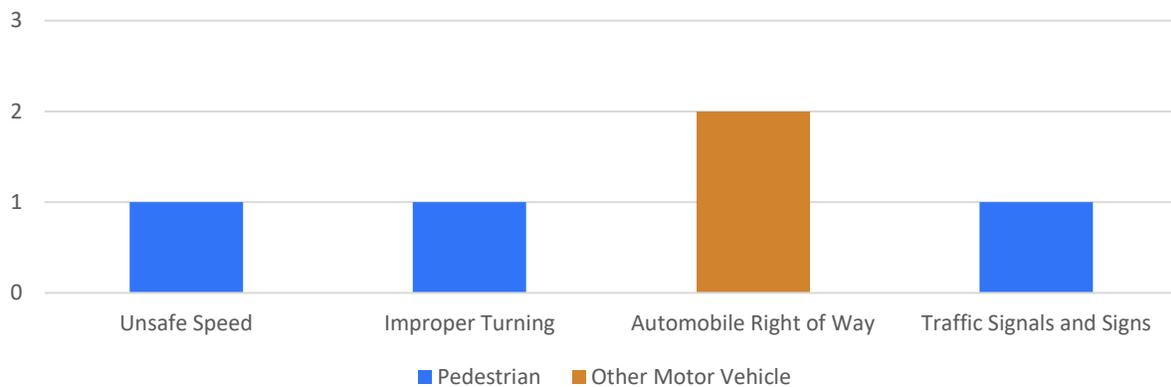


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**Motor Vehicle Involved with and Violation Category**

For all collisions that led to a fatality or severe injury, the collision violation category of collisions that led to the highest amount of collisions was automobile right of way collisions. The results, with violation category and motor vehicle involved with, are shown in **Figure 20**.

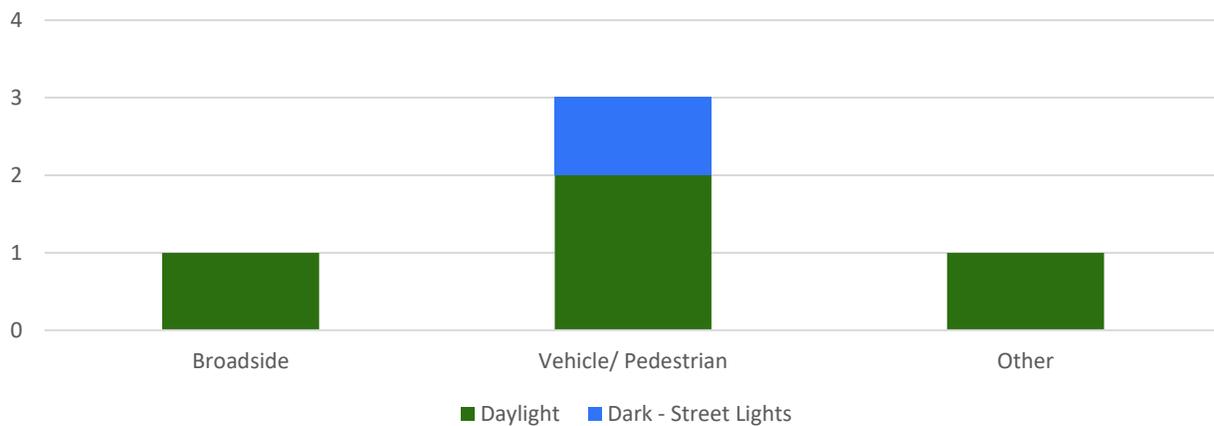
**Figure 20. F+SI Collisions: Motor Vehicle Involved with vs Violation Category**



**Collision Type and Lighting Conditions**

For all F+SI collisions, 4 collisions occurred in the daylight. The only collisions that occurred in the dark was a vehicle/pedestrian collision. **Figure 21** illustrates the lighting condition and the collision type as observed for F+SI collisions.

**Figure 21. F+SI Collisions: Collision Type Vs Lighting Conditions**

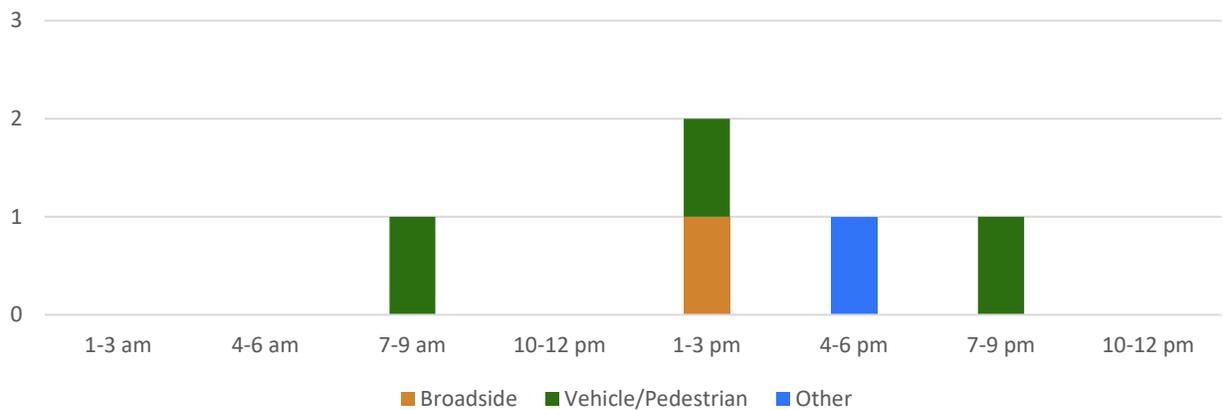


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**Collision Type and Time of the Day**

For all the F+SI collisions, the most common collision type was vehicle pedestrian collisions, which occurred throughout the day. The only broadside collision occurred during the afternoon. **Figure 22** illustrates the collision type by the time of the day for all F+SI collisions.

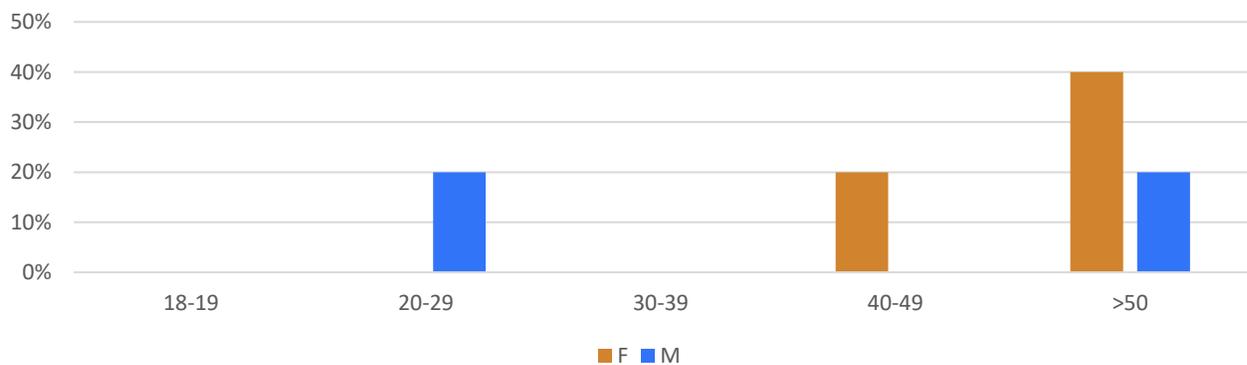
**Figure 22. F+SI Collisions: Collisions Type vs Time of the Day**



**Gender vs. Age**

For F+SI collisions, the sex of the party at fault was slightly more likely to be female than male (60 percent of F+SI collisions vs 40 percent). The party at fault for F+SI collisions are also more likely to be older, with the majority age 40 or older (80 percent). **Figure 23** illustrates the sex and age of the party at fault for F+SI collisions.

**Figure 23. F+SI Collisions: Age vs Sex**



## Geographic Collision Analysis

This section describes a detailed geographic collision analysis performed for injury collisions occurring at roadway segments and intersections in the City of Fort Bragg. The above collision analysis was used to identify four main collision factors that highlight the top collision trends in the City of Fort Bragg. These four collision factors were identified to be vehicle pedestrian collisions, automobile right of way violation collisions, unsafe speed collisions and broadside collisions.

### Vehicle/Pedestrian Collisions

For F+SI collisions in the City of Fort Bragg, 60 percent of collisions were pedestrian involved collisions, compared to just 4 percent for collisions of all severity, meaning pedestrian collisions are more likely to result in a fatal or severe injury. **Figure 24** shows the distribution of pedestrian collisions throughout the City of Fort Bragg between 2015 and 2019. Redwood Avenue, Highway 1, Franklin Street and Harold Street have a higher concentration of pedestrian collisions, compared to other roads in Fort Bragg. The Office of Traffic Safety ranked Fort Bragg 15<sup>th</sup> out of 75 similarly sized cities with high levels of pedestrian collisions (one being the highest, or worst)<sup>4</sup>.

### Automobile Right of Way Collisions

For F+SI collisions in the City of Fort Bragg, 40 percent of collisions were automobile right of way collisions compared to 18 percent of collisions of all severity, meaning automobile right of way collisions are more likely to result in a fatal or severe injury. **Figure 25** shows the distribution of automobile right of way collisions throughout Fort Bragg between 2015 and 2019. South Main Street, East Bush Street, East Laurel Street, East Oak Street and Highway 20 have a higher concentration of automobile right of way collisions, compared to other Fort Bragg roads.

### Unsafe Speed Collisions

For F+SI collisions in the City of Fort Bragg, 20 percent of collisions were unsafe speed collisions compared to 12 percent of collisions of all severity, meaning unsafe speed collisions are more likely to result in a fatal or severe injury. **Figure 26** shows the distribution of unsafe speed collisions throughout Fort Bragg between 2015 and 2019. South Main Street, West Oak Street, East Fir Street and South Harold Street have a higher concentration of unsafe speed collisions, compared to other Fort Bragg roads. The Office of Traffic Safety ranked Fort Bragg 16<sup>th</sup> out of 75

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<sup>4</sup> California Office of Traffic Safety. (2018). Office of Traffic Safety Rankings 2018. [https://www.ots.ca.gov/media-and-research/crash-rankings-results/?wpv-wpcf-year=2018&wpv-wpcf-city\\_county=Fort+Bragg&wpv\\_filter\\_submit=Submit](https://www.ots.ca.gov/media-and-research/crash-rankings-results/?wpv-wpcf-year=2018&wpv-wpcf-city_county=Fort+Bragg&wpv_filter_submit=Submit)

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similarly sized cities with high levels of speed related collisions (one being the highest, or worst)<sup>2</sup>.

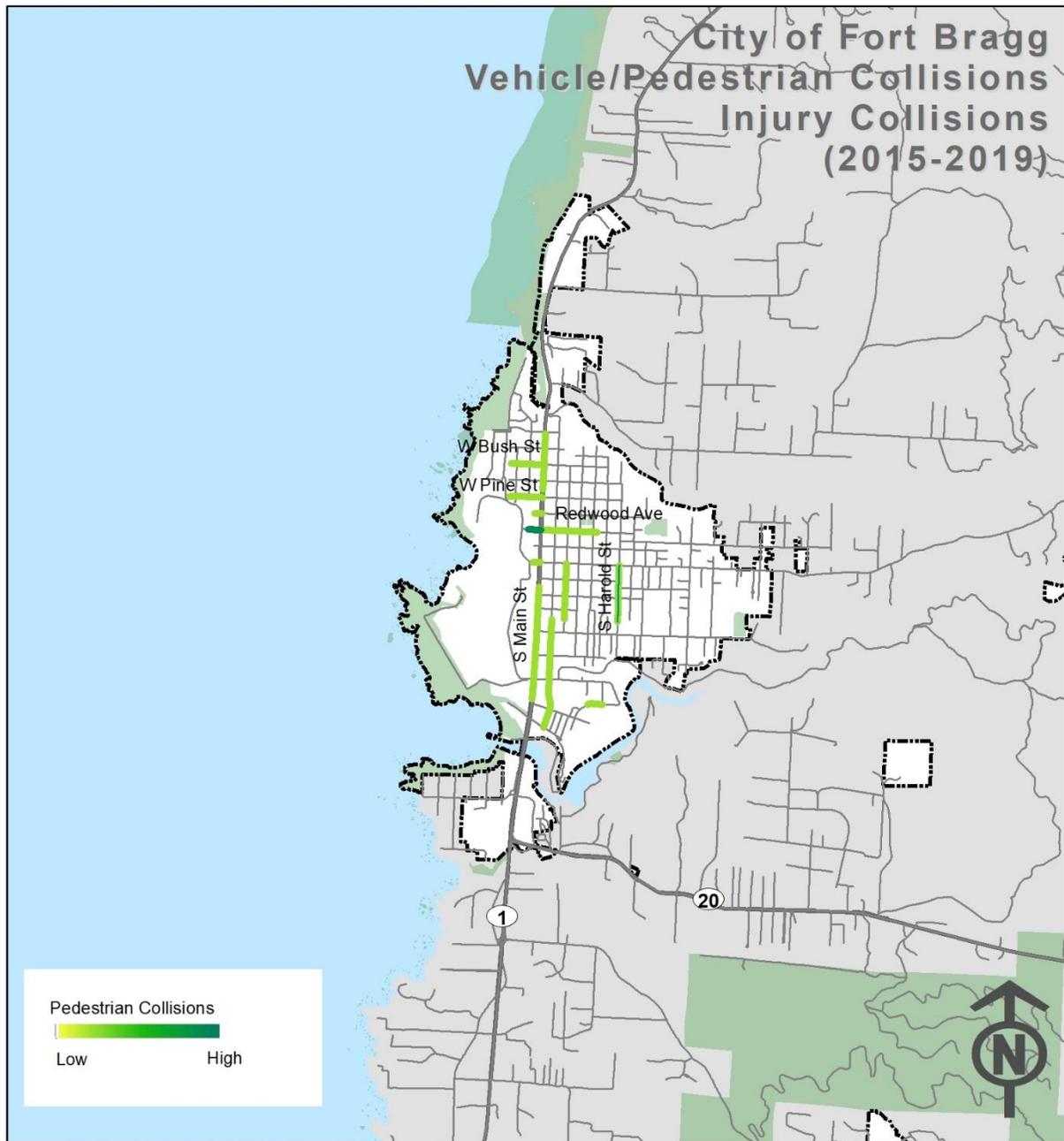
#### **Broadside Collisions**

For F+SI collisions in the City of Fort Bragg, 20 percent of collisions were broadside collisions.

**Figure 27** shows the distribution of broadside collisions throughout Fort Bragg between 2015 and 2019. Main Street, Laurel Street, Oak Street and Maple Street have a higher concentration of broadside collisions, compared to other Fort Bragg roads.

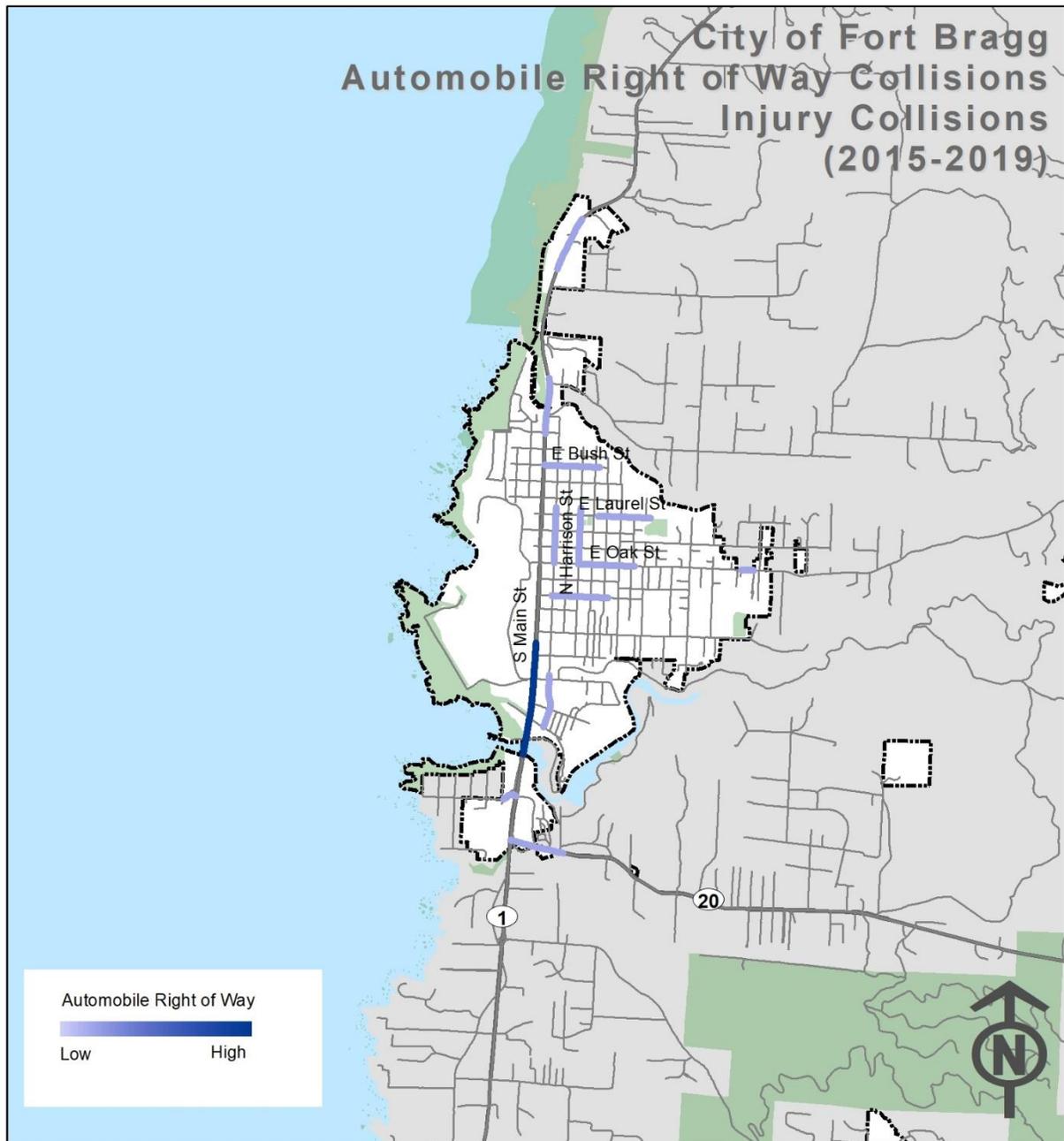
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Figure 24. Vehicle/Pedestrian Collisions



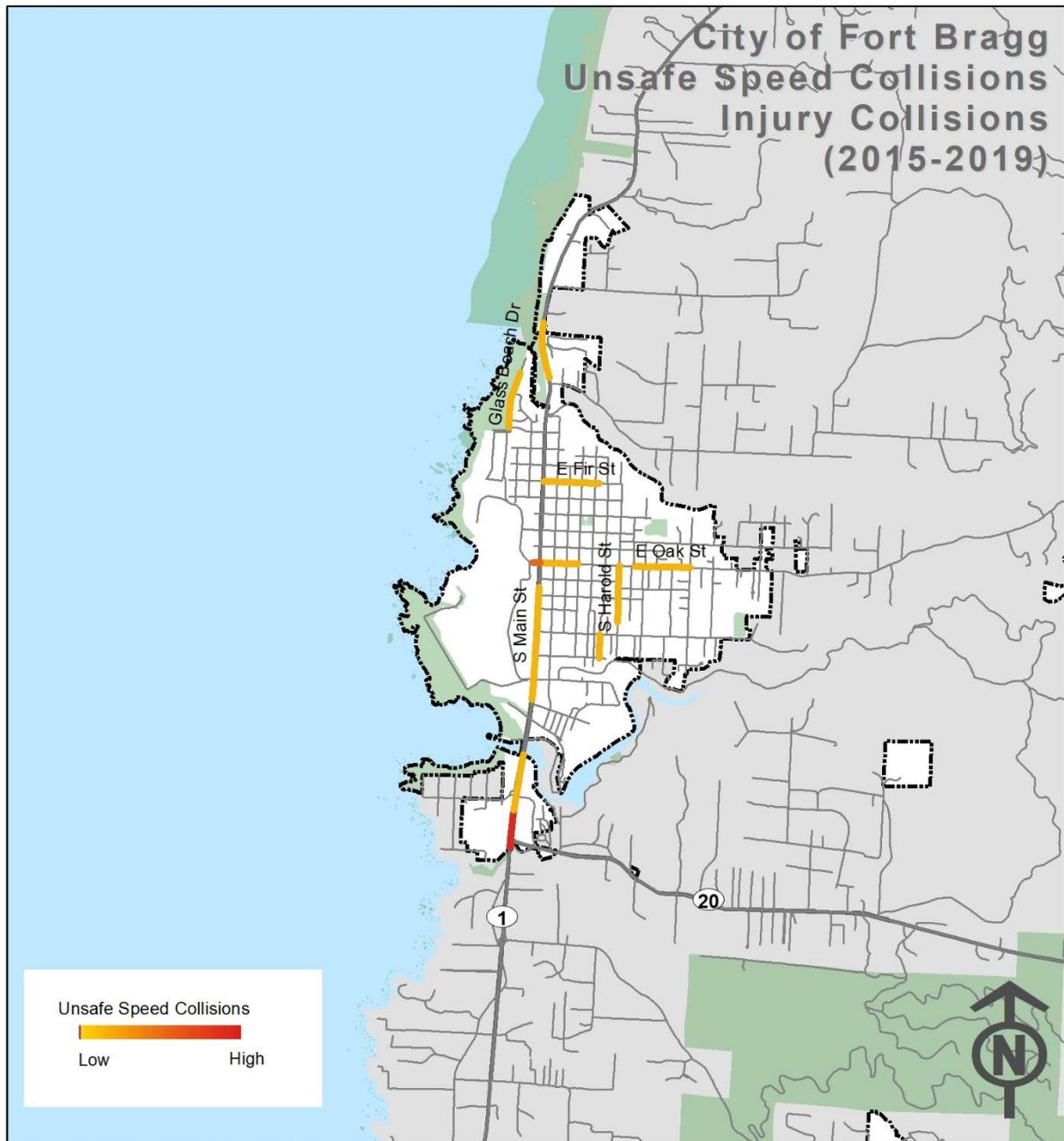
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Figure 25. Automobile Right of Way Collisions



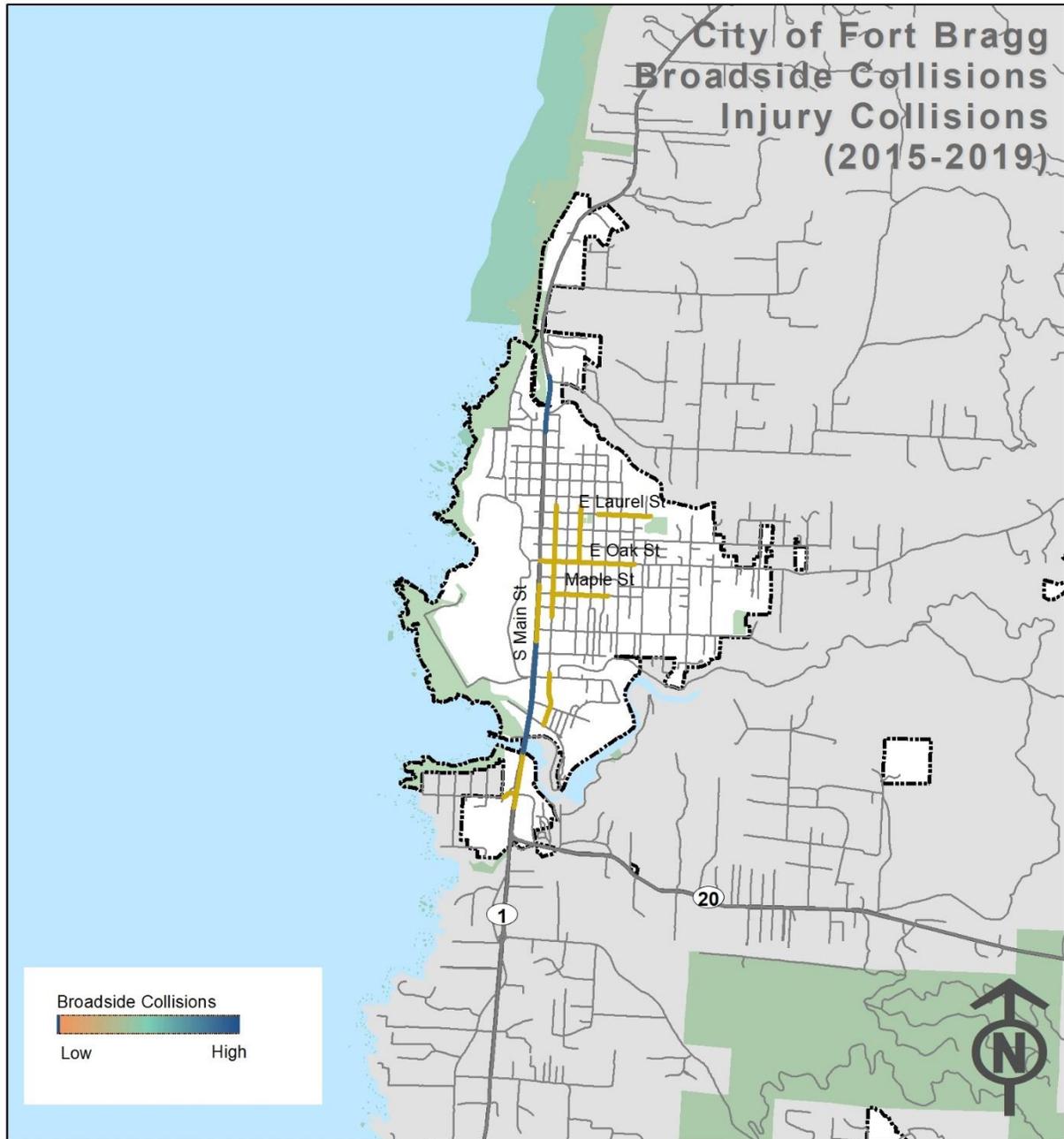
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Figure 26. Unsafe Speed Collisions



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Figure 27. Broadside Collisions



### Collision Severity Weight

A collision severity weight was used to identify the high severity collision network, using the Equivalent Property Damage Only (EPDO) method. The EPDO method accounts for both the severity and frequency of collisions by converting each collision to an equivalent number of property damage only (PDO) collisions. The EPDO method assigns a crash cost and score to each collision according to the severity of the crash weighted by the comprehensive crash cost. These EPDO scores are calculated using a simplified version of the comprehensive crash costs per HSIP Cycle 10 application. The weights used in the analysis are shown below in **Table 7**.

**Table 7. EPDO Score used in HSIP Cycle 10**

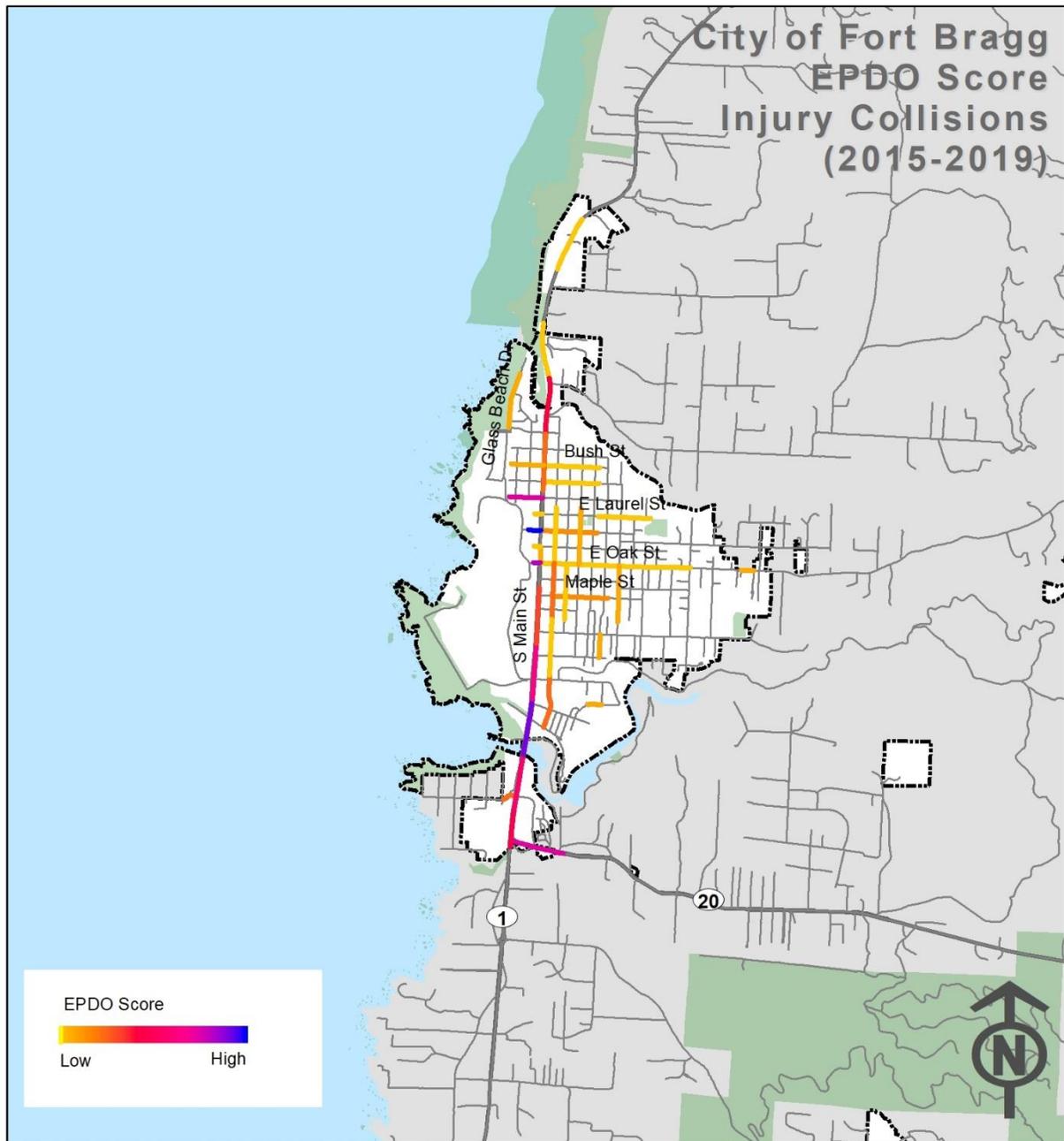
Collision Severity	EPDO Score
Fatal and Severe Injury Combined	165*
Visible Injury	11
Possible Injury	6
PDO	1

\*This is the score used in HSIP Cycle 10 for collisions on roadway segments, to simplify the analysis this study uses the same score for all F+SI collisions regardless of location

The EPDO scores for all collisions can then be aggregated in a variety of ways to identify collision patterns, such as location hot-spots. The weighted collisions for the City of Fort Bragg were geolocated onto Fort Bragg’s road network. **Figure 28** shows the location and geographic concentration of collisions by their EPDO score.

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Figure 28. Fort Bragg EPDO Score



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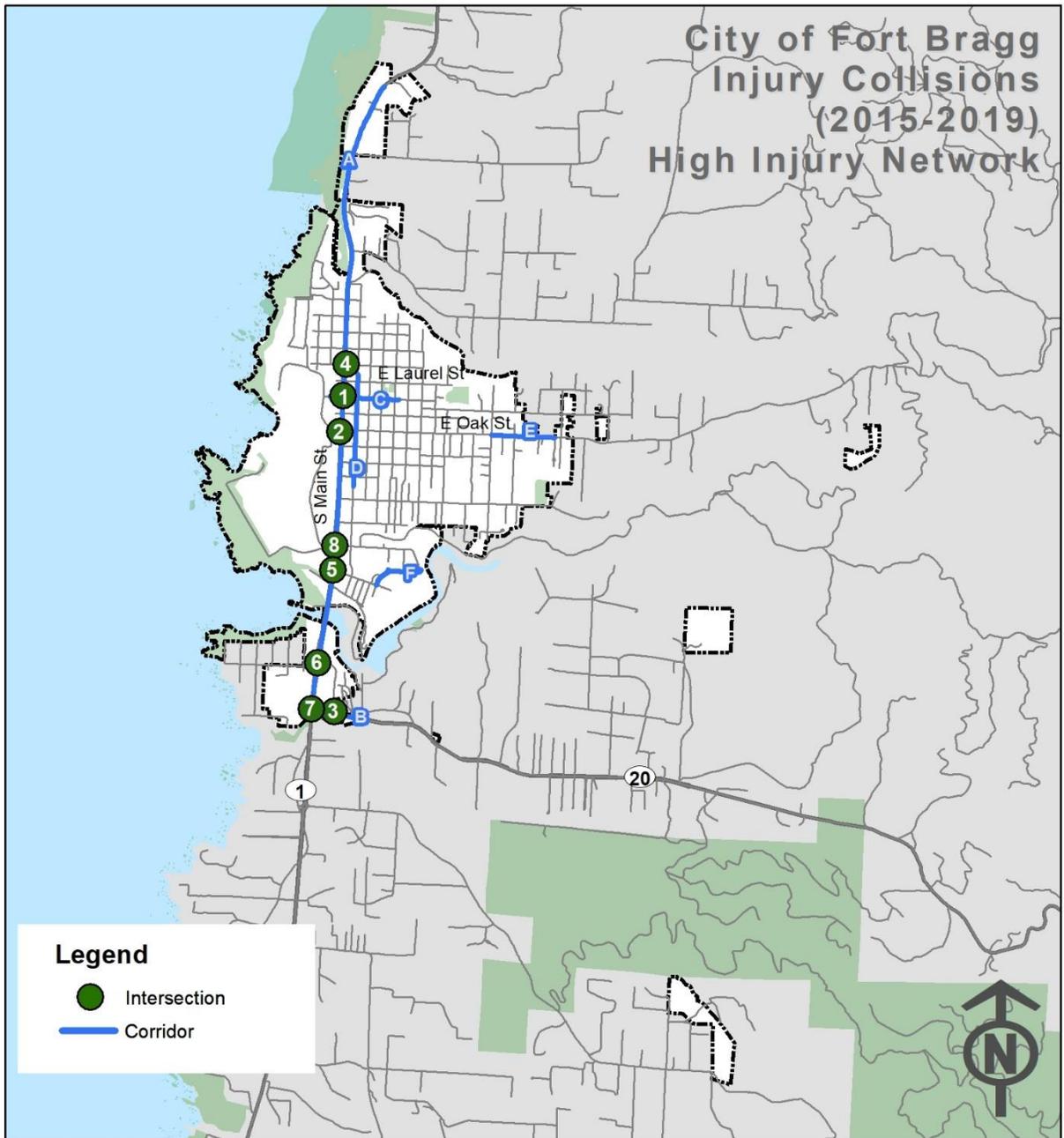
### High-Injury Locations

Following the detailed collision analysis in Section 4 and 5 the next step was to identify the high-risk roadway segments and intersections in the City of Fort Bragg. The methodology for scoring the high injury locations is the same method used in the severity weight section. **Figure 29** shows the top 6 high-collision corridors, and top 8 high-collision intersections. This high collision network has a total of 49 injury collisions with 5 F+SI collisions, which represents 62 percent of injury collisions and 100 percent of F+SI collisions in Fort Bragg on about 2 percent of Fort Bragg's roadway network.

For the identification of the high collision network, intersections include collisions that occurred within 250 feet of it and roadways include all collisions that occurred along the roadway except for collisions that occurred occur directly at an intersection, or collisions that occurred at a distance of 0 feet as listed in the statewide integrated traffic records system (SWITRS).

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Figure 29. City of Fort Bragg High Injury Network



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**High Injury Intersections**

A total of eight intersections were identified as high injury intersections. There were a total of 5 F+SI collisions that occurred at these intersections. The intersection of Redwood Avenue and South Main Street/Route 1 has the highest EPDO score.

**Table 8** lists the collision rate of the top 8 identified high-collision intersections along with their collision total and the number of F+SI collisions.

**Table 8. High Injury Intersections**

ID	Intersection	Total	F+SI	Vehicle /Ped	Auto R/W	Unsafe Speed	Broad-side	EPDO Score
1	Redwood Ave and Route 1/S Main St	5	1	4	0	0	0	171
2	Oak St and Route 1/S Main St	2	1	1	0	2	0	165
3	Boat Yard Dr and Route 20	1	1	0	1	0	0	165
4	Pine St and Route 1/ S Main St	1	1	1	0	0	0	165
5	South St and Route 1/S Main St	1	1	0	1	0	1	165
6	Boat Yard Drive and Route 1/ S Main St	3	0	0	1	0	1	28
7	Route 1 and Route 20	3	0	0	0	3	0	23
8	Cypress St and Route 1/ S Main St	3	0	1	0	0	1	18

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**High Injury Corridors**

Six corridors were identified as high injury corridors. There were a total 2 F+SI collisions on these corridors. The corridor with the highest number of F+SI collisions is Main Street/Route 1 and Fort Bragg-Willits Road/Highway 20 with 1 F+SI collision each.

**Table 9** lists the collision rate of the top 6 identified high-collision corridors along with the number of F+SI collisions and total collisions.

**Table 9. High Injury Corridors**

ID	Corridors	Total	F+SI	Vehicle/ Ped	Auto R/W	Unsafe Speed	Broadside	Length (miles)	EPDO Score
A	Main St/Route 1: Jane Ln to Highway 20/ Fort Bragg Willits Rd	29	1	2	5	6	6	3.6	383
B	Highway 20/ Fort Bragg Willits Rd: Route 1 to South Harbor Dr	1	1	0	1	0	0	0.1	165
C	Redwood Ave: West Terminus to North Whipple St	5	0	4	0	0	0	0.3	35
D	Franklin St: Laurel St to E Chestnut St	3	0	0	0	0	2	0.6	23
E	Fort Bragg Sherwood Rd: California Way to Dana St	1	0	0	1	0	0	0.2	11
F	River Dr/ Kemppe Way: South St to Cypress St	1	0	1	0	0	0	0.3	11

## **4. Emphasis Areas**

Emphasis areas are focus areas for the local roadway safety plan that are identified through the comprehensive collision analysis of the identified high injury locations within the City of Fort Bragg. Emphasis areas help in identifying appropriate safety strategies and countermeasures with the greatest potential to reduce collisions occurring at these high injury locations. In addition, traffic safety related concerns were heard at a Stakeholder's Meeting conducted for this plan on June 17<sup>th</sup>, 2021.

This chapter summarizes the top 6 emphasis areas identified for the City of Fort Bragg. These emphasis areas were derived from the consolidated high injury collision database (**Appendix B**) where top injury factors were identified by combining the data manually. Along with findings from the data analysis, stakeholder input was also considered while identifying emphasis areas specific to the City of Fort Bragg.

The following are the identified emphasis areas –

- Intersection safety
- Collisions within 250 feet of intersections
- Pedestrian safety
- Improper Turning Collisions
- Route 1 Collisions
- Alley Ways Collisions
- Older Adult Party at Fault Collisions

### **The Four E's OF Traffic Safety**

LRSP utilizes a comprehensive approach to safety incorporating "4 E's of traffic safety":

**E**ngineering, **E**nforcement, **E**ducation and **E**mergency Medical Services (EMS). This approach recognizes that not all locations can be addressed solely by infrastructure improvements. Incorporating the 4 E's of traffic safety is often required to ensure successful implementation of significant safety improvements and reduce the severity and frequency of collisions throughout a jurisdiction.

Some of the common violation types that may require a comprehensive approach are speeding, failure-to-yield to pedestrians, red light running, aggressive driving, failure to wear safety belts, distracted driving, and driving while impaired. When locations are identified as having these types of violations, coordination with the appropriate law enforcement agencies is needed to

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arrange visible targeted enforcement to reduce the potential for future driving violations and related crashes and injuries.

To improve safety, education efforts can also be used to supplement enforcement. Additionally, education efforts can supplement enforcement to improve the efficiency of each. Education can also be employed in the short-term to address high crash locations until the recommended infrastructure project can be implemented, addressed under Engineering improvements and countermeasures. Similarly, Emergency Medical Services entails strategies around supporting organizations that provide rapid response and care when responding to collisions causing injury, by stabilizing victims and transporting them to facilities

**Existing Traffic Safety Efforts in the City of Fort Bragg**

The City of Fort Bragg has already implemented safety strategies corresponding to the 4 E’s of traffic safety. The strategies detailed in this chapter can supplement these existing programs and concentrate them on high injury collision locations and crash types. These initiatives are summarized in the following table:

**Table 10. Existing Programs Summary**

<b>Document/ Program</b>	<b>Description</b>	<b>E’s Addressed</b>
<b>2018 Street Safety Plan</b>	This plan recommends infrastructure improvements that will enhance the safety of pedestrians, bicyclists and motorists on residential neighborhoods and commercial streets in Fort Bragg.	Engineering
<b>South Main Street Access and Beautification Plan (2011)</b>	This project enhances pedestrian crossings of Highway 1, with curb extensions, high visibility striping, stop bars, pedestrian signage and strategically placed median refuge islands. It also improves safety by reducing vehicle speeds, as well as beautifies the streetscape with trees and landscape strips.	Engineering
<b>City of Trails: Trails Feasibility Study (2016)</b>	This City of Trails Feasibility Study evaluates three potential new priority trails which could be developed to expand the existing trail network in Fort Bragg.	Engineering
<b>Mendocino County Safe Routes to School Plan (2014)</b>	In addition to the Citywide program the countywide Safe Routes to School (SRTS) is also a resource to a program with a simple goal: helping more children get to school by walking and bicycling.	Engineering Education
<b>Mendocino County Regional Active</b>	Details bicycle and pedestrian improvements on County significant corridors. Includes detailed priority bike and pedestrian projects.	Engineering

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Document/ Program	Description	E's Addressed
<b>Transportations Plan (2017)</b>		
<b>Mendocino Council of Governments 2020 Regional Transportation Improvement Program (2019)</b>	The Regional Transportation Improvement Program (RTIP) is a program of highway, local road, transit and active transportation projects that a region plans to fund with State and Federal revenue.	Engineering
<b>Fort Bragg Police Department Ongoing Programs and Resources</b>	The City Police Department has a number of programs and resources to reduce traffic fatalities and injuries including a crosswalk safety pamphlet, a bicycle safety pamphlet and an ongoing commitment to enforcing traffic violations at key location in Fort Bragg including schools.	Enforcement Education
<b>Walk and Bike Mendocino</b>	Walk and Bike Mendocino promotes walking and biking as a primary transportation choice in short distance travel in Mendocino County.	Education

**Factors Considered in the Determination of Emphasis Areas**

This section presents collision data analysis of collision type, collision factors, facility type, roadway geometries, analyzed for the various emphasized areas. Emphasis areas were determined by factors that led to the highest amount of injury collisions, with a specific emphasis on fatal and severe (F+SI) injury collisions. In addition to the collision data, emphasis areas were also identified from the feedback received from stakeholders. This section also presents comprehensive programs, policies and countermeasures to reduce collisions in specific emphasis areas.

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**Emphasis Area 1 – Intersections Collisions**

The City of Fort Bragg experienced a total of 49 reported collisions on the high injury network. 42 (86 percent) of these collisions occurred at intersection, including 5 fatal and severe injuries (F+S) collisions. The following collision data is based on only intersection injury collisions in the high injury network in the City of Fort Bragg.

28%                      24%                      66%  
**Pedestrian collisions**            **Improper turning**                      **Occurred on Route 1**

Table 11. Emphasis Area 1 Strategies

Objective:			
Reduce the number of fatal and severe injury collisions at intersections.			
	Strategy	Performance Measure	Agencies/ Organizations
Education	Conduct public information and education campaign for intersection safety laws regarding traffic signals, stop signs, and turning left or right.	Number of education campaigns	City/ School District/ Police Department
Enforcement	Targeted enforcement at high-risk intersections to monitor traffic law violations right-of-way violations, speed limit laws and other violations that occur at intersections.	Number of tickets issued.	Police Department
Engineering	<ul style="list-style-type: none"> <li>• S02, Improve signal hardware: lenses, back-plates with retroreflective borders, mounting, size, and number</li> <li>• S03, Improve signal timing (coordination, phases, red, yellow, or operation)               <ul style="list-style-type: none"> <li>• S08, Convert signal to mast arm (from pedestal-mounted)</li> <li>• S09, Install raised pavement markers and striping (Through Intersection)                   <ul style="list-style-type: none"> <li>• S16/NS04/NS05, Convert intersection to roundabout</li> </ul> </li> </ul> </li> <li>• NS06, Install/upgrade larger or additional stop signs or other intersection warning/regulatory signs               <ul style="list-style-type: none"> <li>• NS07, Upgrade intersection pavement markings (NS.I.)                   <ul style="list-style-type: none"> <li>• R01, Add Segment Lighting</li> </ul> </li> </ul> </li> <li>• R22, Install/Upgrade signs with new fluorescent sheeting (regulatory or warning)</li> <li>• R27, Install delineators, reflectors and/or object markers</li> </ul>	Number of intersections improved.	City
EMS	S05, Install emergency vehicle pre-emption systems	EMS vehicle response time.	Mendocino County Local Emergency Services Agency

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**Emphasis Area 2 – Pedestrian Safety**

The City of Fort Bragg experienced a total of 49 reported collisions on the high injury network. 13 (27 percent) of these collisions were pedestrian collisions, including 3 fatal or severe injury (F+SI) collisions. The following collision data is based on only pedestrian injury collisions in the high injury network in the City of Fort Bragg.

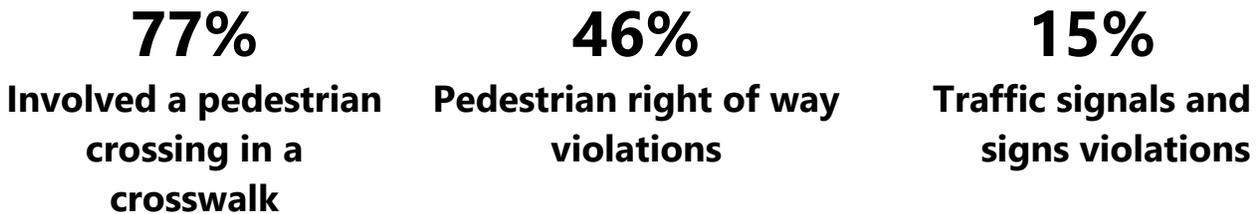


Table 12. Emphasis Area 2 Strategies

Objective:			
Reduce the number of fatal and severe pedestrian injury collisions.			
	Strategy	Performance Measure	Agencies/ Organizations
<b>Education</b>	Conduct pedestrian safety campaigns and outreach to raise their awareness of pedestrian safety needs through media outlets, social media and Bike and Walk Mendocino. Update pamphlet for crosswalk safety for Fort Bragg every 3-5 years	Number of education campaigns	City/ School District/ Police Department
<b>Enforcement</b>	Targeted enforcement at high-risk locations especially near schools and downtown	Number of tickets issued.	Police Department
<b>Engineering</b>	<ul style="list-style-type: none"> <li>• S21PB, Modify signal phasing to implement a Leading Pedestrian Interval (LPI)</li> <li>• NS07, Upgrade intersection pavement markings (NS.I.)               <ul style="list-style-type: none"> <li>• NS19PB, Install raised medians (refuge islands)</li> </ul> </li> <li>• NS21PB/R35PB, Install/upgrade pedestrian crossing (with enhanced safety features)               <ul style="list-style-type: none"> <li>• R36PB, Install raised pedestrian crossing</li> </ul> </li> <li>• R37PB, Install Rectangular Rapid Flashing Beacons (RRFB)               <ul style="list-style-type: none"> <li>• High-visibility ladder crosswalks</li> <li>• Mid-block curb extension</li> </ul> </li> <li>• In-road yield sign for pedestrian crossing at crosswalk</li> <li>• The City should apply for HSIP pedestrian set aside funds every two years</li> </ul>	Number of locations improved.	City
<b>EMS</b>	S05, Install emergency vehicle pre-emption systems	EMS vehicle response time.	Mendocino County Local Emergency Services Agency

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**Emphasis Area 3 – Improper Turning Collisions**

The City of Fort Bragg experienced a total of 49 reported collisions on the high injury network. 10 (20 percent) of these collisions were improper turning collisions, including 1 fatal or severe injury (F+SI) collisions. The following collision data is based on only improper turning injury collisions in the high injury network in the City of Fort Bragg.

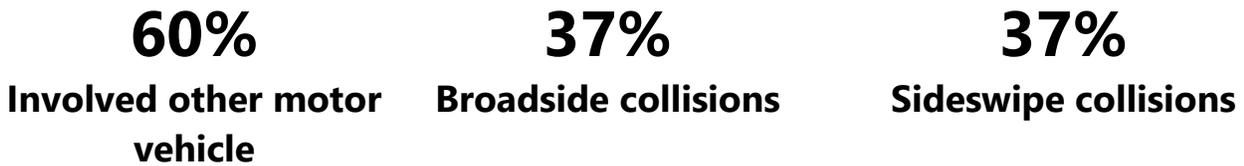


Table 13. Emphasis Area 3 Strategies

Objective:			
Reduce the number of fatal and severe injury collisions at intersections that are a result of improper turning.			
	Strategy	Performance Measure	Agencies/ Organizations
<b>Education</b>	Conduct public information and education campaign for intersection safety laws regarding traffic lights, stop signs, and turning left or right.	Number of education campaigns	City/ School District/ Police Department
<b>Enforcement</b>	Targeted enforcement at high-risk intersections to monitor improper turning violations.	Number of tickets issued.	Police Department
<b>Engineering</b>	<ul style="list-style-type: none"> <li>• S02, Improve signal hardware: lenses, back-plates with retroreflective borders, mounting, size, and number</li> <li>• S03, Improve signal timing (coordination, phases, red, yellow, or operation)               <ul style="list-style-type: none"> <li>• S08, Convert signal to mast arm (from pedestal-mounted)</li> </ul> </li> <li>• S09, Install raised pavement markers and striping (Through Intersection)               <ul style="list-style-type: none"> <li>• S16/NS04/NS05, Convert intersection to roundabout</li> </ul> </li> <li>• NS06, Install/upgrade larger or additional stop signs or other intersection warning/regulatory signs               <ul style="list-style-type: none"> <li>• NS07, Upgrade intersection pavement markings (NS.I.)                   <ul style="list-style-type: none"> <li>• R01, Add Segment Lighting</li> </ul> </li> </ul> </li> <li>• R22, Install/Upgrade signs with new fluorescent sheeting (regulatory or warning)               <ul style="list-style-type: none"> <li>• R27, Install delineators, reflectors and/or object markers</li> </ul> </li> </ul>	Number of intersections improved.	City
<b>EMS</b>	S05, Install emergency vehicle pre-emption systems	EMS vehicle response time.	Mendocino County Local Emergency Services Agency

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**Emphasis Area 4 – Route 1 Collisions**

The City of Fort Bragg experienced a total of 49 reported collisions on the high injury network. 36 (73 percent) of these collisions were collisions that occurred on Route 1, including 3 fatal or severe injury (F+SI) collisions. The following collision data is based on only Route 1 injury collisions in the high injury network in the City of Fort Bragg.

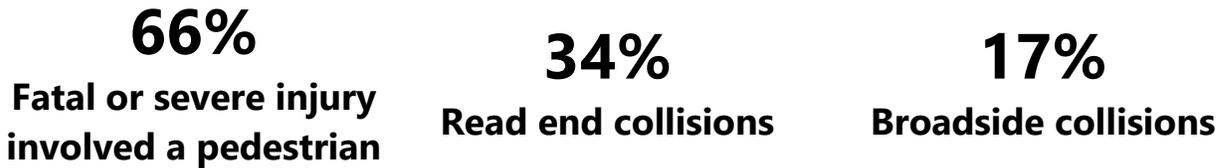


Table 14. Emphasis Area 4 Strategies

<b>Objective:</b>			
<b>Reduce the number of fatal and severe injury collisions that occur on Route 1.</b>			
	<b>Strategy</b>	<b>Performance Measure</b>	<b>Agencies/ Organizations</b>
<b>Education</b>	Conduct public information and education campaign for intersection safety laws regarding traffic lights, stop signs, turning left or right, and speeding.	Number of education campaigns	City/ School District/ Police Department
<b>Enforcement</b>	Targeted enforcement at high-risk intersections to monitor safety along Route 1.	Number of tickets issued.	Police Department
<b>Engineering</b>	<ul style="list-style-type: none"> <li>S02, Improve signal hardware: lenses, back-plates with retroreflective borders, mounting, size, and number</li> <li>S03, Improve signal timing (coordination, phases, red, yellow, or operation)</li> <li>S09, Install raised pavement markers and striping (Through Intersection)               <ul style="list-style-type: none"> <li>S16/NS04/NS05, Convert intersection to roundabout</li> </ul> </li> <li>NS06, Install/upgrade larger or additional stop signs or other intersection warning/regulatory signs</li> <li>S21PB, Modify signal phasing to implement a Leading Pedestrian Interval (LPI)               <ul style="list-style-type: none"> <li>NS07, Upgrade intersection pavement markings (NS.I.)</li> <li>NS19PB, Install raised medians (refuge islands)</li> </ul> </li> <li>NS21PB/R35PB, Install/upgrade pedestrian crossing (with enhanced safety features)R27, Install delineators, reflectors and/or object markers</li> </ul>	Number of locations improved.	City
<b>EMS</b>	S05, Install emergency vehicle pre-emption systems	EMS vehicle response time.	Mendocino County Local Emergency Services Agency

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**Emphasis Area 5 – Alley Way Safety**

The City of Fort Bragg experienced a total of 3 reported collisions on alley ways. The following collision data is based on only alley way collisions in the City of Fort Bragg.

**Substandard width**

**Sightline issues**

**Pedestrian conflicts**

Table 15. Emphasis Area 5 Strategies

<b>Objective:</b>			
<b>Reduce the number of collisions at Alley Ways.</b>			
	<b>Strategy</b>	<b>Performance Measure</b>	<b>Agencies/Organizations</b>
<b>Engineering</b>	<ul style="list-style-type: none"> <li>Pave and install mark crosswalks at alleyway driveways</li> </ul>	Number of alley ways improved.	City

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**Emphasis Area 6 – Older Adult Party at Fault Collisions**

The City of Fort Bragg reported a total of 49 reported collisions on the high injury network. The following is a review of the demographic data, provided in the party data of the collisions occurring on the high injury network.

**60%**  
**Fatal or severe injury collisions party at fault was between the ages of 50-69**

**60%**  
**Fatal or severe injury collisions party at fault was a female**

Table 16. Emphasis Area 6 Strategies

<b>Objective:</b>			
<b>Reduce the number of older adult fatal and severe injury collisions.</b>			
	<b>Strategy</b>	<b>Performance Measure</b>	<b>Agencies/Organizations</b>
<b>Education</b>	Target education programs for older adults. Distribute brochures/fliers with basic red light running, speeding, distracted driving, aggressive driving and stop sign violations information at driver training programs. Include statistics of older adult larger risks of fatalities.	Number of education campaigns	City/ Police Department

## 5. Countermeasure Identification

This section summarizes the process of selecting countermeasures on Fort Bragg streets as part of the analysis for the LRSP. Countermeasures were selected for each of the identified high-risk intersections and roadway segments based on extensive review of existing conditions at the site and characteristics of identified collisions on the High Injury Network.

Identified collision factors and existing conditions were cross referenced with the Caltrans LRSM identified countermeasures that are HSIP approved. Countermeasures that best fit the site and had the highest opportunity for systemic implementation were selected. Countermeasures were selected not only for each high-risk location, but also for each identified citywide Emphasis Area.

### Countermeasures Selection

In 2010, the Federal Highway Administration (FHWA) published a set of three manuals local and rural road owners to present a simple, data driven safety analysis framework for rural agencies across the country. In conjunction with these documents, California Department of Transportation (Caltrans) developed the Local Roadway Safety Manual (LRSM). The goal of this manual is to *“maximize the safety benefits for local roadways by encouraging all local agencies to proactively identify and analyze their safety issues and to position themselves to compete effectively in Caltrans’ statewide, data-driven call-for-projects.”*<sup>5</sup> Although, the LRSM identifies all of California’s local roadway safety issues and the countermeasures that address them, this document only highlights the issues and countermeasures relevant to the local roads of the City of Chowchilla. This section identifies the different solutions for the City from HSIP-qualified and non-HSIP countermeasures. It also provides a brief description along with their corresponding crash reduction factors (CRF), expected life and baseline cost. An excerpt of the LRSM, detailing each available HSIP countermeasure referenced in the recommendations tables, is included as **Appendix C**.

The countermeasures have been divided into three categories:

- Signalized (S) – countermeasures only applicable for signalized intersections;
- Non-Signalized (NS) – countermeasures only applicable to stop-controlled, or uncontrolled intersections;
- Roadway Segment (RS) – countermeasures only applicable to roadway segments;
- Other (O) – countermeasures that do not qualify for HSIP funding.

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<sup>5</sup> <https://dot.ca.gov/-/media/dot-media/programs/local-assistance/documents/hsip/2020/lrsm2020.pdf>

## Draft Countermeasure Toolbox

**Appendix D** detail the draft countermeasures for each high-risk location and Emphasis Area, separated by intersections and roadway segments. While not all of these countermeasures will be included in the resulting safety projects, they are included to give the City a toolbox for implementing future safety improvements through other means, such as the City's Capital Improvement Program.

### Signalized Intersections Countermeasures

**S03 – Improve signal timing (coordination, phases, red, yellow, or operation) Improve signal hardware: lenses, back-plates with retroreflective borders, mounting, size, and number.** Includes adding phases, lengthening clearance intervals, eliminating or restricting higher-risk movements, and coordinating signals at multiple locations.

- Crash Reduction Factor – 15%
- Expected Life – 10 years
- Baseline Cost – Approximately \$11,000 per intersection

**S09 – Install raised pavement markers and striping (Through Intersection)** Addition of clear pavement markings, raised pavement marking to help guide motorists through complex intersections.

- Crash Reduction Factor – 10%
- Expected Life – 10 years
- Baseline Cost – Approximately \$35,000 per intersection

**S12 – Install raised median on approaches (S.I.)** Addition of raised medians next to left-turn lanes at intersections, directly over existing pavement.

- Crash Reduction Factor – 25%
- Expected Life – 20 years
- Baseline Cost – Approximately \$45,000 - \$40,000

**S17PB – Install pedestrian countdown signal heads** A pedestrian countdown signal contains a timer display and counts down the number of seconds left to finish crossing the street. Countdown signals can reassure pedestrians who are in the crosswalk when the flashing "DON'T WALK" interval appears that they still have time to finish crossing.

- Crash Reduction Factor – 25%
- Expected Life – 20 years
- Baseline Cost – Approximately \$10,000

**S21PB - Modify signal phasing to implement a Leading Pedestrian Interval (LPI).** A leading pedestrian interval (LPI) gives pedestrians the opportunity to enter an intersection 3-7

- Crash Reduction Factor – 15%
- Expected Life – 10 years

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seconds before vehicles are given a green indication. With this head start, pedestrians can better establish their presence in the crosswalk before vehicles have priority to turn left.

- Baseline Cost –  
Approximately \$4,000 per intersection

### Non-Signalized Intersections Countermeasures

**NS06 – Install/upgrade larger or additional stop signs or other intersection warning/regulatory signs.** The visibility of intersections and, thus, the ability of approaching drivers to perceive them can be enhanced by installing larger regulatory and warning signs at or prior to intersections. A key to success in applying this strategy is to select a combination of regulatory and warning sign techniques appropriate for the conditions on a particular unsignalized intersection approach.

- Crash Reduction Factor –  
15%
- Expected Life – 10 years
- Baseline Cost –  
Approximately \$4,200 per intersection

**NS07 – Upgrade intersection pavement markings (NS.I.).** Unsignalized intersections that are not clearly visible to approaching motorists, particularly approaching motorists on the major road. The strategy is particularly appropriate for intersections with patterns of rear-end, right-angle, or turning crashes related to lack of driver awareness of the presence of the intersection

- Crash Reduction Factor –  
25%
- Expected Life – 10 years
- Baseline Cost –  
Approximately \$900 per intersection

**NS20 – Install pedestrian crossing at uncontrolled locations (signs and markings only).** Adding pedestrian crossings has the opportunity to enhance pedestrian safety at locations noted as being problematic. Pavement markings delineate a portion of the roadway that is designated for pedestrian crossing. These markings will often be different for controlled verses uncontrolled locations

- Crash Reduction Factor –  
25%
- Expected Life – 10 years
- Baseline Cost –  
Approximately \$5,000

**NS21PB – Install/upgrade pedestrian crossing at uncontrolled locations (with enhanced safety features).** Adding pedestrian crossings that include enhances safety features has the opportunity to enhance pedestrian safety at locations noted as being especially problematic. The enhanced safety elements help delineate a portion of the roadway that is designated for pedestrian crossing.

- Crash Reduction Factor –  
35%
- Expected Life – 20 years
- Baseline Cost –  
Approximately \$15,000

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**NS22PB – Install Rectangular Rapid Flashing Beacon (RRFB)** Rectangular Rapid Flashing Beacon (RRFB) includes pedestrian-activated flashing lights and additional signage that enhance the visibility of marked crosswalks and alert motorists to pedestrian crossings

- Crash Reduction Factor – 35%
- Expected Life – 20 years
- Baseline Cost – Approximately \$40,000

### Roadway Countermeasures

**R01 – Add segment lighting.** Providing roadway lighting improves the safety during nighttime conditions by (1) making drivers more aware of the surroundings, which improves drivers' perception-reaction times, (2) enhancing drivers' available sight distances to perceive roadway characteristic in advance of the change, and (3) improving non-motorist's visibility and navigation.

- Crash Reduction Factor – 35%
- Expected Life – 20 years
- Baseline Cost – Approximately \$100,000

**R22 – Install/Upgrade signs with new fluorescent sheeting (regulatory or warning).** The target for this strategy should be on roadway segments with patterns of head on, nighttime, non-intersection, run-off road, and sideswipe crashes related to lack of driver awareness of the presence of a specific roadway feature or regulatory requirement. Ideally this type of safety CM would be combined with other sign evaluations and upgrades (install chevrons, warning signs, delineators, markers, beacons, and relocation of existing signs per MUTCD standards.).

- Crash Reduction Factor – 15%
- Expected Life – 10 years
- Baseline Cost – Approximately \$2,000

**R26 – Install dynamic/variable speed warning signs.** This strategy primarily addresses crashes caused by motorists traveling too fast around sharp curves. It is intended to get the drivers attention and give them a visual warning that they may be traveling over the recommended speed for the approaching curve. Care should be taken to limit the placement of these signs to help maintain their effectiveness.

- Crash Reduction Factor – 30%
- Expected Life – 10 years
- Baseline Cost – Approximately \$ 20,000

**R34PB – Install sidewalk/pathway (to avoid walking along roadway).** Sidewalks and walkways provide people with space to travel within the public right-of-way that is separated from roadway vehicles. The presence of sidewalks on both sides of the street has been found to be related to significant reductions

- Crash Reduction Factor – 80%
- Expected Life – 20 years
- Baseline Cost – Approximately \$150,000

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in the “walking along roadway” pedestrian crash risk compared to locations where no sidewalks or walkways exist.

**R35PB – Install/upgrade pedestrian crossing (with enhanced safety features).** Adding pedestrian crossings has the opportunity to greatly enhance pedestrian safety at locations noted as being problematic. The enhanced safety elements, which may include curb extensions, medians and pedestrian crossing islands, beacons, and lighting, combined with pavement markings delineating a portion of the roadway that is designated for pedestrian crossing.

- Crash Reduction Factor – 35%
- Expected Life – 20 years
- Baseline Cost – Approximately \$25,000

### Other Countermeasures

**Bulb outs/curb extensions.** Curb extensions (also called bulb-outs) extend the sidewalk into the parking lane to narrow the roadway and provide additional pedestrian space at key locations; they can be used at corners and at mid-block. Curb extensions enhance pedestrian safety by increasing pedestrian visibility, shortening crossing distances, slowing turning vehicles, and visually narrowing the roadway.

**Speed Feedback Signs.** Speed feedback signs, also known as dynamic speed displays, provide drivers with feedback about their speed in relationship to the posted speed limit. When appropriately complemented with police enforcement, speed feedback signs can be an effective method for reducing speeds at a desired location.

**In Road Yield/stop Signs.** In-street pedestrian crossing signs (MUTCD R1-6 or R1-6a) are placed within the roadway, either between travel lanes or in a median. The sign may be used to remind road users of laws regarding right-of-way at an unsignalized pedestrian crossing. This countermeasure is used with other crosswalk visibility enhancements to indicate optimal or preferred locations for people to cross and to help reinforce the driver requirement to yield the right-of-way to pedestrians at crossing locations.

## 6. Safety Projects

### High-Collision Network Projects

This section summarizes the process of selecting safety projects as part of the analysis for the City of Fort Bragg's LRSP. The next step after the identification of high-risk locations, emphasis areas and applicable countermeasures is to identify location-specific safety improvements for all high-risk roadway segments and intersections.

Specific countermeasures and improvements were selected from the 2020 LRSM, where:

- S refers to improvements at signalized locations,
- NS refers to improvements at non-signalized locations, and
- R refers to improvements at roadway segments.

The corresponding number refers to the countermeasure number in the LRSM (2020). The countermeasures were grouped into safety projects for high-risk intersections and roadway segments. A total of four safety projects were developed. All countermeasures were identified based on the technical teams' assessment of viability that consisted of extensive analysis, observations, and City staff input. The most applicable and appropriate countermeasures as identified have been grouped together to form projects that can help make high-risk locations safer.

**Table 17** lists the safety projects for high-risk intersections and roadway segments, along with total base planning level cost (2021 dollar amounts) estimates and the resultant preliminary Benefit-Cost (B/C) Ratio. The "Total Benefit" estimates were calculated for the proposed improvements being evaluated in the proactive safety analysis. This "Total Benefit" is divided by the "Total Cost per Location" estimates for the proposed improvements, giving the resultant B/C Ratio. The B/C Ratio Calculation follows the methodology as mentioned in the LRSM (2020).

**Appendix E** lists the detailed methodology to calculate B/C Ratio, the complete cost, benefit and B/C Ratio calculation spreadsheet.

The next step in the process will be to prepare grant ready materials for HSIP Cycle 11 applications. TJKM has scoped to provide the City with materials for up to three applications. However, it should be noted that while the LRSP projects were based on high-risk locations, HSIP applications can be expanded to include many locations across the city.

Once the three desired projects are selected, our team recommends three potential options for selecting locations to include in the HSIP applications:

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- Select the top projects ranked by crash cost
- City identifies desired intersections
- Apply for various intersections citywide with more generic cost estimates

These safety projects were chosen based on the previously completed collisions analysis, which was used to identify main collision attributes that were found to be leading factors of fatal and severe collisions in Fort Bragg. These collision factors were identified to be pedestrian collisions, automobile right of way collisions, unsafe speed collisions and intersection collisions.

For fatal and severe injury (F+SI) collisions, 60 percent of collisions involved a pedestrian. Redwood Avenue and Highway 1/ Main Street have a higher concentration of pedestrian collisions, compared to other roads in Fort Bragg. Recommended improvements at these locations include reducing modifying signal phasing to implement a leading pedestrian interval, upgrading pedestrian crossings at uncontrolled locations, installing sidewalks and installing Rectangular Rapid Flashing Beacon.

For F+SI collisions in the City of Fort Bragg, 40 percent of collisions were automobile right of way collisions. South Main Street, East Bush Street, East Laurel Street, East Oak Street and Highway 20 have a higher concentration of automobile right of way collisions, compared to other Fort Bragg roads. Recommended improvements at these locations include improving signal timing, and installing raised pavement markers and striping (Through Intersection).

For F+SI collisions in the City of Fort Bragg, 20 percent of collisions were unsafe speed collisions compared to 12 percent of collisions of all severity, meaning unsafe speed collisions are more likely to result in a fatal or severe injury. Main Street had a higher concentration of unsafe speed collisions, compared to other Fort Bragg roads. Recommended improvements at these locations include installing dynamic/variable speed warning signs.

When evaluating roadways vs intersections, it was observed that the majority of collisions occurred at intersections. In the City of Fort Bragg, 90% of all collisions occurred at intersections whereas 10% occurred on roadway segments. Many of these collisions occurred along Route 1/ Main Street. Recommended improvements at intersection locations include improving signal timing, installing raised pavement markers and striping and modifying signal phasing to implement a Leading Pedestrian Interval (LPI).

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Table 17. List of Viable Safety Projects

Location	CM1	CM2	CM3	Cost per Location	B/C Ratio
<b>Project 1 - Systemic Improvements at Unsignalized Intersections</b>					
Redwood Ave and Route 1/S Main St	S03		S21PB	\$ 18,410	46.90
Oak St and Route 1/S Main St	S03	S09	S21PB	\$ 28,683	
Boat Yard Drive and Route 1/S Main St	S03	S09		\$ 48,878	
Route 1 and Route 20	S03			\$ 35,210	
Cypress St and Route 1/S Main St	S03		S21PB	\$ 18,410	
<b>Project 2: Pedestrian Improvements at Unsignalized Intersections</b>					
Boat Yard Dr and Route 20	NS06			\$ 840	40.04
Pine St and Route 1/S Main St	NS06	NS21PB	NS22PB	\$ 122,087	
South St and Route 1/S Main St	NS06	NS21PB		\$ 32,928	
Highway 1/Main Street and Pudding Creek Road	NS06			\$ 1,785	
Noyo Point Road and S Main Street	NS06			\$ 1,505	
Harold/Oak St	NS06	NS21PB	NS22PB	\$ 88,928	
<b>Project 3: Systemic Roadway Segment Improvements</b>					
Main St/Route 1: Airport Road to Highway 20/ Fort Bragg Willits Rd	R22	R26	R35PB*	\$ 809,445	23.04
Highway 20/ Fort Bragg Willits Rd: Route 1 to South Harbor Dr	R22	R26		\$ 34,615	
Redwood Ave: West Terminus to North Whipple St	R22			\$ 6,020	
Franklin St: Laurel St to E Chestnut St	R22			\$ 23,310	

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Location	CM1	CM2	CM3	Cost per Location	B/C Ratio
Oak Street: California Way to Harold St	R22			\$ 5,740	
River Dr/ Kemppe Way: South St to Cypress St	R22			\$ 6,440	
Chestnut Street		R26		\$ 28,000	
<i>*Estimated 54 locations.</i>					
<b>Project 4: Pedestrian and Other Roadway Segment Improvements</b>					
Main St/Route 1: Airport Road to Highway 20/ Fort Bragg Willits Rd	R01	R34PB		\$ 1,023,901	6.59
Redwood Ave: West Terminus to North Whipple St	R01			\$ 12,600	
Oak St: California Way to Dana St	R01	R34PB		\$ 742,098	
River Dr/ Kemppe Way: South St to Cypress St	R01	R34PB		\$ 580,580	
<b>Project 5: Pedestrian Set Aside</b>					
Redwood Ave: West Terminus to North Whipple St	R35PB			\$ 245,000	N/A

**Notes:** CM – countermeasure. B/C ratio is the dollar amount of benefits divided by the cost of the countermeasure. S03 - Improve signal timing (coordination, phases, red, yellow, or operation), S09 – Install raised pavement markers and striping (Through Intersection), S21PB- Modify signal phasing to implement a Leading Pedestrian Interval (LPI), NS06 - Install/upgrade larger or additional stop signs or other intersection warning/ regulatory signs, NS20PB - Install pedestrian crossing at uncontrolled locations (new signs and markings only), NS21PB - Install/upgrade pedestrian crossing at uncontrolled locations (with enhanced safety features), NS22PB - Install Rectangular Rapid Flashing Beacon (RRFB), R01- Add segment lighting, R22 - Install/Upgrade signs with new fluorescent sheeting (regulatory or warning) , R26 - Install dynamic/variable speed warning signs, R34PB- Install sidewalk/pathway (to avoid walking along roadway), R35PB - Install/upgrade pedestrian crossing (with enhanced safety features)  
 Costs include contingency, PS&E, environmental and construction costs

**HSIP Applications**

The next step will be to prepare HSIP grant ready materials, so that the City may submit them for HSIP Cycle 11 funding in 2022.

## 7. Evaluation and Implementation

This chapter describes the steps the City may take to evaluate the success of this plan and steps needed to update the plan in the future. The LRSP is a guidance document and requires periodic updates to assess its efficacy and re-evaluate potential solutions. It is recommended to update the plan every two to five years in coordination with the identified safety partners. This document was developed based on community needs, stakeholder input, and collision analysis conducted to identify priority emphasis areas throughout the City. The implementation of strategies under each emphasis area would aim to reduce fatal and severe injury collisions in the coming years.

Funding is a critical component of implementing any safety project. While the HSIP program is a common source of funding for safety projects, there are numerous other funding sources that could be pursued for such projects. Potential funding sources are listed below in **Table 18**.

**Table 18. Potential Funding Sources**

Funding Source	Funding Agency	Amount Available	Next Estimated Call for Projects	Applicable E's	Notes
<b>Active Transportation Program</b>	Caltrans, California Transportation Commission	~\$223 million per year	2022	Engineering, Education	Can use used for most active transportation related safety projects as well as education programs
<b>Highway Safety Improvement Program</b>	Caltrans	TBD	Early 2022	Engineering	Most common grant source for safety projects
<b>Surface Transportation Block Group Program</b>	FHWA (Administered through MCTC)	Varies by FY	TBD	Engineering	Typically used for roadway projects
<b>Congestion Mitigation and Air Quality (CMAQ)</b>	FHWA (Administered through MCTC)	Varies by FY	TBD	Engineering	Focused on projects that improve air quality
<b>Office of Traffic Safety Grants</b>	California Office of Traffic Safety	Varies by grant	Closes January 31 <sup>st</sup> annually	Education, Enforcement, Emergency Response	10 grants available to address various components of traffic safety

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<b>Funding Source</b>	<b>Funding Agency</b>	<b>Amount Available</b>	<b>Next Estimated Call for Projects</b>	<b>Applicable E's</b>	<b>Notes</b>
<b>Affordable Housing and Sustainable Communities Program</b>	Strategic Growth Council and Dept. of Housing and Community Development	~\$405 million	2022	Engineering, Education	Must be connected to affordable housing projects; typically focuses on bike/ped infrastructure/programs
<b>Urban Greening</b>	California Natural Resources Agency	\$28.5 million	2022	Engineering	Focused on bike/pedestrian infrastructure and greening public spaces
<b>Local Streets and Road Maintenance and Rehabilitation</b>	CTC (distributed to local agencies)	\$1.5 billion statewide	N/A; distributed by formula	Engineering	Typically pays for road maintenance type projects
<b>RAISE Grant</b>	USDOT	~\$1 billion	2022	Engineering	Typically used for larger infrastructure projects
<b>Sustainable Transportation Equity Project</b>	California Air Resources Board	~\$19.5 million	TBD; most recent call in 2020	Engineering, Education	Targets projects that will increase transportation equity in disadvantaged communities
<b>Transformative Climate Communities</b>	Strategic Growth Council	~\$90 million	TBD; most recent call in 2020	Engineering	Funds community-led projects that achieve major reductions in greenhouse gas emissions in disadvantaged communities.

**Implementation**

The LRSP document provides engineering, education, enforcement, and emergency medical service related countermeasures that can be implemented throughout the City to reduce F+SI collisions. It is recommended that the City of Fort Bragg implement the selected projects high-collision locations in coordination with other projects proposed for the City’s infrastructure development in their future Capital Improvement Plans.

The success of the LRSP can be achieved by fostering communication among the City and the safety partners.

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### Monitoring and Evaluation

For the success of the LRSP, it is crucial to monitor and evaluate the four E-strategies continuously. Monitoring and evaluation help provide accountability, ensures the effectiveness of the countermeasures for each emphasis area, and help making decisions on the need for new strategies. The process would help the City make informed decisions regarding the implementation plan's progress and accordingly, update the goals and objectives of the plan.

After implementing countermeasures, the strategies should be evaluated annually as per their performance measures. The evaluation should be recorded in a before-after study to validate the effectiveness of each countermeasure as per the following observations:

- Number of fatal and severe injury collisions
- Number of police citations
- Number of public comments and concerns

Evaluation should be conducted during similar time periods and durations each year. The most important measure of success of the LRSP should be reduction in fatal and severe injury collisions throughout the City. If the number of F+SI collisions doesn't decrease initially, then the countermeasures should be evaluated as per the other observations, as mentioned above. The effectiveness of the countermeasures should be compared to the goals for each emphasis area.

### LRSP Update

The LRSP is a guidance document and is recommended to be updated every two to five years after adoption. After monitoring performance measures focused on the status and progress of the E's strategies in each emphasis area, the next LRSP update can be tailored to resolve any continuing safety problems. The City of Fort Bragg's Public Works Department will be accountable for the progress of the plan goals. An annual stakeholder meeting with the safety partners is also recommended to discuss the progress for each emphasis area and oversee the implementation plan. The document should then be updated as per the latest collision data, emerging trends, and the E's strategies' progress and implementation.

# Appendices:

## APPENDIX A: TABLE OF POLICIES AND PROJECTS FROM THE LITERATURE REVIEW:

Document	Highlights
<p><b>City of Fort Bragg Coastal General Plan (2008)</b></p>	<ul style="list-style-type: none"> <li>• Policy C-2.12 Roadway Safety: Improve the safety of the roadway system. All safety improvements shall be consistent with the applicable policies of the LCP including, but not limited to, the wetlands, environmentally sensitive habitat area, public access, and visual protection policies.</li> <li>• Program C-2.12.1: Periodically analyze the locations of traffic accidents to identify problems and use this information to set priorities for improvements as a part of the City's Capital Improvement Program.</li> <li>• Program C-4.1.1: Consider traffic safety, the ease and safety of pedestrian movement across Main Street, and adequacy of on-street parking as key factors in evaluation of proposed roadway improvements along Main Street.</li> <li>• Program C-4.1.4: Consider signaling the intersection of Pine Street and Main Street to provide adequate pedestrian safety.</li> <li>• Program C-9.7.1: Continue to provide traffic controls and well-lit intersections in areas with a high volume of pedestrian movement.             <ul style="list-style-type: none"> <li>• Program C-9.7.2: Consider expanded use of illuminated crosswalks</li> </ul> </li> <li>• Policy C-10.1 Comprehensive Bikeway System: Establish a comprehensive and safe system of bikeways connecting all parts of Fort Bragg.</li> <li>• Program C-10.1.1: Complete the bikeway system as indicated in Map C-2: Bicycle Paths. Make the completion of the Pudding Creek Trestle/Glass Beach to Otis Johnson Park a high priority.</li> <li>• Program C-10.1.2: Incorporate bicycle and pedestrian facilities into the design and construction of all road improvements as feasible.</li> </ul>
<p><b>Inland General Plan (2012)</b></p>	<p>Goals and Policies:</p> <p>Goal C-1: Complete Street Planning</p> <p>Goal C-2: Coordinate land use and transportation planning</p> <p>Goal C-3: Develop and manage a roadway system that accommodates future growth and maintains acceptable Levels of Service while considering the other policies and programs of the General Plan.</p> <p>Policy C-3.1.1: When a traffic analysis of levels of service and/or safety hazards indicates the need, construct the following roadway improvements:</p>

Document	Highlights
	<p>a) Signalize the Main Street/Pudding Creek Road intersection;</p> <p>b) Signalize the Franklin Street/Oak Street intersection;</p> <p>c) Widen the section of Main Street from the Pudding Creek Bridge to the northern City Limits to three lanes, adding a center turn lane;</p> <p>d) Signalize the Main Street/Pine Street intersection; and</p> <p>e) Consider extending Harrison Street south from Walnut Street to Cypress Street.</p> <p>Policy C-3.2 Roadway Standards: Continue to provide consistent standards for the City's street system.</p> <p>Program C-3.2.1: Establish standards for public streets, which allow for the following:</p> <p>a) Traffic "calming" measures;</p> <p>b) Sidewalks with curbs, gutters, and a planting strip between the sidewalk and the roadway;</p> <p>c) Rounded street corners with "bulb-outs" at key intersections;</p> <p>d) Continuation of the grid street system; and</p> <p>e) Standards for radius returns for local, collector, and arterial streets.</p> <p>Policy C-3.4 Continuation and Connectivity of Streets: Require the continuation of streets, bicycle and pedestrian paths through new developments wherever possible, and require connectivity to the street grid at as many points as feasible.</p> <p>Program C-3.4.1: Review site plans for new development to facilitate the continuation of streets to improve local circulation. Where streets are not feasible, priority shall be given to providing pedestrian and bicycle trails that establish bicycle and pedestrian connections to streets wherever possible.</p> <p>Policy C-3.6 Roadway Safety: Improve the safety of the roadway system.</p> <p>Program C-3.6.1: Periodically analyze the locations of traffic accidents to identify problems and use this information to set priorities for improvements as a part of the City's Capital Improvement Program.</p> <p>Goal C-8 Improve emergency access to the City.</p> <p>Policy C-8.1 Emergency Access: Establish an access route out of Fort Bragg that could be used in the event of damage to the Noyo River and Pudding Creek Bridges.</p> <p>Program C-8.1.1: Work with the property owners to obtain temporary use, in the event of an emergency, of the logging road that begins on Cypress Street and provides access to Highway 20 (aka the A&amp;W Haul Road), east of Fort Bragg.</p> <p>Program C-8.1.2: Work with the Mendocino Council of Governments and Mendocino County to upgrade Sherwood Road to Willits to provide a year-round emergency access route.</p>

Document	Highlights
<p><b>City of Fort Bragg Bicycle Master Plan (2009)</b></p>	<p>Program C-8.1.3: Prepare an emergency evacuation route plan for the City.</p> <p>Proposed Projects</p> <ul style="list-style-type: none"> <li>• Harold St (Maple to Fir Ave) – Install Class II Bike lanes</li> <li>• Harrison St (Walnut to Fir St) - Install Class II Bike Lanes</li> <li>• Madrone St ( Hwy 1 to Harold St) – Install Class II Bike lanes</li> <li>• Main St (Oak to Hare Creek Bridge) - Install Class II Bike Lanes               <ul style="list-style-type: none"> <li>• Main St (Elm to N City Limits) - Install Class II Bike Lanes</li> <li>• Maple St (Main St to Lincoln St) - Install Class II Bike Lanes</li> </ul> </li> <li>• N Franklin St (Pine St to Manzanita) - Install Class II Bike Lanes</li> <li>• S Lincoln St (Willow to Chestnut) - Install Class II Bike Lanes</li> <li>• <b>Mill Site Bike Trails</b> - A Class 1 bikeway that runs along the entire length of the Mill Site coast parallel and to the west of the proposed Ocean Bluff Drive (see proposed cross section below). Upon development this would become the new Pacific Coast Bike Route (PCBR) through Fort Bragg.</li> <li>• <b>Class Beach Drive</b> - As part of the Coastal Trail project, the City plans to install a ten foot wide multi-use trail (eight feet of asphalt and four feet NaturalPAVE®) in the approximately 18 feet of right of way along the western edge of Glass Beach Drive. This trail will join the Old Haul Road/Pudding Creek Trestle multi-use trail with the bikeway system on the Mill Site.</li> </ul>
<p><b>2018 Street Safety Plan</b></p>	<ul style="list-style-type: none"> <li>• Install a 4-way STOP at the intersection of Laurel Street and Harrison Street;</li> <li>• Install a 4-way STOP at the intersection of Maple Street and Harold Street;</li> <li>• Remove the traffic circle at the intersection of Fir Street and Harrison Street;</li> <li>• Initiate dialogue and negotiations with Caltrans regarding pedestrian safety on Main Street. The focus of these efforts should be:           <ul style="list-style-type: none"> <li>○ The intersection of Redwood Avenue and N Main Street with the recommendation of an advanced pedestrian timing at signal; and</li> <li>○ The intersection of Pine Street and N Main Street with the recommendation of enhanced pedestrian crosswalk.               <ul style="list-style-type: none"> <li>• <b>Maple Street:</b> <b>Lane Striping (Optional)</b> – Convert from dashed yellow to double yellow to emphasize No Passing.</li> <li><b>Narrow Through Lanes</b> – Narrow travel lanes from 12 feet to 11 feet as shown in the cross-section. The cross-section would include eight feet dedicated to parking and five feet for bike lanes on both sides together with the 11-foot travel lanes.</li> <li><b>Green Bike Lane Legend (Optional)</b> – Where there is a bike lane symbol, install a green background. The green markings would consist of paving materials that would not result in a</li> </ul> </li> </ul> </li> </ul>

Document	Highlights
	<p>slippery surface per the Ride-A-Way Colored Coatings Specifications. Ride-A-Way product brochure and specification details are included in Appendix E.</p> <p><b>No Parking</b> – Extend parking prohibitions on “block ends” where frontage housing does not have garage access. At these locations, the bike lane would move closer to the curb frontage. A striped buffer would be installed between the bike lane and the travel lane at these locations.</p> <p><b>Markings at Alleys</b> – Add cross-hatched striping in the parking lane at alley intersections.</p> <p><b>All-Way Stop-Control</b> – Create all-way stop controls at the Maple Street intersections with Whipple Street and Lincoln Street. The City may consider an additional all-way stop control at Harold Street based on a recommended citywide review of stop signs on the grid system (see Next Steps).</p> <p><b>Marked crosswalks</b> – Add north-south marked crosswalks at locations with new all-way stop controls, Whipple Street and Harold Street (there are already marked crosswalks at the intersection of Maple Street/ Lincoln Street).</p> <p><b>Bulb-outs/Curb Extensions</b> – Add striped (painted) bulb-outs on Maple Street at the proposed crosswalk locations, except at Harold Street where a physical concrete bulb-out already exists.</p> <p><b>Maple Street/Franklin Street</b> – Add high visibility ladder crosswalks on both the north and south legs of the intersection. Add advance yield markings (shark’s teeth) and pedestrian warning signs on both Franklin Street approaches.</p> <ul style="list-style-type: none"> <li>• <b>Elm Street</b></li> </ul> <p><b>Bike Cross Markings (Optional)</b> – Add green NACTO-type bike lane crossing markings at the intersections with Glass Beach Drive, Stewart Street, and North Main Street.</p> <p><b>Green Bike Lane Legend (Optional)</b> – Where there is a bike lane symbol, include a green background. The green markings are detailed in the Ride-A-Way pamphlet.</p> <p><b>Crosswalk at Glass Beach Drive</b> – Add a marked crosswalk on the north leg of the intersection along with the bike cross markings on both the north and south legs for bike crossing maneuvers from the trailhead parking to Elm Street.</p> <p><b>Install Ramp</b> – Install a curb ramp on the northwest corner of the intersection for the proposed crosswalk and bike lane crossing markings.</p> <ul style="list-style-type: none"> <li>• <b>Pine Street</b></li> </ul> <p><b>Stop Signs</b> – Convert intersections with Corry Street and Harrison Street to all-way stop control.</p> <p><b>Pedestrian Crossing Enhancements</b> – Add Pedestrian Crossing Signs (W11) on the uncontrolled east and west approaches to</p>

Document	Highlights
	<p>McPherson Street and Whipple Street. (Optional – Install advance yield markings (shark’s teeth) on the uncontrolled approaches.</p> <p><b>(Optional) Centerline Striping</b> – Convert centerline striping from dashed yellow to double yellow.</p> <p><b>Edgeline</b> – Add 6-inch edgeline striping, providing an 11-foot travel lane with the remaining space (approximately 10.5 feet each direction) for parking and bicyclists along the curb as shown on the cross section. Install a sharrow along the edge between the travel lane and the parking lane.</p> <p><b>Bulb-outs/Curb Extensions</b> – Add striped bulb-outs at crosswalk locations.</p> <p><b>Green Bike Lane Legend (Optional)</b> – Where there is a bike lane symbol,</p>
<p><b>South Main Street Access and Beautification Plan (2011)</b></p>	<p>Proposed Projects</p> <ul style="list-style-type: none"> <li>• <b>South Main at Madrone Street Intersection Improvements</b> – median refuge island, high visibility crosswalks, striping improvements <ul style="list-style-type: none"> <li>• <b>South Main at Maple Street</b> - median refuge island, high visibility crosswalks, striping improvements</li> <li>• <b>South Main and Hazel Street</b> – median refuge island, high visibility crosswalks, striping improvements</li> <li>• <b>South Main and Walnut Street</b> – median refuge island, high visibility crosswalks, striping improvements</li> <li>• <b>South Main and South Street</b> - median refuge island, high visibility crosswalks, striping improvements</li> </ul> </li> <li>• <b>South Main and North Harbor Drive</b> - median refuge island, high visibility crosswalks, striping improvements</li> <li>• <b>South Main and South of Noyo Bridge</b> - median refuge island, high visibility crosswalks, striping improvements <ul style="list-style-type: none"> <li>• <b>State Route at Boat Yard Drive</b>- bulb out, striping improvements</li> <li>• <b>South Main and Cypress Street</b> – Bulb outs, Striping</li> </ul> </li> <li>• <b>South Main at State Route</b> - Bulb outs, Striping, remove one slip lane, reconfigure other slip lane</li> <li>• <b>Roundabout Option – South Main and North Harbor Drive</b></li> </ul>
<p><b>City of Trails: Trails Feasibility Study (2016)</b></p>	<p>Projects</p> <ul style="list-style-type: none"> <li>○ <b>Redwood Avenue Connection to Downtown Fort Bragg</b> – Pedestrian improvements are proposed for Chief Celery Drive. Redwood Avenue improvements would include new wayfinding signs leading to/from Franklin Street and information about trails for visitors. A new parking area located on the GP Mill Site due west of Alder Streets would serve the middle section of the Coastal Trail (currently in design).</li> </ul>

Document	Highlights
	<ul style="list-style-type: none"> <li>○ <b>Old Mill Road Redevelopment to North Noyo Harbor</b> – Old Mill Road is an abandoned road that drops from the southern section of the Coastal Trail (near the cemetery) down to Noyo Harbor and Noyo Beach. This report evaluates requirements for redeveloping this old road cut into a multi-use trail that would extend the Coastal Trail to the beach at Noyo Bay, and potentially beyond to North Noyo Harbor.</li> <li>○ <b>South Noyo Harbor Trail</b> – An existing social trail on private property leads from Highway 1 down to South Noyo Harbor. Landowners on the alignment would like to reduce illegal activities there and employers at the Harbor have expressed interest in the trail. This report recommends installation of timber (or concrete timber) steps and surfacing with quarry fines on the inclined sections.</li> </ul>
<p><b>City of Trails:  Supplemental Trail  Feasibility Studies  (2017)</b></p>	<p style="text-align: center;">Projects</p> <ul style="list-style-type: none"> <li>○ <b>Old Mill Road</b> An existing route along the face of the coastal bluff south of the Coastal Trail would be converted to a multi-use trail. The trail would be located on the levee top of the existing Noyo Harbor dredge pond berm west of the cliff face roadway.</li> <li>○ <b>North Harbor Drive</b> A trail separate from the roadway would be implemented on North Harbor Drive between Casa Del Noyo and the Noyo Fishing Center to connect with the lower portion of Harbor Drive. This Study addresses the feasibility of placing a Class 1 or Class II* multi-use trail parallel to the North Harbor Drive. Due to right-of-way and topographic constraints along the roadway, a trail structure cantilevered over the narrow road shoulder and adjacent retaining wall was evaluated.</li> </ul>
<p><b>City of Fort Bragg FY  2020-2021 Budget</b></p>	<p style="text-align: center;">Proposed Projects</p> <ul style="list-style-type: none"> <li>• South Main St Bike and Ped Improvements</li> <li>• Maple Street SD and Alley Rehabilitation</li> </ul>
<p><b>Mill Site Specific Plan  (2012)</b></p>	<p><b>Improvement of Pedestrian Safety has been emphasized.</b></p> <p><b>Policy MM-1. "Complete Streets."</b> As part of the first Master Tentative Subdivision Map for the Plan Area, the applicant shall establish a multi-modal network of "complete streets" that balances the needs for safety and comfort of pedestrians, cyclists, drivers, and transit riders and that substantially conforms to the conceptual street network design.</p> <p><b>Policy MM-14. Complete Streets.</b> All streets shall be designed as complete streets for the safety and comfort of cyclists and pedestrians, including children, the elderly, and people with disabilities, consistent with US Department of Transportation complete streets guidelines.</p> <p><b>Policy MM-16. Safe Streets.</b> The design speed of streets in the Central and Northern Districts shall not exceed 25 miles per hour, with typical operating speeds below 20 miles per hour. In the Southern District, design speeds may be as high as 30 miles per hour, with typical</p>

Document	Highlights
	<p>operating speeds below 25 miles per hour. Streets shall be designed to optimize pedestrian safety and comfort, with the minimum number of travel lanes necessary to accommodate their traffic function at Level of Service E or better, averaged over the midweek peak one hour. If unacceptable traffic congestion is identified, traffic shall be redistributed onto additional streets, or accommodated with a right- or left-turn pocket, rather than by adding a travel lane.</p> <p><b>Specific traffic calming elements included in the site design include:</b></p> <ul style="list-style-type: none"> <li>• Corner “bulb-outs” at most intersections, ensuring low-speed turning movements and improving pedestrian safety;</li> <li>• Ample landscape along the roadway edge;</li> <li>• Small blocks and stop signs at most intersections; and</li> <li>• Bicycle lanes on the wider streets</li> </ul> <p><b>Policy MM-32. Additional Traffic Calming Measures.</b> The City engineer may require additional traffic calming features where necessary to ensure pedestrian safety.</p>
<p><b>Mendocino County Regional Active Transportation Plan (2017)</b></p>	<p>Goals</p> <ul style="list-style-type: none"> <li>• To improve our public spaces so the street, road and transportation system meets the needs of all surface transportation modes, including vehicular, bicycle, pedestrian and transit.</li> <li>• Provide a safe and useable network of bicycle and pedestrian facilities throughout the region as a means to lessen dependence on vehicular travel and improve the health of Mendocino County’s residents.</li> <li>• Maximize investment in non-motorized transportation facilities through maintenance.</li> </ul>
<p><b>Mendocino County Safe Routes to School Plan (2014)</b></p>	<p>Goals</p> <p><b>Goal 1:</b> Improve the health of Mendocino County children by focusing attention on and increasing active travel to school.</p> <p><i>Objective A: Increase the number of students walking and bicycling to school</i></p> <p><i>Objective B: Annually increase the number of children exposed to Safe Routes to School education and encouragement activities</i></p> <p><i>Objective C: Increase the number of county residents that are familiar with SRTS and resources available</i></p> <p><b>Goal 2:</b> Support school travel routes that are accommodating, safe, convenient, and “complete” for all modes.</p> <p><i>Objective A: Increase funding for walking, bicycling and transit investments near schools</i></p> <p><i>Objective B: Review school connections and potential SRTS needs during project development for all county roads</i></p> <p><i>Objective C: Incorporate Safe Routes to School policies, priorities, and design guidance into future county general plan updates</i></p> <p><i>Objective D: Limit traffic speeds and volumes along key routes to schools</i></p>

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Document	Highlights
	<p><b>Goal 3:</b> Maximize interagency cooperation in all SRTS project and programs in an effort to build a sustainable program.</p> <p><i>Objective A: Establish an ongoing countywide SRTS program that serves all interested schools in Mendocino County.</i></p> <p><i>Objective B: Seek and secure outside grant funding for SRTS programs and activities, and leverage local funding for school area improvements</i></p>
<p><b>Mendocino County Pedestrian Facility Needs Inventory and Engineered Feasibility Study (2019)</b></p>	<p>Projects  Tier 1 Projects</p> <ul style="list-style-type: none"> <li>• Elm Street Pedestrian Improvements</li> <li>• South Main Street Corridor Pedestrian Enhancements</li> <li>• Maple Street Pedestrian Improvements</li> </ul>
<p><b>Mendocino Council of Governments 2020 Regional Transportation Improvement Program (2019)</b></p>	<p>Projects</p> <ul style="list-style-type: none"> <li>• S. Main St Bicycle &amp; Pedestrian Access Project – Fort Bragg - This project will continue pedestrian improvements along a state highway, linking to existing facilities. The project will improve access to a major shopping area, school facility, and tourist attractions. Pedestrian safety will be improved.</li> </ul>



## **APPENDIX B. CONSOLIDATED COLLISION DATABASE**



## **APPENDIX C: HSIP ELIGIBLE COUNTERMEASURES**



## **APPENDIX D: COUNTERMEASURE TOOLBOX**



**APPENDIX E: B/C RATIO CALCULATION - LRSM (2020)**



Case ID	Accident Year	Collision Date	Primary Road	Secondard Road	Distance	Direction
6792537	2015	1/21/2015	NORTH MAIN ST	E REDWOOD AV	3	S
7004210	2015	7/25/2015	RT 1	BOAT YARD DR	20	N
7009890	2015	7/31/2015	MAIN ST	RT 1	38	W
7010680	2015	7/24/2015	RT 1	RT 20	166	N
7062865	2015	9/27/2015	RT 1	BUSH ST	8	N
7076588	2015	9/3/2015	E REDWOOD AV	NORTH MAIN ST	0	
7113323	2015	10/28/2015	FRANKLIN ST	HAZEL ST	224	S
7114920	2015	11/2/2015	RT 1	PUDDING CREEK BRIDGE	108	N
8024052	2016	4/7/2016	RT 1	E PINE ST	0	
8025139	2016	5/12/2016	RT 1	CYPRESS ST	287	S
8050202	2016	5/27/2016	RT 1	OAK ST	310	N
8064636	2016	6/9/2016	RT 1	WALNUT ST	91	N
8064789	2016	6/10/2016	SOUTH MAIN ST	CYPRESS ST	141	N
8079801	2016	7/2/2016	RT 1	E ALDER ST	2	S
8169129	2016	11/9/2016	FRANKLIN ST	E REDWOOD AV	154	S
8204769	2016	42733	RT 1	E REDWOOD AV	0	
8290669	2017	1/12/2017	RT 1	BOAT YARD DR	29	N
8320405	2017	2/21/2017	RT 1	MAPLE ST	16	N
8351905	2017	4/14/2017	RT 1	MANZANITA ST	8	W
8420750	2017	7/25/2017	RT 1	CYPRESS ST	0	
8451084	2017	9/17/2017	RT 1	W REDWOOD AV	3	S
8462036	2017	9/28/2017	RT 1	CYPRESS ST	311	N
8469680	2018	1/14/2018	RT 1	RT 20	40	N
8471916	2017	10/8/2017	RT 1	RT 20	455	N
8472307	2017	10/9/2017	RT 1	OCEAN VIEW DR	250	N
8504330	2017	12/29/2017	RT 1	CHESTNUT ST	136	S
8504603	2017	12/29/2017	RT 1	NOYO POINT RD	10	N
8504755	2017	12/26/2017	RT 1	NORTH HARBOR DR	61	S
8524282	2017	12/18/2017	KEMPE WY	RIVER DR	35	E
8574315	2018	2/25/2018	MAPLE ST	FRANKLIN ST	15	E
8586649	2018	3/19/2018	RT 1	OCEAN VIEW DR	200	S
8599950	2018	3/30/2018	RT 1	E REDWOOD AV	6	N
8604238	2018	4/20/2018	MAPLE ST	RT 1	3	E
8660586	2018	7/5/2018	SOUTH MAIN ST	SOUTH ST	46	S
8707747	2018	7/18/2018	OAK ST	RT 1	44	E
8779995	2018	12/30/2018	OAK ST	RT 1	0	
8820623	2019	3/2/2019	FRANKLIN ST	MADRONE ST	77	S
8832627	2019	3/23/2019	OAK ST	HOCKER LN	128	W
8875476	2019	6/10/2019	RT 1	RT 20	178	S
8886037	2019	5/30/2019	BOAT YARD DR	RT 20	201	N
8896645	2019	7/1/2019	RT 1	AIRPORT RD	650	N
8898670	2019	7/6/2019	MAIN ST	SPRUCE ST	137	N
8910462	2019	8/2/2019	RT 1	CYPRESS ST	0	
8923682	2019	8/14/2019	E REDWOOD AV	N FRANKLIN ST	145	W
8924871	2019	8/13/2019	REDWOOD AV	N MCPHERSON ST	26	W

## B.1 Intersection Countermeasures – Signalized

### S01, Add intersection lighting (Signalized Intersection => S.I.)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	"night" crashes	40%	20 years
Notes:	This CM only applies to "night" crashes (all types) occurring within limits of the proposed roadway lighting 'engineered' area.		
General information			
<b>Where to use:</b>			
Signalized intersections that have a disproportionate number of night-time crashes and do not currently provide lighting at the intersection or at its approaches. Crash data should be studied to ensure that safety at the intersection could be improved by providing lighting (this strategy would be supported by a significant number of crashes that occur at night).			
<b>Why it works:</b>			
Providing lighting at the intersection itself, or both at the intersection and on its approaches, improves the safety of an intersection during nighttime conditions by (1) making drivers more aware of the surroundings at an intersection, which improves drivers' perception-reaction times, (2) enhancing drivers' available sight distances, and (3) improving the visibility of non-motorists. Intersection lighting is of particular benefit to non-motorized users. Lighting not only helps them navigate the intersection, but also helps drivers see them better.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
A lighting project can usually be completed relatively quickly, but generally requires at least 1 year to implement because the lighting system must be designed and the provision of electrical power must be arranged. The provision of lighting involves both a fixed cost for lighting installation and an ongoing maintenance and power cost which results in a moderate to high cost. Some locations can result in high B/C ratios, but due to higher costs, these projects often result in medium to low B/C ratios.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Night, All	CRF: 20-74%

### S02, Improve signal hardware: lenses, back-plates with retroreflective borders, mounting, size, and number

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	15%	10 years
Notes:	This CM only applies to crashes occurring on the approaches / influence area of the upgraded signals. This CM does not apply to improvements like "battery backup systems", which do not provide better intersection/signal visibility or help drivers negotiate the intersection (unless applying past crashes that occurred when the signal lost power). If new signal mast arms are part of the proposed project, CM "S2" should not be used and the signal improvements would be included under CM "S7".		
General information			
<b>Where to use:</b>			
Signalized intersections with a high frequency of right-angle and rear-end crashes occurring because drivers are unable to see traffic signals sufficiently in advance to safely negotiate the intersection being approached. Signal intersection improvements include new LED lighting, signal back plates, retro-reflective tape outlining the back plates, or visors to increase signal visibility, larger signal heads, relocation of the signal heads, or additional signal heads.			
<b>Why it works:</b>			
Providing better visibility of intersection signals aids the drivers' advance perception of the upcoming intersection. Visibility and clarity of the signal should be improved without creating additional confusion for drivers.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Installation costs and time should be minimal as these type strategies are classified as low cost and implementation does not typically require the approval process normally associated with more complex projects. When considered at a single location, these low cost improvements are usually funded through local funding by local maintenance crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in low to moderate cost projects that are more appropriate to seek state or federal funding.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Rear-End, Angle	CRF: 0-46%

### S13PB, Install pedestrian median fencing on approaches

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
90%	Pedestrian and Bicycle	35%	20 years
Notes:	This CM only applies to "Ped & Bike" crashes occurring on the approaches/influence area of the new pedestrian median fencing.		
General information			
<b>Where to use:</b>			
Signalized Intersections with high pedestrian-generators nearby (e.g. transit stops) may experience a high volumes of pedestrians J-walking across the travel lanes at mid-block locations instead of walking to the intersection and waiting to cross during the walk-phase. When this safety issue cannot be mitigated with signal timing and shoulder/sidewalk treatments, then installing a continuous pedestrian barrier in the median may be a viable solution.			
<b>Why it works:</b>			
Adding pedestrian median fencing has the opportunity to enhance pedestrian safety at locations noted as being problematic involving pedestrians running/darting across the roadway outside the intersection crossings. Pedestrian median fencing can significantly reduce this safety issue by creating a positive barrier, forcing pedestrians to the designated pedestrian crossing.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Costs associated with this strategy will vary widely depending on the type and placement of the median fencing. Impacts to transit and other land uses may need to be considered and controversy can delay the implementation. In general, this CM can be effective as a spot-location approach.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian, Bicycle	CRF: 25- 40%

### S14, Create directional median openings to allow (and restrict) left-turns and U-turns (S.I.)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
90%	All	50%	20 years
Notes:	This CM only applies to crashes occurring in the intersection / influence area of the new directional openings.		
General information			
<b>Where to use:</b>			
Crashes related to turning maneuvers include angle, rear-end, pedestrian, and sideswipe (involving opposing left turns) type crashes. If any of these crash types are an issue at an intersection, restriction or elimination of the turning maneuver may be the best way to improve the safety of the intersection.			
<b>Why it works:</b>			
Restricting turning movement into and out of an intersection can help reduce conflicts between through and turning traffic. The number of access points, coupled with the speed differential between vehicles traveling along the roadway, contributes to crashes. Affecting turning movements by either allowing them or restricting them, based on the application, can ensure safe movement of traffic.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Turn prohibitions that are implemented by closing a median opening can be implemented quickly. The cost of this strategy will depend on the treatment. Impacts to businesses and other land uses must be considered and controversy can delay the implementation. In general, This CM can be very effective and can be considered on a systematic approach.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	All	CRF: 51%

### S20PB, Install advance stop bar before crosswalk (Bicycle Box)

For HSIP Calls-for-projects				
Funding Eligibility	Crash Types Addressed	CRF	Expected Life	
100%	Pedestrian and Bicycle	15%	10 years	
Notes:	This CM only applies to "Ped & Bike" crashes occurring in the intersection-crossing with the new advanced stop bars.			
General information				
<b>Where to use:</b>				
Signalized Intersections with a marked crossing, where significant bicycle and/or pedestrians volumes are known to occur.				
<b>Why it works:</b>				
Adding advance stop bar before the striped crosswalk has the opportunity to enhance both pedestrian and bicycle safety. Stopping cars well before the crosswalk provides a buffer between the vehicles and the crossing pedestrians. It also allows for a dedicated space for cyclists, making them more visible to drivers (This dedicated space is often referred to as a bike-box.)				
<b>General Qualities (Time, Cost and Effectiveness):</b>				
Costs and time of installation will vary based on the number of intersections included in this strategy and if it requires new signal controllers capable of accommodating the enhancement. When considered at a single location, these low cost improvements are usually funded through local funding by local crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate cost projects that are more appropriate to seek state or federal funding.				
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian, Bicycle	CRF:	35%

### S21PB, Modify signal phasing to implement a Leading Pedestrian Interval (LPI)

For HSIP Calls-for-projects				
Funding Eligibility	Crash Types Addressed	CRF	Expected Life	
100%	Pedestrian and Bicycle	60%	10 years	
Notes:	This CM only applies to "Ped & Bike" crashes occurring in the intersections with signalized pedestrian crossing with the newly implemented Leading Pedestrian Interval (LPI).			
General information				
<b>Where to use:</b>				
Intersections with signalized pedestrian crossing that have high turning vehicles volumes and have had pedestrian vs. vehicle crashes.				
<b>Why it works:</b>				
A leading pedestrian interval (LPI) gives pedestrians the opportunity to enter an intersection 3-7 seconds before vehicles are given a green indication. With this head start, pedestrians can better establish their presence in the crosswalk before vehicles have priority to turn left. LPIs provide (1) increased visibility of crossing pedestrians; (2) reduced conflicts between pedestrians and vehicles; (3) Increased likelihood of motorists yielding to pedestrians; and (4) enhanced safety for pedestrians who may be slower to start into the intersection.				
<b>General Qualities (Time, Cost and Effectiveness):</b>				
Costs for implementing LPIs are very low, since only minor signal timing alteration is required. This makes it an easy and inexpensive countermeasure that can be incorporated into pedestrian safety action plans or policies and can become routine agency practice. When considered at a single location, the LPI is usually local-funded. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate cost projects that are more appropriate to seek state or federal funding.				
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian, Bicycle	CRF:	59%

## B.2 Intersection Countermeasures – Non-signalized

### NS01, Add intersection lighting (NS.I.)

For HSIP Calls-for-projects				
Funding Eligibility	Crash Types Addressed		CRF	Expected Life
100%	Night		40%	20 years
Notes:	This CM only applies to "night" crashes (all types) occurring within limits of the proposed roadway lighting 'engineered' area.			
General information				
<b>Where to use:</b>				
Non-signalized intersections that have a disproportionate number of night-time crashes and do not currently provide lighting at the intersection or at its approaches. Crash data should be studied to ensure that safety at the intersection could be improved by providing lighting (this strategy would be supported by a significant number of crashes that occur at night).				
<b>Why it works:</b>				
Providing lighting at the intersection itself, or both at the intersection and on its approaches, improves the safety of an intersection during nighttime conditions by (1) making drivers more aware of the surroundings at an intersection, which improves drivers' perception-reaction times, (2) enhancing drivers' available sight distances, and (3) improving the visibility of non-motorists. Intersection lighting is of particular benefit to non-motorized users as lighting not only helps them navigate the intersection, but also helps drivers see them better.				
<b>General Qualities (Time, Cost and Effectiveness):</b>				
A lighting project can usually be completed relatively quickly, but generally requires at least 1 year to implement because the lighting system must be designed and the provision of electrical power must be arranged. The provision of lighting involves both a fixed cost for lighting installation and an ongoing maintenance and power cost. For rural intersections, studies have shown the installation of streetlights reduced nighttime crashes at unlit intersections and can be more effective in reducing nighttime crashes than either rumble strips or overhead flashing beacons. Some locations can result in high B/C ratios, but due to higher costs, these projects often result in medium to low B/C ratios.				
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Night, All	CRF:	25- 50%

### NS02, Convert to all-way STOP control (from 2-way or Yield control)

For HSIP Calls-for-projects				
Funding Eligibility	Crash Types Addressed		CRF	Expected Life
100%	All		50%	10 years
Notes:	This CM only applies to crashes occurring in the intersection and/or influence area of the new control. CA-MUTCD warrant must be met.			
General information				
<b>Where to use:</b>				
Unsignalized intersection locations that have a crash history and have no controls on the major roadway approaches. However, all-way stop control is suitable only at intersections with moderate and relatively balanced volume levels on the intersection approaches. Under other conditions, the use of all-way stop control may create unnecessary delays and aggressive driver behavior. MUTCD warrants should always be followed.				
<b>Why it works:</b>				
All-way stop control can reduce right-angle and turning collisions at unsignalized intersections by providing more orderly movement at an intersection, reducing through and turning speeds, and minimizing the safety effect of any sight distance restrictions that may be present. Advance public notification of the change is critical in assuring compliance and reducing crashes.				
<b>General Qualities (Time, Cost and Effectiveness):</b>				
The costs involved in converting to all-way stop control are relatively low. All-way stop control can normally be implemented at multiple intersections with just a change in signing on intersection approaches, and typically are very quick to implement. When considered at a single location, these low cost improvements are usually funded through local funding by local maintenance crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate cost projects that are more appropriate to seek state or federal funding.				
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Left-turn, Angle	CRF:	6 - 80%

### NS05, Convert intersection to roundabout (from 2-way stop or Yield control)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	Varies	20 years
Notes:	This CM only applies to crashes occurring in the intersection and/or influence area of the new control. The benefit of this CM is calculated using Caltrans procedure. The CRF is dependent on the ADT, project location (Rural/Urban) and the roundabout type (1 lane or 2 lanes). The benefit comes from both the reduction in the number and the severity of the crashes.		
General information			
<b>Where to use:</b>			
Intersections that have a high frequency of right-angle and left-turn type crashes. Whether such intersections have existing crash patterns or not, a roundabout provides an alternative to signalization. The primary target locations for roundabouts should be moderate-volume unsignalized intersections. Roundabouts may not be a viable alternative in many suburban and urban settings where right-of-way is limited.			
<b>Why it works:</b>			
Roundabouts provide an important alternative to signalized and all-way stop-controlled intersections. Modern roundabouts differ from traditional traffic circles in that they operate in such a manner that traffic entering the roundabout must yield the right-of-way to traffic already in it. Roundabouts can serve moderate traffic volumes with less delay than all-way stop-controlled intersections and provide fewer conflict points. Crashes at roundabouts tend to be less severe because of the speed constraints and elimination of left-turn and right-angle movements.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Construction of roundabouts are usually relatively costly and major projects, requiring the environmental process, right-of-way acquisition, and implementation under an agency's long-term capital improvement program. (For this reason, roundabouts may not be appropriate for California's Federal Safety Programs that have relatively short delivery requirements.) Even with roundabouts higher costs, they still can have a relatively high effectiveness.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Left-turn, Angle	CRF: 12 - 78 %

### NS06, Install/upgrade larger or additional stop signs or other intersection warning/regulatory signs

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	15%	10 years
Notes:	This CM only applies to crashes occurring in the influence area of the new signs. The influence area must be determined on a location by location basis.		
General information			
<b>Where to use:</b>			
The target for this strategy should be approaches to unsignalized intersections with patterns of rear-end, right-angle, or turning collisions related to lack of driver awareness of the presence of the intersection.			
<b>Why it works:</b>			
The visibility of intersections and, thus, the ability of approaching drivers to perceive them can be enhanced by installing larger regulatory and warning signs at or prior to intersections. A key to success in applying this strategy is to select a combination of regulatory and warning sign techniques appropriate for the conditions on a particular unsignalized intersection approach.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Signing improvements do not require a long development process and can typically be implemented quickly. Costs for implementing this strategy are nominal and depend on the number of signs. When considered at a single location, these low cost improvements are usually funded through local funding by local maintenance crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate cost projects that are more appropriate to seek state or federal funding.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	All	CRF: 11 - 55%

### NS07, Upgrade intersection pavement markings (NS.I.)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	25%	10 years
Notes:	This CM only applies to crashes occurring on the approaches / influence area of the new pavement markings. This CM is not intended to be used for general maintenance activities (i.e. the replacement of existing pavement markings in-kind) and must include upgraded safety features over the existing pavement markings and striping.		
General information			
<b>Where to use:</b>			
Unsignalized intersections that are not clearly visible to approaching motorists, particularly approaching motorists on the major road. The strategy is particularly appropriate for intersections with patterns of rear-end, right-angle, or turning crashes related to lack of driver awareness of the presence of the intersection. Also at minor road approaches where conditions allow the stop bar to be seen by an approaching driver at a significant distance from the intersection. Typical improvements include "Stop Ahead" markings and the addition of Centerlines and Stop Bars.			
<b>Why it works:</b>			
The visibility of intersections and, thus, the ability of approaching drivers to perceive them can be enhanced by installing appropriate pavement delineation in advance of and at intersections will provide approaching motorists with additional information at these locations. Providing visible stop bars on minor road approaches to unsignalized intersections can help direct the attention of drivers to the presence of the intersection. Drivers should be more aware that the intersection is coming up, and therefore make safer decisions as they approach the intersection.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Pavement marking improvements do not require a long development process and can typically be implemented quickly. Costs for implementing this strategy are nominal and depend on the number of markings. When considered at a single location, these low cost improvements are usually funded through local funding by local maintenance crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate cost projects that are more appropriate to seek state or federal funding. Note: When federal safety funding is used for these installations in high-wear-locations, the local agency is expected to maintain the improvement for a minimum of 10 years.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	All	CRF: 13 - 60%

### NS08, Install Flashing Beacons at Stop-Controlled Intersections

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	15%	10 years
Notes:	This CM only applies to crashes occurring on the stop-controlled approaches / influence area of the new beacons.		
General information			
<b>Where to use:</b>			
Flashing beacons can reinforce driver awareness of the Non-Signalized intersection control and can help mitigate patterns of right-angle crashes related to stop sign violations. Post-mounted advanced flashing beacons or overhead flashing beacons can be used at stop-controlled intersections to supplement and call driver attention to stop signs.			
<b>Why it works:</b>			
Flashing beacons provide a visible signal to the presence of an intersection and can be very effective in rural areas where there may be long stretches between intersections as well as locations where night-time visibility of intersections is an issue.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Flashing beacons can be constructed with minimal design, environmental and right-of-way issues and have relatively low costs. Before choosing this CM, the agency needs to confirm the ability to provide power to the site (solar may be an option). In general, This CM can be very effective and can be considered on a systematic approach.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Angle, Rear-End	CRF: 5-34%

## NS19PB, Install raised medians (refuge islands)

For HSIP Calls-for-projects					
Funding Eligibility		Crash Types Addressed		CRF	Expected Life
90%		Pedestrian and Bicycle		45%	20 years
Notes:	This CM only applies to "Ped & Bike" crashes occurring in the crossing with the new islands. All new raised medians funded with federal HSIP funding must not include the removal of the existing roadway structural section and must be doweled into the existing roadway surface. This new requirement is being implemented to maximize the safety-effectiveness of the limited HSIP funding and to minimize project impacts.				
General information					
<b>Where to use:</b>					
Intersections that have a long pedestrian crossing distance, a higher number of pedestrians, or a crash history. Raised medians decrease the level of exposure for pedestrians and allow pedestrians to concentrate on (or cross) only one direction of traffic at a time.					
<b>Why it works:</b>					
Raised pedestrian refuge islands, or medians at crossing locations along roadways, are another strategy to reduce exposure between pedestrians and motor vehicles. Refuge islands and medians that are raised (i.e., not just painted) provide pedestrians more secure places of refuge during the street crossing. They can stop partway across the street and wait for an adequate gap in traffic before completing their crossing.					
<b>General Qualities (Time, Cost and Effectiveness):</b>					
Median and pedestrian refuge areas are a low-cost countermeasure to implement. This cost can be applied to retrofit improvements or if it is a new construction project, implementing this countermeasure is even more cost-effective. In general, This CM can be very effective and can be considered on a systematic approach. When agencies opt to install landscaping in conjunction with new raised medians, the portion of the cost for landscaping and other non-safety related items that exceeds 10% of the project total cost is not federally participated and must be funded by the applicant.					
<b>FHWA CMF Clearinghouse:</b>		<b>Crash Types Addressed:</b>		<b>CRF:</b>	<b>30 - 56 %</b>

## NS20PB, Install pedestrian crossing at uncontrolled locations (signs and markings only)

For HSIP Calls-for-projects					
Funding Eligibility		Crash Types Addressed		CRF	Expected Life
100%		Pedestrian and Bicycle		25%	10 years
Notes:	This CM only applies to "Ped & Bike" crashes occurring in the intersection/crossing with the new crossing. This CM is not intended to be used for high-cost aesthetic enhancements to intersection crosswalks (i.e. stamped concrete or stamped asphalt).				
General information					
<b>Where to use:</b>					
Non-signalized intersections without a marked crossing, where pedestrians are known to be crossing intersections that involve significant vehicular traffic. They are especially important at school crossings and intersections with right and/or left turns pockets. See Zegeer study (Safety Effects of Marked vs. Unmarked Crosswalks at Uncontrolled Locations) for additional guidance regarding when to install a marked crosswalk.					
<b>Why it works:</b>					
Adding pedestrian crossings has the opportunity to enhance pedestrian safety at locations noted as being problematic. Pavement markings delineate a portion of the roadway that is designated for pedestrian crossing. These markings will often be different for controlled verses uncontrolled locations. The use of "ladder", "zebra" or other enhanced markings at uncontrolled crossings can increase both pedestrian and driver awareness to the increased exposure at the crossing. Incorporating advanced "stop" or "yield" markings provides an extra safety buffer and can be effective in reducing the 'multiple-threat' danger to pedestrians. Nearly one-third of all pedestrian-related crashes occur at or within 50 feet of an intersection. Of these, 30 percent may involve a turning vehicle. There are several types of pedestrian crosswalks, including: continental, ladder, zebra, and standard. When agencies opt to install aesthetic enhancement to intersection crosswalks like stamped concrete/asphalt, the project design and construction costs can significantly increase. For HSIP applications, these costs must be accounted for in the B/C calculation, but these costs (over standard crosswalk markings) must be tracked separately and are not federally reimbursable and will increase the agency's local-funding share for the project costs.					
<b>General Qualities (Time, Cost and Effectiveness):</b>					
Costs associated with this strategy will vary widely, depending upon if curb ramps and sidewalk modifications are required with the crossing. When considered at a single location, these low cost improvements are usually funded through local funding by local crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate cost projects that are more appropriate to seek state or federal funding.					
<b>FHWA CMF Clearinghouse:</b>		<b>Crash Types Addressed:</b>		<b>CRF:</b>	<b>25 %</b>

NS21PB, Install/upgrade pedestrian crossing at uncontrolled locations (with enhanced safety features)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	Pedestrian and Bicycle	35%	20 years
Notes:	This CM only applies to "Ped & Bike" crashes occurring in the new crossing (influence area) with enhanced safety features. This CM is not intended to be used for high-cost aesthetic enhancements to intersection crosswalks (i.e. stamped concrete or stamped asphalt).		
General information			
<b>Where to use:</b>			
Non-signalized intersections where pedestrians are known to be crossing intersections that involve significant vehicular traffic. They are especially important at school crossings and intersections with turn pockets. Based on the Zegeer study (Safety Effects of Marked vs. Unmarked Crosswalks at Uncontrolled Locations) at many locations, a marked crosswalk alone may not be sufficient to adequately protect non-motorized users. In these cases, <b>flashing beacons, curb extensions, advanced "stop" or "yield" markings, and other safety features</b> should be added to complement the standard crossing elements.			
<b>Why it works:</b>			
Adding pedestrian crossings that include enhanced safety features has the opportunity to enhance pedestrian safety at locations noted as being especially problematic. The enhanced safety elements help delineate a portion of the roadway that is designated for pedestrian crossing. Incorporating advanced "yield" markings provide an extra safety buffer and can be effective in reducing the 'multiple-threat' danger to pedestrians. Nearly one-third of all pedestrian-related crashes occur at or within 50 feet of an intersection. When agencies opt to install aesthetic enhancement to intersection crosswalks like stamped concrete/asphalt, the project design and construction costs can significantly increase. For HSIP applications, these costs must be accounted for in the B/C calculation, but these costs (over standard crosswalk markings) must be tracked separately and are not federally reimbursable and will increase the agency's local-funding share for the project costs.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Costs associated with this strategy will vary widely, depending upon the types of enhanced features that will be combined with the standard crossing improvements. The need for new curb ramps and sidewalk modifications will also be a factor. This CM may be effectively and efficiently implemented using a systematic approach with more than one location and can have relatively high B/C ratios based on past non-motorized crash history.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian and Bicycle	CRF: 37%

NS22PB, Install Rectangular Rapid Flashing Beacon (RRFB)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	Pedestrian and Bicycle	35%	20 years
Notes:	This CM only applies to "Ped & Bike" crashes occurring in the influence area (expected to be a maximum of within 250') of the crossing which includes the RRFB.		
General information			
<b>Where to use:</b>			
Rectangular Rapid Flashing Beacon (RRFB) includes pedestrian-activated flashing lights and additional signage that enhance the visibility of marked crosswalks and alert motorists to pedestrian crossings. It uses an irregular flash pattern that is similar to emergency flashers on police vehicles. RRFBs are installed at unsignalized intersections and mid-block pedestrian crossings.			
<b>Why it works:</b>			
RRFBs can enhance safety by increasing driver awareness of potential pedestrian conflicts and reducing crashes between vehicles and pedestrians at unsignalized intersections and mid-block pedestrian crossings. The addition of RRFB may also increase the safety effectiveness of other treatments, such as crossing warning signs and markings.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
RRFBs are a lower cost alternative to traffic signals and hybrid signals. This CM can often be effectively and efficiently implemented using a systematic approach with numerous locations.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian, Bicycle	CRF: 7 – 47.4%

## B.3 Roadway Countermeasures

### R01, Add Segment Lighting

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	Night	35%	20 years
Notes:	This CM only applies to "night" crashes (all types) occurring within limits of the proposed roadway lighting 'engineered' area.		
General information			
<b>Where to use:</b>			
Where to use: Noted substantial patterns of nighttime crashes. In particular, patterns of rear-end, right-angle, turning or roadway departure collisions on the roadways may indicate that night-time drivers can be unaware of the roadway characteristics.			
<b>Why it works:</b>			
Providing roadway lighting improves the safety during nighttime conditions by (1) making drivers more aware of the surroundings, which improves drivers' perception-reaction times, (2) enhancing drivers' available sight distances to perceive roadway characteristic in advance of the change, and (3) improving non-motorist's visibility and navigation.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
It expected that projects of this type may be constructed in a year or two and are relatively costly. There are several types of costs associated with providing lighting, including the cost of providing a permanent source of power to the location, the cost for the luminaire supports (i.e., poles), and the cost for routinely replacing the bulbs and maintenance of the luminaire supports. Some locations can result in high B/C ratios, but due to higher costs, these projects often result in medium to low B/C ratios.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Night, All	CRF: 18 - 69 %

### R02, Remove or relocate fixed objects outside of Clear Recovery Zone

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
90%	All	35%	20 years
Notes:	This CM only applies to crashes occurring within the limits of the new clear recovery zone (per Caltrans' HDM).		
General information			
<b>Where to use:</b>			
Known locations or roadway segments prone to collisions with fixed objects such as utility poles, drainage structures, trees, and other fixed objects, such as the outside of a curve, end of lane drops, and in traffic islands. A clear recovery zone should be developed on every roadway, as space is available. In situations where public right-of-way is limited, steps should be taken to request assistance from property owners, as appropriate.			
<b>Why it works:</b>			
While this strategy does not prevent the vehicle leaving the roadway, it does provide a mechanism to reduce the severity of a resulting crash. A clear zone is an unobstructed, traversable roadside area that allows a driver to stop safely or regain control of a vehicle that has left the roadway. Removing or moving fixed objects, flattening slopes, or providing recovery areas reduces the likelihood of a crash.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Projects involving removing fixed objects from highway right-of-way can typically be accomplished quickly, assuming the objects are readily moveable. Clearing objects on private property requires more time for discussions with the property owner. Costs will generally be low, assuming that in most cases the objects to be removed are within the right-of-way. This CMs can be very effective and can be implemented by agencies' maintenance staff and/or implemented on a systematic approach. High-cost removals or removals implemented using a systematic approach would be good candidates for Caltrans Federal Safety Funding.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Fixed Object	CRF: 17 - 100 %

## R20, Convert from two-way to one-way traffic

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
90%	All	35%	20 years
Notes:	This CM only applies to crashes occurring within the limits of the new one-way sections.		
General information			
<b>Where to use:</b>			
One-way streets can offer improved signal timing and accommodate odd-spaced signals. One-way streets can simplify crossings for pedestrians, who must look for traffic in only one direction. While studies have shown that conversion of two-way streets to one-way generally reduces pedestrian crashes and the number of conflict points, one-way streets tend to have higher speeds which creates new problems. Care must be taken not to create conditions that cause driver confusion and erratic maneuvers.			
<b>Why it works:</b>			
Studies have shown a 10 to 50-percent reduction in total crashes after conversion of a two-way street to one-way operation. While studies have shown that conversion of two-way streets to one-way generally reduces pedestrian crashes, one-way streets tend to have higher speeds which creates new problems. At the same time, this strategy (1) increases capacity significantly and (2) can have safety-related drawbacks including pedestrian confusion and minor sideswipe crashes.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
The costs will vary depending on length of treatment and if the conversion requires modification to signals. Conversion costs can be high to build "crossovers" where the one-way streets convert back to two-way streets and to rebuild traffic signals. It's also likely that these types of modifications will require public involvement and could significantly add to the time it takes to complete the project. The expected effectiveness of this CM must be assessed for each individual location.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	All	CRF: 26 - 43 %

## R21, Improve pavement friction (High Friction Surface Treatments)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	55%	10 years
Notes:	This CM only applies to crashes occurring within the limits of the improved friction overlay. This CM is not intended to apply to standard chip-seal or open-graded <b>maintenance</b> projects for long segments of corridors or structure repaving projects intended to fix failed pavement.		
General information			
<b>Where to use:</b>			
Nationally, this countermeasure is referred to as "High Friction Surface Treatments" or HFST. Areas as noted having crashes on wet pavements or under dry conditions when the pavement friction available is significantly less than actual roadway speeds; including but not limited to curves, loop ramps, intersections, and areas with short stopping or weaving distances. This treatment is intended to target locations where skidding is determined to be a problem, in wet or dry conditions and the target vehicle is one that runs (skids) off the road or is unable to stop due to insufficient skid resistance.			
<b>Why it works:</b>			
Improving the skid resistance at locations with high frequencies of wet-road crashes and/or failure to stop crashes can result in a reduction of 50 percent for wet-road crashes and 20 percent for total crashes. Applying HFST can double friction numbers, e.g. low 40s to high 80s. This CM represents a special focus area for both FHWA and Caltrans, which means there are extra resources available for agencies interested in more details on High Friction Surface Treatment projects.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
This strategy can be relatively inexpensive and implemented in a short timeframe. The installation would be done by either agency personnel or contractors and can be done by hand or machine. In general, This CM can be very effective and can be considered on a systematic approach.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Wet, Rear-End, All	CRF: 17 - 68 %

R22, Install/Upgrade signs with new fluorescent sheeting (regulatory or warning)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	15%	10 years
Notes:	This CM only applies to crashes occurring within the influence area of the new/upgraded signs. This CM is not intended for maintenance upgrades of street-name, parking, guide, or any other signs without a primary focus on roadway safety. <b>This CM is not eligible unless</b> it is done as part of a larger sign audit project, including the study of: 1) the existing signs' locations, sizes and information per MUTCD standards, 2) missing signs per MUTCD standards, and 3) sign retroreflectivity. The overall sign audit scope (or a special exception from the HSIP program manager) must be documented in the Narrative Questions in the application. Based on the scope of the project/audit, it may be appropriate to combine other CMs in the B/C calculation.		
General information			
<b>Where to use:</b>			
The target for this strategy should be on roadway segments with patterns of head on, nighttime, non-intersection, run-off road, and sideswipe crashes related to lack of driver awareness of the presence of a specific roadway feature or regulatory requirement. Ideally this type of safety CM would be combined with other sign evaluations and upgrades (install chevrons, warning signs, delineators, markers, beacons, and relocation of existing signs per MUTCD standards.)			
<b>Why it works:</b>			
This strategy primarily addresses crashes caused by lack of driver awareness (or compliance) roadway signing. It is intended to get the drivers attention and give them a visual warning by using fluorescent yellow sheeting (or other retroreflective material).			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Signing improvements do not require a long development process and can typically be implemented quickly. Costs for implementing this strategy are nominal and depend on the number of signs. When considered at a single location, these low cost improvements are usually funded through local funding by local maintenance crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate cost projects that are more appropriate to seek state or federal funding. When considering any type of federally funded sign upgrade project, California local agencies are encouraged to consider "Roadway Safety Signing Audit (RSSA) and Upgrade Projects". Including RSSAs in the development phase of sign projects are expected to identify non-standard (per MUTCD) sign features and missing signs that may otherwise go unnoticed. More information on RSSA is available on the Local Assistance HSIP webpage.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Head on, Run-off road, Sideswipe, Night	CRF: 18 - 35%

R27, Install delineators, reflectors and/or object markers

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	15%	10 years
Notes:	This CM only applies to crashes occurring within the limits / influence area of the new features. <b>{This is not a striping-related CM}</b>		
General information			
<b>Where to use:</b>			
Roadways that have an unacceptable level of crashes on curves (relatively flat to sharp) during periods of light and darkness. Any road with a history of fixed object crashes is a candidate for this treatment, as are roadways with similar fixed objects along the roadside that have yet to experience crashes. If a fixed object cannot be relocated or made break-away, placing an object marker can provide additional information to motorists. Ideally this type of safety CM would be combined with other sign evaluations and upgrades (install warning signs, chevrons, beacons, and relocation of existing signs per MUTCD standards.)			
<b>Why it works:</b>			
Delineators, reflectors and/or object markers are intended to warn drivers of an approaching curve or fixed object that cannot easily be removed. They are intended to provide tracking information and guidance to the drivers. They are generally less costly than Chevron Signs as they don't require posts to place along the roadside, avoiding an additional object with which an errant vehicle can crash into.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
These improvements do not require a long development process and can typically be implemented quickly. Costs for implementing this strategy are nominal and depend on the number of locations. When considered at a single location, these low cost improvements are usually funded through local funding by local maintenance crews. However, This CM can be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in low to moderate cost projects that are more appropriate to seek state or federal funding. When considering any type of federally funded sign upgrade project, California local agencies are encouraged to consider "Roadway Safety Signing Audit (RSSA) and Upgrade Projects". Including RSSAs in the development phase of sign projects are expected to identify non-standard (per MUTCD) sign features and missing signs that may otherwise go unnoticed. More information on RSSA is available on the Local Assistance HSIP webpage.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	All	CRF: 0 - 30 %

## R28, Install edge-lines and centerlines

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
100%	All	25%	10 years
<b>Notes:</b>	This CM only applies to crashes occurring within the limits of the new centerlines and/or edge-lines. This CM is not intended to be used for general maintenance activities (i.e. the replacement of existing striping and RPMs in-kind) and must include upgraded safety features over the existing striping. For two lane roadways allowing passing, a striping audit must be done to ensure the passing limits meeting the MUTCD standards. Both the centerline and edge-lines are expected to be upgraded, unless prior approval is granted by Caltrans staff in writing and attached to application.		
General information			
<b>Where to use:</b>			
Any road with a history of run-off-road right, head-on, opposite-direction-sideswipe, or run-off-road-left crashes is a candidate for this treatment - install where the existing lane delineation is not sufficient to assist the motorist in understanding the existing limits of the roadway. Depending on the width of the roadway, various combinations of edge line and/or center line pavement markings may be the most appropriate. Incorporating raised/reflective pavement markers (RPMs) into centerlines (and edge-lines) should be considered as it has been shown to improve safety.			
<b>Why it works:</b>			
Installing edge-lines and centerlines where none exists or making significant upgrades to existing lines (paint to thermoplastic, adding audible disks/bumps in the thermoplastic stripes, or adding RPMs) are intended/designed to help drivers who might leave the roadway because of their inability to see the edge of the roadway along the horizontal edge of the pavement or cross-over the centerline of the roadway into oncoming traffic. New pavement marking products tend to be more durable, are all-weather, more visible, and have a higher retroreflectivity than traditional pavement markings.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
These improvements do not require a long development process and can typically be implemented quickly. Costs for implementing this strategy are nominal and depend on the number and length of locations. This CM can be effectively and efficiently implemented using a systematic approach with numerous and long locations, resulting in low to moderate cost projects that are more appropriate to seek state or federal funding. When considering any type of federally funded striping upgrade project, California local agencies are encouraged to consider "Roadway Safety Striping Audit and Upgrade Projects". Including wide-scale striping audits in the development phase of striping projects are expected to identify non-standard (per MUTCD) striping/markings features, no-passing zone limits needing adjustment, and missing striping/markings that may otherwise go unnoticed. More information on this concepts is available on the Local Assistance HSIP webpage under an RSSA example document. Note: When federal safety funding is used for these installations in high-wear-locations, the local agency is expected to maintain the improvement for a minimum of 10 years.			
<b>FHWA CMF Clearinghouse:</b>	<b>Crash Types Addressed:</b>	Head-on, Run-off Road, All	<b>CRF:</b> 0 - 44 %

### R33PB, Install Separated Bike Lanes

For HSIP Calls-for-projects					
Funding Eligibility		Crash Types Addressed		CRF	Expected Life
90%		Pedestrian and Bicycle		45%	20 years
Notes:	This CM only applies to "Ped & Bike" crashes occurring within the limits of the separated bike lanes. When an off-street bike-path is proposed that is not adjacent to the roadway, the applicant must document the engineering judgment used to determine which "Ped & Bike" crashes to apply.				
General information					
<b>Where to use:</b>					
Separated bikeways are most appropriate on streets with high volumes of bike traffic and/or high bike-vehicle collisions, presumably in an urban or suburban area. Separation types range from simple, painted buffers and flexible delineators, to more substantial separation measures including raised curbs, grade separation, bollards, planters, and parking lanes. These options range in feasibility due to roadway characteristics, available space, and cost. In some cases, it may be possible to provide additional space in areas where pedestrian and bicyclists may interact, such as the parking buffer, or loading zones, or extra bike lane width for cyclists to pass one another.					
<b>Why it works:</b>					
Separated bike lanes provide increased safety and comfort for bicyclists beyond conventional bicycle lanes. By separating bicyclists from motor traffic, "protected" or physically separated bike lanes can offer a higher level of comfort and are attractive to a wider spectrum of the public. Intersections and approaches must be carefully designed to promote safety and facilitate left-turns for bicyclists from the primary corridor to cross street. In combination with this CM, better guidance signs and markings for non-motorized and motorized roadway users should be considered, including: sign and markings directing cyclists on appropriate/legal travel paths and signs and markings warning motorists of non-motorized uses of the roadway that should be expected.					
<b>General Qualities (Time, Cost and Effectiveness):</b>					
The cost of Installing separated bike lanes can be low to medium or high, depending on whether roadway widening, right-of-way and environmental impacts are involved. It is most cost efficient to create bike lanes during street reconstruction, street resurfacing, or at the time of original construction. The expected effectiveness of this CM must be assessed for each individual location.					
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian, Bicycle	CRF:	3.7 - 100 %	

### R34PB, Install sidewalk/pathway (to avoid walking along roadway)

For HSIP Calls-for-projects					
Funding Eligibility		Crash Types Addressed		CRF	Expected Life
90%		Pedestrian and Bicycle		80%	20 years
Notes:	This CM only applies to "Ped & Bike" crashes occurring within the limits of the new walkway. This CM is not intended to be used where an existing sidewalk is being replaced with a wider one, unless prior Caltrans approval is included in the application. When an off-street multi-use path is proposed that is not adjacent to the roadway, the applicant must document the engineering judgment used to determine which "Ped & Bike" crashes to apply.				
General information					
<b>Where to use:</b>					
Areas noted as not having adequate or no sidewalks and a history of walking along roadway pedestrian crashes. In rural areas asphalt curbs and/or separated walkways may be appropriate.					
<b>Why it works:</b>					
Sidewalks and walkways provide people with space to travel within the public right-of-way that is separated from roadway vehicles. The presence of sidewalks on both sides of the street has been found to be related to significant reductions in the "walking along roadway" pedestrian crash risk compared to locations where no sidewalks or walkways exist. Reductions of 50 to 90 percent of these types of pedestrian crashes. In combination with this CM, better guidance signs and markings for non-motorized and motorized roadway users should be considered, including: sign and markings directing pedestrians and cyclists on appropriate/legal travel paths and signs and markings warning motorists of non-motorized uses of the roadway that should be expected.					
<b>General Qualities (Time, Cost and Effectiveness):</b>					
Costs for sidewalks will vary, depending upon factors such as width, materials, and existing of curb, gutter and drainage. Asphalt curbs and walkways are less expensive, but require more maintenance. The expected effectiveness of this CM must be assessed for each individual location. These projects can be very effective in areas of high-pedestrian volumes with a past history of crashes involving pedestrians.					
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian, Bicycle	CRF:	65 - 89 %	

R35PB, Install/upgrade pedestrian crossing (with enhanced safety features)

For HSIP Calls-for-projects			
Funding Eligibility	Crash Types Addressed	CRF	Expected Life
90%	Pedestrian and Bicycle	35%	20 years
<b>Notes:</b>	This CM only applies to "Ped & Bike" crashes occurring in the influence area (expected to be a maximum of within 250') of the new crossing which includes new enhanced safety features. Note: This CM is not intended to be combined with the "Install raised pedestrian crossing" when calculating the improvement's B/C ratio. This CM is not intended to be used for high-cost aesthetic enhancements (i.e. stamped concrete or stamped asphalt).		
General information			
<b>Where to use:</b>			
Roadway segments with no controlled crossing for a significant distance in high-use midblock crossing areas and/or multilane roads locations. Based on the Zegeer study (Safety Effects of Marked vs. Unmarked Crosswalks at Uncontrolled Locations) at many locations, a marked crosswalk alone may not be sufficient to adequately protect non-motorized users. In these cases, flashing beacons, curb extensions, medians and pedestrian crossing islands and/or other safety features should be added to complement the standard crossing elements. For multi-lane roadways, advance "yield" markings can be effective in reducing the 'multiple-threat' danger to pedestrians.			
<b>Why it works:</b>			
Adding pedestrian crossings has the opportunity to greatly enhance pedestrian safety at locations noted as being problematic. The enhanced safety elements, which may include curb extensions, medians and pedestrian crossing islands, beacons, and lighting, combined with pavement markings delineating a portion of the roadway that is designated for pedestrian crossing. Care must be taken to warn drivers of the potential for pedestrians crossing the roadway and enhanced improvements added to the crossing increase the likelihood of pedestrians crossing in a safe manner. In combination with this CM, better guidance signs and markings for non-motorized and motorized roadway users should be considered, including: sign and markings directing pedestrians and cyclists on appropriate/legal travel paths and signs. When agencies opt to install aesthetic enhancement to crossing like stamped concrete/asphalt, the project design and construction costs can significantly increase. For HSIP applications, these costs must be accounted for in the B/C calculation, but these costs (over standard crosswalk markings) must be tracked separately and are not federally reimbursable and will increase the agency's local-funding share for the project costs.			
<b>General Qualities (Time, Cost and Effectiveness):</b>			
Costs associated with this strategy will vary widely, depending on the extent of the curb extensions, raised medians, flashing beacons, and other pedestrian safety elements that are needed with the crossing. When considered at a single location, these improvements can sometimes be low cost and funded through local funding by local crews. This CM can often be effectively and efficiently implemented using a systematic approach with numerous locations, resulting in moderate to high cost projects that are appropriate to seek state or federal funding.			
<b>FHWA CMF Clearinghouse:</b>	Crash Types Addressed:	Pedestrian, Bicycle	CRF: 8 - 56%

City of Fort Bragg LRSP  
**CM Toolbox for Intersections**

Signalized						
Sr. No.	Code	Countermeasure Name	CM Description	CRF	Federal Funding	Systemic Approach Opportunity
HSIP/Non-HSIP Code						
1	S02	Improve signal hardware: lenses, back-plates with retroreflective borders, mounting, size, etc.	Includes new LED lighting, signal back plates, retro-reflective tape outlining the back of the signal.	15%	100%	Very High
2	S03	Improve signal timing (coordination, phases, red, yellow, or operation)	Includes adding phases, lengthening clearance intervals, eliminating or restricting higher-risk movements, and coordinating signals at multiple locations.	15%	50%	Very High
3	S08	Convert signal to mast arm (from pedestal-mounted)	Intersections currently controlled by pedestal mounted traffic signals (in medians and/or on outside shoulder) that have a high frequency of right-angle and rear-end crashes occurring because drivers are unable to see traffic signals in advance to safely negotiate the intersection.	30%	100%	Medium
4	S09	Install raised pavement markers and striping (Through Intersection)	Addition of clear pavement markings, raised pavement marking to help guide motorists through complex intersections.	10%	100%	Very High
5	S12	Install raised median on approaches (S.I.)	Addition of raised medians next to left-turn lanes at intersections, directly over existing pavement.	25%	90%	Medium
6	S17PB	Install pedestrian countdown signal heads	A pedestrian countdown signal contains a timer display and counts down the number of seconds left to finish crossing the street. Countdown signals can reassure pedestrians who are in the crosswalk when the flashing "DON'T WALK" interval appears that they still have time to finish crossing.	25%	100%	Very High
7	S21PB	Modify signal phasing to implement a Leading Pedestrian Interval (LPI)	Addition of LPI gives pedestrians the opportunity to enter an intersection 3-7 seconds before vehicles are given a green indication; only minor signal timing alteration is required.	60%	100%	Very High

Unsignalized						
Sr. No.	Code	Countermeasure Name	CM Description	CRF	Federal Funding	Systemic Approach Opportunity
1	NS03	Install signals	Installation of traffic signals	30%	100%	Low
2	NS06	Install/upgrade larger or additional stop signs or other intersection warning/regulatory	enhance the ability of approaching drivers to perceive them	15%	100%	Very High
3	NS07	Upgrade intersection pavement markings	perceive them can be enhanced by installing	25%	100%	Very High
4	NS17	Install right turn lane	Provision of exclusive right-turn lanes, particularly on high-volume and high-speed major-road approaches.	20%	90%	Low
5	NS21PB	features)	opportunity to enhance pedestrian safety at locations noted as being especially	35%	100%	Medium
6	NS22PB	Install Rectangular Rapid Flashing Beacon (RRFB)	Rectangular Rapid Flashing Beacon (RRFB) includes pedestrian-activated flashing lights and additional signage that enhance the visibility of marked crosswalks and alert motorists to pedestrian crossings	35%	100%	Medium

**CM Toolbox for Roadway Segments**

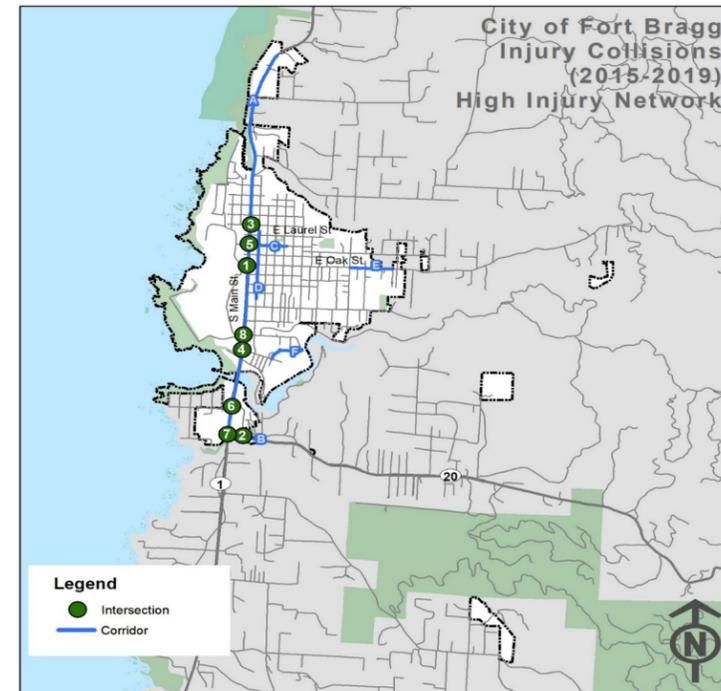
Sr. No.	Code	Countermeasure Name	CM Description	CRF	Federal Funding	Systemic Approach Opportunity
1	R22	Install/Upgrade signs with new fluorescent sheeting (regulatory or warning)	Additional or new signage can address crashes caused by lack of driver awareness or compliance of roadway signing.	15%	100%	Very High
2	R26	Install dynamic/variable speed warning signs	Includes the addition of dynamic regulatory signs (also known as Radar Speed Feedback Signs)	30%	100%	High
3	R34PB	Install sidewalk/pathway (to avoid walking along roadway)	Sidewalks and walkways provide people with space to travel within the public right-of-way that is separated from roadway vehicles.	80%	90%	Medium
4	R35PB	Install/upgrade pedestrian crossing (with enhanced safety features)	The enhanced safety elements, which may include curb extensions, medians and pedestrian crossing islands, beacons, and lighting, combined with pavement markings delineating a portion of the roadway that is designated for pedestrian crossing.	35%	90%	Medium
5	R36PB	Install raised pedestrian crossing	Ramped speed tables spanning the entire width of roadway or intersection	35%	90%	Medium

High-risk Intersections

ID	Intersection	Control	Consolidated CMs (HSIP-Eligible - Refer to LRSM* 2020)				Additional CM (non-HSIP)**	EA - 1 Improve Intersection Safety			EA - 2 Improve Pedestrian Safety			EA - 3 Reduce Improper Turning Collisions			EA - 4 Reduce Route 1 Collisions			EA - 5 Reduce Alley Way collisions		
			CM1	CM2	CM3	CM4		CM1	CM2	CM3	CM1	CM2	CM3	CM1	CM2	CM3	CM1	CM2	CM3	CM1	CM2	CM3
I-1	Redwood Ave and Route 1/S Main St	Signalized		S03	S08	S21PB	Install ADA curb ramps, install advance ped warning signs, verify crosswalk width		S03	S08	S17PB	S21PB			S08			S03	S08			
I-2	Oak St and Route 1/S Main St	Signalized		S03	S9	S21PB	Advance ped warning signs		S03	S09		S21PB		S09				S03	S09			
I-3	Boat Yard Dr and Route 20	Two way stop controlled	NS06				Reduce corner radius of NW and NE corners with planters or striping	NS06						NS06								
I-4	Pine St and Route 1/ S Main St	Two way stop controlled	NS21PB	NS22PB			Stripe high visibility crosswalk, update ADA ramps, radar speed feedback signs along Main St	NS21PB	NS22PB		NS21PB	NS22PB					NS21PB	NS22PB				
I-5	South St and Route 1/S Main St	One way stop controlled	NS06	NS21PB			Reduce corner radius for southeast corner	NS06	NS21PB		NS21PB						NS06	NS21PB				
I-6	Boat Yard Drive and Route 1/ S Main St	Signalized		S03	S09	S12	Install ADA curb ramps, install intersection warning signs		S03	S12					S12			S03	S12			
I-7	Route 1 and Route 20	Signalized		S03			Install intersection warning signs, radar feedback signs on SB approach		S03						S03			S03				
I-8	Cypress St and Route 1/ S Main St	Signalized	S03	S21PB			Stripe high visibility crosswalk, install ADA ramps, upgrade pavement markings		S03			S21PB						S03				
<b>Identified from Stakeholder Input</b>																						
I-9	Traffic Safety around Schools		NS06	NS22PB			Other traffic calming measures near schools	NS06	NS22PB		NS22PB			NS06								
I-10	Highway 1/Main Street and Pudding Creek Road	One way stop controlled	NS03	NS06				NS03	NS06					NS03	NS06		NS03	NS06				
I-11	Noyo Point Road and S Main Street	Two way stop controlled	NS06	NS07			Restrict left turns from Noyo	NS07						NS07			NS07					

Code	Countermeasure Name
HSIP/Non-HSIP Code	
S02	Improve signal hardware: lenses, back-plates with retroreflective borders, mounting, size, and number
S03	Improve signal timing (coordination, phases, red, yellow, or operation)
S08	Convert signal to mast arm (from pedestal-mounted)
S09	Install raised pavement markers and striping (Through Intersection)
S12	Install raised median on approaches (S.I.)
S17PB	Install pedestrian countdown signal heads
S21PB	Modify signal phasing to implement a Leading Pedestrian Interval (LPI)

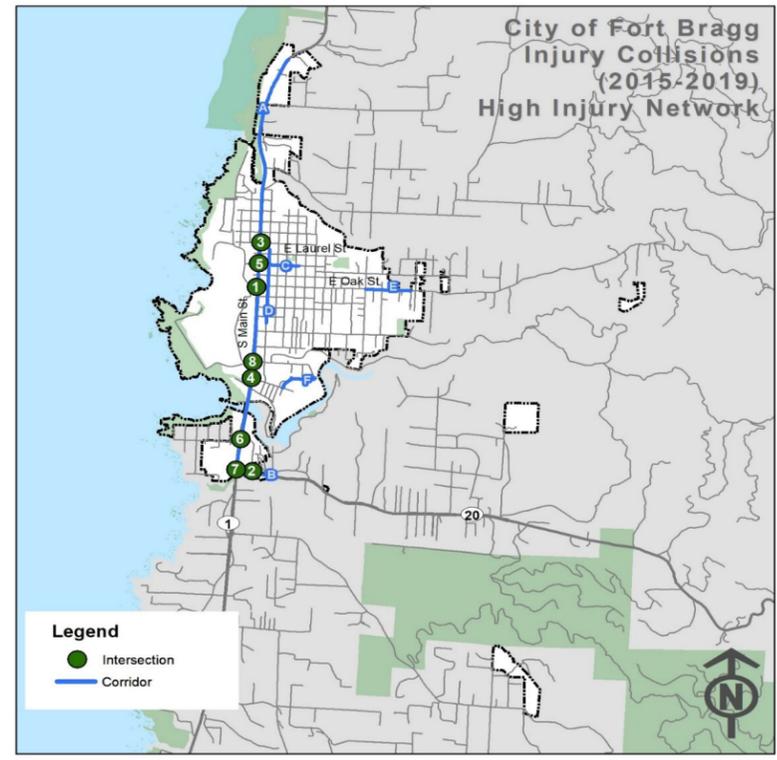
Code	Countermeasure Name
NS03	Install signal
NS06	Install/upgrade larger or additional stop signs or other intersection warning/regulatory signs
NS07	Upgrade intersection pavement markings
NS17	Install right turn lane
NS21PB	Install/upgrade pedestrian crossing at uncontrolled locations (with enhanced safety features)
NS22PB	Install Rectangular Rapid Flashing Beacon (RRFB)



High-risk Roadway Segments

ID	Roadway Segment	Consolidated CMs (HSIP-Eligible - Refer to LRSM* 2020)				Additional CM (non-HSIP)**	EA - 1 Improve Intersection Safety			EA - 2 Improve Pedestrian Safety			EA - 3 Reduce Improper Turning Collisions			EA - 4 Reduce Route 1 Collisions			EA - 5 Reduce Alley Way Collisions		
		CM1	CM2	CM3	CM4		CM1	CM2	CM3	CM1	CM2	CM3	CM1	CM2	CM3	CM1	CM2	CM3	CM1	CM2	CM3
A	Main St/Route 1: Jane Ln to Highway 20/ Fort Bragg Willits Rd	R22	R26	R34PB	R35PB	bike route signange				R34PB	R35PB		R22			R22	R26	R35PB			
B	Highway 20/ Fort Bragg Willits Rd: Route 1 to South Harbor Dr	R22	R26			Restripe intersection corners							R22								
C	Redwood Ave: West Terminus to North Whipple St	R01	R22	R22	R22	Install ADA curb ramps; upgrade pavement markings				R35PB	R36PB		R22								
D	Franklin St: Laurel St to E Chestnut St	R22				Restrict parking near intersection to increase sight distance (red curbs)							R22								
E	Fort Bragg Sherrwood Rd: California Way to Dana St	R22	R34PB			Install Class III bike route markings, reduce parking near intersection corners				R34PB			R22								
F	River Dr/ Kempe Way: South St to Cypress St	R22	R35PB	R01		Install streetlights to increase ped visibility near hospital				R01	R35PB		R22								
<b>Identified from Stakeholder Input</b>																					
G	Alleyways					Limit parking near alley exits to increase sight distance															
H	Chestnut Street	R26				traffic calming measures															

Code	Countermeasure Name
R22	Install/Upgrade signs with new fluorescent sheeting (regulatory or warning)
R26	Install dynamic/variable speed warning signs
R34PB	Install sidewalk/pathway (to avoid walking along roadway)
R35PB	Install/upgrade pedestrian crossing (with enhanced safety features)
R36PB	Install raised pedestrian crossing



	Strategy	Performance Measure	Organizations to be involved
<b>Education</b>	Conduct public information and education campaign for intersection safety laws, unsafe speeds, distracted driving, improper turning and driving under the influence.	Number of education campaigns	City/ School District/ Police Department
	Conduct pedestrian safety campaigns and outreach to raise their awareness of pedestrian safety needs through media outlets, social media and Bike and Walk Mendocino. Update pamphlet for crosswalk safety for Fort Bragg every 3-5 years	Number of education campaigns	City/ School District/ Police Department
	Conduct bicycle safety campaigns and outreach to raise their awareness of bicycle safety needs through media outlets, social media and Bike and Walk Mendocino. Update pamphlet for bicycle safety for Fort Bragg every 3-5 years	Number of education campaigns	City/ School District/ Police Department
<b>Enforcement</b>	Targeted enforcement at high-risk locations.	Number of tickets issued.	Police Department
	Increase the number of personnel who have completed Advanced Roadside impaired Driving Enforcement (ARIDE) training	Number of personnel who have completed Advanced Roadside impaired Driving Enforcement (ARIDE) training	Police Department
<b>Emergency Medical Services (EMS)</b>	S05, Install emergency vehicle pre-emption systems	EMS vehicle response time.	Mendocino County Local Emergency Services Agency
	Increase the number of EMS/fire control personnel taking Traffic Incident Management Training	number of EMS/fire control personnel taking Traffic Incident Management Training	Mendocino County Local Emergency Services Agency

## Benefit/Cost Ratio Calculations

This appendix includes the Benefit/Cost methodology used in the Caltrans calls-for-projects in the HSIP programs. The HSM, Part B - Chapter 7, includes more details on conducting Economic Appraisal for roadway safety projects. Local agencies will be required to utilize the HSIP Analyzer to calculate the B/C ratio as part of their application for HSIP funding. Starting in Cycle 7 call for projects, the fatality and severe injury costs have been combined for calculating the benefit. Because fatality figures are small and are a matter of randomness, this change is being made to reduce the possibility of selecting an improvement project on the basis of randomness.

$$1) \text{ Benefit (Annual)} = \sum_{s=0}^3 \frac{CRF \times N \times CC_{ave}}{Y}$$

- $CRF$  : Crash reduction factor in each countermeasure.
- $S$  : Severity (0: PDO, 1: Minor Injury, 2: Injury, 3: Severe Injury/Fatal). See the below table.
- $N$  : Number of Crashes, in severity levels, related to selected countermeasure.
- $Y$  : Crash data time period (Year).
- $CC_{ave}$  : Crash costs in severity levels.

Severity (S)	Crash Severity *	Location Type	Crash Cost ***
3	**Fatality and Severe Injury Combined (KA)	Signalized Intersection	\$1,590,000
3		Non Signalized Intersection	\$2,530,000
3		Roadway	\$2,190,000
2	Evident Injury – Other Visible (B)		\$142,300
1	Possible Injury–Complaint of Pain (C)		\$80,900
0	Property Damage Only (O)		\$13,300

\* The letters in parenthesis (K, A, B, C and O) refer to the KABCO scale; it is commonly used by law enforcement agencies in their crash reporting efforts and is further documented in the HSM.

\*\* Figures were calculated based on an average Fatality (K) / Severe Injury (A) ratio for each area type, a crash cost for a Fatality (K) of \$7,219,800, and a crash cost of a Severe/Disabling Injury (A) of \$389,000. These costs are used in the HSIP Analyzer.

\*\*\* Based on Table 7-1, Highway Safety Manual (HSM), First Edition, 2010. Adjusted to 2020 Dollars.

$$2) \text{ Benefit (Life)} = \text{Benefit (annual)} \times \text{Years of service life}$$

$$3) \text{ Benefit/Cost Ratio (each countermeasure): } \text{Benefit Cost Ratio}_{(CM)} = \frac{\text{Benefit (Life)}_{(CM)}}{\text{Total Pr oject Cost}_{(CM)}}$$

$$4) \text{ Benefit/Cost Ratio (project): } \text{Benefit/Cost Ratio (Pr oject)} = \frac{\sum_{CM=1}^3 \text{Benefit (Life)}_{(CM)}}{\text{Total Pr oject Cost}}$$



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-173

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Business

**In Control:** City Council

**File Type:** Resolution

**Agenda Number:** 8B.

Receive Report and Consider Adoption of City Council Resolution Approving Professional Services Agreement with De Novo Planning Group to Prepare an Environmental Impact Report for a Proposed Grocery Outlet at 825, 845, and 851 S. Franklin St. and Authorizing City Manager to Execute Contract (Amount Not to Exceed \$56,013; Account No. 119-0000-2668)



<b>AGENCY:</b>	City Council
<b>MEETING DATE:</b>	April 25, 2022
<b>DEPARTMENT:</b>	Community Development
<b>PRESENTED BY:</b>	Heather Gurewitz
<b>EMAIL ADDRESS:</b>	hgurewitz@fortbragg.com

## AGENDA ITEM SUMMARY

**TITLE:**

**Receive Report and Consider Adoption of City Council Resolution Approving Professional Services Agreement with De Novo Planning Group to Prepare an Environmental Impact Report for a Proposed Grocery Outlet at 825, 845, and 851 S. Franklin St. and Authorizing City Manager to Execute Contract (Amount Not to Exceed \$56,013; Account No. 119-0000-2668)**

**ISSUE:**

The Community Development Department has received a complete application for a proposed Grocery Outlet at 825, 845, and 851 S. Franklin St. This project requires an Environmental Impact Report (EIR).

**ANALYSIS:**

The City issued a Request for Proposals (RFP) to conduct the EIR for the proposed Grocery Outlet project which requires a Coastal Development Permit, a Lot Merger, and Design Review. Three responses were received to the RFP. The proposals were reviewed by three staff members and one contractor. De Novo was selected based on experience, price, and proposed timeline. The applicant was informed that the City intended to award the contract to De Novo and they have concurred and agreed that they will cover all costs incurred by the contractor in the production of the document.

**RECOMMENDED ACTION:**

Adopt a resolution approving the Professional Services Agreement with De Novo Planning Group and authorize City Manager to execute contract.

**ALTERNATIVE ACTION(S):**

Continue item to a future date and request additional information.

**FISCAL IMPACT:**

All costs incurred will be paid by the applicant, so there is no fiscal impact to the City.

**GREENHOUSE GAS EMISSIONS IMPACT:**

N/A

**CONSISTENCY:**

N/A

**IMPLEMENTATION/TIMEFRAMES:**

If approved, the contract will be circulated for signatures and the contractor will begin work immediately.

**ATTACHMENTS:**

1. Resolution Approving Professional Services Agreement
2. De Novo Proposal to Conduct an EIR for the Grocery Outlet
3. Proposed Professional Service Agreement

**NOTIFICATION:**

1. Terry Johnson, applicant
2. Steve McMurtry, consultant

**RESOLUTION NO. \_\_\_\_-2022**

**RESOLUTION OF THE FORT BRAGG CITY COUNCIL  
APPROVING PROFESSIONAL SERVICES AGREEMENT WITH DE NOVO  
PLANNING GROUP TO PREPARE AN ENVIRONMENTAL IMPACT REPORT  
FOR A PROPOSED GROCERY OUTLET AT 825-851 S. FRANKLIN STREET  
AND AUTHORIZING CITY MANAGER TO EXECUTE CONTRACT (AMOUNT  
NOT TO EXCEED \$56,013; ACCOUNT NO. 119-0000-2668)**

**WHEREAS**, on March 14, 2022, the City Council approved a Scope of Work for a Request for Proposals for an Environmental Impact Report for a proposed Grocery Outlet at 825, 845, and 851 S. Franklin Street; and

**WHEREAS**, on March 29, 2022, the City received three proposals for the preparation of an Environmental Impact Report from De Novo Planning Group, Helix Environmental Planning, and Metropolitan Planning Group; and

**WHEREAS**, those proposals were reviewed and evaluated on the basis of capabilities, qualifications, and responsiveness; and

**WHEREAS**, the applicant (Best Development Group) has agreed to cover the proposed cost of \$56,013 and has an established Developer Deposit Account; and

**WHEREAS**, based on all the evidence presented, the City Council finds as follows:

1. De Novo Planning Group is qualified to provide necessary professional services to complete the Environmental Impact Report.

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Fort Bragg does hereby approve a Professional Services Agreement with De Novo Planning Group for the preparation of an Environmental Impact Report for a proposed Grocery Outlet and authorizes the City Manager to execute the same upon execution by Contractor (Amount Not to Exceed \$56,013.00 Account 119-0000-2668).

The above and foregoing Resolution was introduced by Councilmember \_\_\_\_\_, seconded by Councilmember \_\_\_\_\_, and passed and adopted at a regular meeting of the City Council of the City of Fort Bragg held on the 25<sup>th</sup> day of April, 2022, by the following vote:

**AYES:  
NOES:  
ABSENT:  
ABSTAIN:  
RECUSED:**

---

**BERNIE NORVELL**  
Mayor

**ATTEST:**

---

**June Lemos, MMC**  
**City Clerk**



Proposal for Professional Services to  
Prepare Environmental Documentation for the  
Grocery Outlet, Fort Bragg, CA



March 28, 2022

Submitted to:

Attn: June Lemos, MMC, City Clerk  
City of Fort Bragg  
416 North Franklin Street  
Fort Bragg, CA 95437

Submitted by:

**De Novo Planning Group**

A Land Use Planning, Design, and Environmental Firm

1020 Suncast Lane, Suite 106 | El Dorado Hills, CA 95762  
info@denovoplanning.com | TEL 916-580-9818



# De Novo Planning Group

A Land Use Planning, Design, and Environmental Firm

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## FIRM DESCRIPTION

### DE NOVO PLANNING GROUP



De Novo Planning Group is a land use and environmental planning firm specializing in general plan updates, community planning, environmental studies (CEQA and NEPA), and sustainability planning. The founding principals have successfully completed over 200 projects consisting of comprehensive general plans, specific plans, housing elements, environmental impact reports, negative declarations, initial studies, NEPA analyses, climate action plans, biological assessments, wetland delineations, and development projects throughout California.

The founding principals have over 50 years of combined experience and have successfully completed hundreds of projects consisting of environmental impact reports, negative declarations, initial studies, NEPA analyses, climate action plans, biological assessments, wetland delineations, general plans, specific plans, housing elements, and development projects throughout California. Our areas of expertise include environmental documentation, permitting, and compliance, with technical abilities in air quality, agricultural resources, biology, climate change, land use, and water resources.

Our mission is to provide municipal and private sector clients with world-class professional services, through principal-level attention to every client and every project. We pride ourselves on our ability to work with clients to balance the often-conflicting goals of economic, social, environmental, legal, and political forces. Our services result in an integrated planning and environmental solution for every project that is technically sound, cost-effective, and delivered within the client's schedule.



Mr. Saxelby has been practicing acoustic and noise control engineering for the past 15 years and is a Board Certified member of the Institute of Noise Control Engineering (INCE). He is an expert in the requirements of the California Environmental Quality Act (CEQA)

relating to noise impacts and has been involved in the preparation of hundreds of noise studies, including CEQA Environmental Impact Reports (EIR) and Mitigated Negative Declarations (MND). He is also a recognized expert witness in issues relating to building acoustics and noise control and has given testimony on various legal projects including construction defect claims and CEQA litigation.

## RELEVANT EXPERIENCE AND REFERENCE

The following three summaries are provided to illustrate EIR experience that included community opposition and potential legal challenges. It is noted that our firm has significant experience on other similar projects, and can provide a more comprehensive list of other projects upon request. It is also noted, that the project manager, Steve McMurtry, serves as an expert witness for several law firms in northern and southern California.

### CITY OF DAVIS – CANNERY MASTER PLAN EIR



The De Novo team completed a detailed and complex project-level EIR for the Cannery Project in Davis. The Cannery project consists of redevelopment and reuse of a 100-acre industrial site, formerly used as a tomato cannery. The project includes approximately 550 residential units, 136,000 square feet of mixed commercial and office space, parks, extensive bicycle networks, and urban farm, and a wide range of off-site infrastructure improvements. The project received an exceptionally high level of public scrutiny, and underwent several significant changes during development of the EIR. The project received an exceptionally high level of public scrutiny, and underwent several significant changes during development of the EIR. De Novo successfully addressed a wide range of complex environmental issues in the EIR, and the EIR was successfully certified by the City Council in November 2013. The American Planning Association has labeled the project as one of

the most “Innovative New Smart Growth Communities” in the country.

#### Reference:

Katherine Hess, Community Development Administrator, City of Davis  
 (530) 757-5652 | [khess@cityofdavis.org](mailto:khess@cityofdavis.org)

Budget: \$233,000

### CITY OF MANTECA OAKWOOD LANDING MASTER PLAN EIR



De Novo Planning Group prepared an EIR for the Oakwood Landing project in Manteca. The project encompassed 258.29 acres and included 290 high density residential units and up to 975 low density residential units. Additionally, the Project included 13.0 acres of general commercial uses anticipated to provide up to approximately 237,838 square feet of commercial. The Project provides approximately 16.06 acres of parkland and maintains approximately 6.77 acres of open space.

Reference: Rochelle Henson, Senior Planner, City of Manteca  
(925) 584-5434 | rhenson@ci.manteca.ca.us

Budget: \$219,000

### CITY OF BRENTWOOD, PRIORITY AREA 1 (PA-1) SPECIFIC PLAN AND EIR



De Novo is prepared a Specific Plan and Environmental Impact Report for the Priority Area 1 (PA-1). The project team included Ben Ritchie leading the Specific Plan project team, and Steve McMurtry leading the EIR team. PA-1 is a 373-acre project site located in the northwest corner of Brentwood, and is a projected focal point for jobs and mixed-use development. The Specific Plan created the precise criteria for the growth and development of PA-1 by establishing a vision for the area, identifying uses desired and allowed in PA-1, and planning for infrastructure improvements to support future development. The Specific Plan was approved and the EIR was certified.

Reference: Tim Nielson, Associate Planner, City of Brentwood  
(925) 516-5151 | tnielsen@brentwoodca.gov

Budget: \$263,000

## KEY PERSONNEL QUALIFICATIONS

### **STEVE MCMURTRY – PROJECT MANAGER/PRINCIPAL PLANNER**

Steve has successfully led multidisciplinary teams to complete hundreds of environmental, transportation and land use planning, and development projects throughout California. Steve’s experience includes service in engineering and planning firms, as well as in the home-building industry. His environmental experience encompasses field research, public outreach, mitigation development, document writing, and permitting. During his tenure in the construction industry he was responsible for planning, design, and construction of projects valued over \$300 million for a Fortune 500 company, which included the construction of roadways, utilities, pump stations, parks, and trails. Steve graduated from Cal Poly San Luis Obispo with a bachelor’s degree in Natural Resource Management with graduate studies in Biological Sciences at San Jose State University.



### **BEN RITCHIE – PRINCIPAL PLANNER**



Ben is a Principal with De Novo and is responsible for the management of complex CEQA and NEPA projects, climate action plans, sustainable policy development, general plan updates, specific plans, redevelopment plans, and municipal service reviews. His experience includes residential, commercial, mixed-use, transportation, annexation and redevelopment projects throughout California. Ben has served as an Environmental Coordinator for municipalities, which included oversight of an Environmental Planning Division. Ben graduated from Cal Poly, San Luis Obispo with a bachelor’s degree in Political Science and a Master of City and

Regional Planning (MCRP) where he received the California Planning Foundation Scholarship for Academic Excellence.

### **ELISE CARROLL – SENIOR PLANNER**

Elise specializes in environmental impact reports and urban planning for both the public and private sectors. As an experienced environmental impact assessment practitioner (CEQA and NEPA), Elise analyzes and summarizes the environmental impacts of existing and proposed developments. On behalf of De Novo Planning Group, she regularly provides informative research summaries and mitigation recommendations to public and private agencies. Elise holds a bachelor’s of science degree from University of California, Davis in Environmental Policy Analysis and Planning (with an emphasis in Urban & Regional Planning).



### **JOSH SMITH – ASSOCIATE PLANNER**

Josh has been in the planning industry since 2010, and is responsible for the preparation of CEQA/NEPA documents, climate change planning for local governments, development of air quality and greenhouse gas technical studies, and Health Risk Assessments. Josh has expertise utilizing best-practice standards for developing greenhouse gas (GHG) inventories and context-specific GHG mitigation measures, as well as developing custom air pollutant emissions calculators for complex projects. He also has prior experience working in state and local government. Josh graduated from UC Davis in 2010 with a Bachelor's of Science in Environmental Policy Analysis & Planning, where he participated in the Davis Honors Challenge Program. He is a LEED AP O+M professional.



### **LUKE SAXELBY - PRINCIPAL**



Mr. Saxelby has been practicing acoustic and noise control engineering for the past 15 years and is a Board Certified member of the Institute of Noise Control Engineering (INCE). He is an expert in the requirements of the California Environmental Quality Act (CEQA) relating to noise impacts and has been involved in the preparation of hundreds of noise studies, including CEQA Environmental Impact Reports (EIR) and Mitigated Negative Declarations (MND). He is also a recognized expert witness in issues relating to building acoustics and noise control and has given testimony on various legal projects including construction defect claims and CEQA litigation.

Mr. Saxelby has extensive experience in the use of complex noise modeling programs including the Federal Highway Administration (FHWA) Traffic Noise Model (TNM), the CadnaA (Computer Aided Noise Abatement) sound prediction model, the SoundPLAN acoustic model, the Federal Aviation Administration's (FAA) Integrated Noise Model (INM), and the Enhanced Acoustic Simulator for Engineers (EASE) model.

## PROJECT UNDERSTANDING, APPROACH, AND SCOPE OF WORK

### PROJECT UNDERSTANDING

The City of Fort Bragg is seeking proposals from qualified firms interested in contracting with the City to prepare an Environmental Impact Report (EIR) for the proposed Grocery Outlet Project, in accordance with the California Environmental Quality Act (CEQA) and the City's Local Coastal Program.

An Initial Study and Mitigated Negative Declaration was prepared for a proposed Grocery Outlet in Fort Bragg, which was circulated for a 30-day review period January 14 – February 16, 2021. It was adopted by the Planning Commission on June 9, 2021 and affirmed by the City Council on July 12, 2021. Due to a lawsuit, the applicant has vacated their existing permits and will reapply. They have requested that the City prepare an Environmental Impact Report.

The proposed site consists of 1.63 acres of land in the Coastal Zone and Commercial General Zone. The proposed project is a 16,000 ft<sup>2</sup> grocery store with a 54-space parking lot surrounded by urban uses, located at 825, 845, and 851 South Franklin Street, Fort Bragg. The proposed Project will require Planning Commission approval for the following: Coastal Development Permit, Design Review, and a Lot Merger.

The original application was submitted in 2019, and was reviewed by the California Coastal Commission, California Department of Fish and Wildlife, Caltrans, Fort Bragg Fire Department, Mendocino County's Department of Planning and Building Services, and City of Fort Bragg Public Works Department. We understand that there are a variety of technical studies, which will require peer review, but are anticipated to be valuable information that can be used in an analysis of the Project.

### APPROACH

The De Novo Team will prepare an Environmental Impact Report (EIR) for the proposed project. An EIR is an informational document intended to inform public decision-makers, responsible or interested agencies and the general public of the potential environmental effects of a project, and where applicable, provide mitigation measures that can be implemented to reduce or avoid the potential adverse environmental effects.

While CEQA requires that major consideration be given to avoiding adverse environmental effects, the lead agency and other responsible public agencies must balance adverse environmental effects against other public objectives, including the economic and social benefits of a proposed project, in determining whether a proposed project should be approved. A Project-level EIR is described in State CEQA Guidelines § 15161 as:

*“The most common type of EIR (which) examines the environmental impacts of a specific development project. This type of EIR should focus primarily on the changes in the environment that would result from the development project. The EIR shall examine all phases of the project including planning, construction, and operation.”*

## SCOPE OF WORK

### TASK A – PROJECT INITIATION

Within one week of project commencement, the De Novo team will meet with City Staff to discuss the following:

- Refinement of project work scope and schedule,
- City preferences for point of contact, method of communication, meeting responsibilities, project updates, etc.
- Collection of relevant background documents (adopted documents, reports, and studies), and
- Project deliverables.

### TASK B – PREPARE PROJECT DESCRIPTION

De Novo will prepare a detailed description including text and graphics utilizing the information provided to the City by the applicant and applicant’s engineer. The project description will include a regional and local setting, project history and land uses, past ownership, objectives, characteristics, important project features including discretionary actions and entitlements, consistency with the General Plan and zoning designations, a list of responsible and other agencies expected to use the product document in decision making, and a list of approvals for which the product document will be used. We will provide the draft project description to the City staff for review and comment. Upon receipt of comments from the City staff we will finalize the project description for use in the NOP/Initial Study as described in Task C, which will involve a second review of the Project Description along with the Initial Study.

### TASK C – NOP/INITIAL STUDY

De Novo will prepare an Initial Study and Notice of Preparation (NOP) in an administrative draft form for City staff to review. Comments received will be incorporated into the Initial Study and Notice of Preparation for public circulation. After the document is “Screen Checked” by City staff, we will finalize the document. The public draft will be published and distributed with the proper notices to the State Clearinghouse. We will provide City staff with a copy to be filed by the City with the County Clerk, and a newspaper of regional circulation. The results of the Initial Study and NOP will be presented at a public scoping meeting in coordination with City staff.

The intent of the Initial Study/NOP and public scoping meeting is to narrow the focus of the environmental analysis in the EIR to the most pertinent and relevant environmental issues. The Initial Study will review the following topics: Aesthetics, Agricultural Resources, Air Quality, Biological Resources, Cultural Resources, Energy, Greenhouse Gas Emissions, Hazards and Hazardous Materials, Hydrology and Water Quality, Land Use Planning, Mineral Resources, Population, Public Services, Noise, Recreation, Traffic, Tribal Resources, and Utilities. Once the Initial Study is completed, we may find that the project will not have significant adverse impacts

on certain topics and a cursory level of analysis is all that would be needed in the EIR for that particular topic. For other topics, we would require a full EIR analysis of the topic.

From our initial review of the project relative, we believe that the following topics will be scoped out of the EIR: Aesthetics/Visual Resources, Agricultural Resources, Cultural and Tribal Resources, Geology/Soils/Seismicity, Hazards and Hazardous Materials, Hydrology and Water Quality, Mineral Resources, Population and Housing, Public Services/Recreation, and Wildfires. If the above topics are scoped out during the NOP phase of the project, we would have the following topics be the focus of the EIR: Air Quality, Biological Resources, Energy, Greenhouse Gas Emissions, Land Use Planning, Noise, Traffic, and Utilities. This Scope of Work is based on these assumptions. Additional topics may need to be added to the scope after the NOP phase concludes, which would require additional budget.

The Initial Study and Scoping Meeting will also serve as an opportunity to define and solidify the project alternatives in a public forum. This will be an important step during this project task in an effort to streamline the Environmental Review process. We will review all available community information to assist in the development of up to four (4) project alternatives to be addressed in the EIR.

*Deliverables: One (1) electronic copy of the Admin Draft NOP with appendices, in MS Word and PDF format. One (1) electronic copy of the Final NOP with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK D – TECHNICAL STUDIES

The following discusses the individual technical studies that will be prepared for the project.

#### HEALTH RISK ASSESSMENT (HRA)

The objective of the HRA is to determine the public health risks from existing emissions from nearby rail and other toxic air sources, combined with the new public health risks from project-related traffic. The HRA consists of two main tasks: 1. Exposure Assessment, and 2. Risk Calculations.

- *Task 1 Exposure Assessment:* The main steps involved in exposure assessment includes estimating the emission rates of toxic air pollutants and running an air dispersion model to calculate the concentration of a toxic air pollutant at each location in the modeling domain. The modeling domain would include proposed project site, portions of adjacent roadways, and any other known source(s) of toxic air pollutants proximate to the site. The main toxic air contaminant (TAC) associated with industrial uses is diesel particulate matter (DPM), although various manufacturing uses also emit TACs. The emission rates of DPM will be calculated on the basis of site-specific characteristics. We propose to use the AERMOD air dispersion model with 5 years of meteorological data collected at the closest monitoring station. The proposed dispersion model is recommended by the Air District.

- *Task 2 Risk Calculations:* We propose to calculate the residential (70 year) cancer health risks by multiplying the concentration of DPM by its unit risk factor. The current unit risk factor recommended by the Office of Environmental Health Hazard Assessment is  $4.15 \times 10^{-4}$  (ug/m<sup>3</sup>)-1. The resulting cancer risks will be plotted on the base map.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts on air quality.

#### RUN CALCEEMOD

The project's unmitigated and mitigated NO<sub>x</sub> and PM<sub>10</sub> emissions will be modelled with the CalEEMod. The model will incorporate project details as provided to the City by the Project Applicant. We will confer with the City and Project Applicant regarding the feasibility of a wide variety of project NO<sub>x</sub> and PM<sub>10</sub> emissions reduction (mitigation) measures.

#### NOISE STUDY

The De Novo team includes Saxelby Acoustics to prepare a Noise and Vibration Study. The following outlines the scope of work for this study:

1. 1A. Existing Noise Environment:
  - **Traffic Noise:** Existing noise levels due to nearby transportation noise sources will be quantified. Saxelby Acoustics uses the Federal Highway Administration (FHWA) traffic noise prediction model for the prediction of traffic noise levels. Direct inputs to the traffic model will include traffic data provided by the project traffic consultant, existing posted speed limits, truck count information, and 24-hour traffic split data collected by Saxelby Acoustics.
  - **Community Noise Survey:** Saxelby Acoustics will conduct a noise survey within the project site to quantify existing background noise levels. The noise survey will consist of short-term noise level measurements and continuous noise level measurements for a minimum period of 24-hours.
2. **Analysis of Transportation Noise Environment:** Saxelby Acoustics will evaluate increased traffic noise levels at existing sensitive receptors in the project vicinity. This task will be performed using traffic volumes provided by the traffic engineer. We anticipate providing traffic noise levels for existing, existing plus project, cumulative, and cumulative plus project scenarios. However, should additional scenarios be included in the traffic study, we will also evaluate those scenarios. We will also calculate exterior and interior traffic noise levels on the proposed residential uses. If necessary, we will evaluate any required exterior or interior noise control measures needed to achieve compliance with the City noise level standards.
3. **Analysis of Stationary Noise Environment:** Saxelby Acoustics will provide an analysis of the noise and vibration impacts associated with construction of the project at existing

sensitive receptors in the project vicinity. It is expected that this analysis will follow the assumptions used in the project air quality analysis.

4. Report Preparation: Saxelby Acoustics will provide a draft report which details our findings, methodology, and noise reduction measures (if required). The report will be prepared to meet the requirements of the City and CEQA.
5. Response to Comments: Saxelby Acoustics will respond to comments on the draft technical report. After comments are received, a final report will be provided.

#### TASK E –ADMINISTRATIVE DRAFT EIR

De Novo will prepare the project-level EIR for the project in an administrative draft form for City staff to review. The EIR will be intended to provide the information and environmental analysis necessary to assist public agency decision-makers in considering approval of the project.

The EIR will consider the potential environmental effects of the project to determine the level of significance and will analyze these potential effects to the detail necessary to make these determinations on significance. Each section will include GIS graphics and figures to create an easy to comprehend document that is user-friendly. It is noted that the scope of the EIR is focused on the following topics: Air Quality, Biological Resources, Energy, Greenhouse Gas Emissions, Land Use Planning, Noise, Traffic, and Utilities.

The EIR will consist of the following sections:

#### EXECUTIVE SUMMARY

This section will provide a concise description of the project, the potential areas of controversy, issues to be resolved, project alternatives, and a summary of impacts and mitigation measures. The intent of this section is to provide the City and the public with a simple and easy to understand overview of the project and related issues, which will be analyzed and discussed much more thoroughly in the contents of the EIR.

#### INTRODUCTION

The Introduction will serve as an overview of the EIR, describing its purpose and relevant environmental review procedures, the document organization, and the methodology used.

#### PROJECT DESCRIPTION

The Project Description section will consist of a detailed description of the project (See Task B), including the proposed actions, the project goals and objectives, and the relationship of the project to other regional plans and projects. This section will also present the City's and other agency involvement in the project, and the use of the EIR by other agencies, including permits and approvals. This section will be consistent with the requirements of State CEQA Guidelines Section 15124.

De Novo will prepare the Project Description prior to including it in the Initial Study and Notice of Preparation. The Project Description will be provided in an administrative draft form for City staff to review. Comments received will be incorporated into the revised Project Description and included in the Initial Study and Notice of Preparation. After the document is “Screen Checked” by City staff, we will finalize the Project Description for public release.

#### ENVIRONMENTAL SETTING, IMPACTS, AND MITIGATION MEASURES

The Environmental Setting, Impacts, and Mitigation Measures section will present a detailed discussion of each individual environmental topic. Each discussion will include the following:

- An environmental setting and environmental baseline conditions (including figures and GIS graphics);
- The applicable local, state, and federal regulatory setting;
- The threshold of significance used for each impact determination;
- The methodology used for conducting the environmental analysis and making significance determinations;
- An analysis of all identified direct and indirect impacts associated with project;
- An analysis of the cumulative impacts associated with the project;
- Identification of mitigation measures to reduce impacts; and
- A determination of the significance of each impact after mitigation.

De Novo will work closely with City staff to formulate the appropriate mitigation measure language and timing that is appropriate for inclusion in the EIR. Each EIR section will be organized concisely for ease of use and future reference.

#### AIR QUALITY

The project will include an assessment of short-term construction-related emissions and long-term operational emissions, primarily attributable to emissions from vehicle trips and from energy consumption by the uses. We will consult with the local Air District regarding the project’s potential to cause impacts, and the applicability of the Air District’s Rules and Regulations. The Air Quality analysis will include the following:

- Regional air quality and local air quality in the vicinity of the project site will be described. Meteorological conditions in the vicinity of the project site that could affect air pollutant dispersal or transport will be described. Applicable air quality regulatory framework, standards, and significance thresholds will be discussed.
- Short-term (i.e., construction) increases in regional criteria air pollutants will be quantitatively assessed. The ARB-approved CalEEMod computer model will be used to estimate regional mobile source and particulate matter emissions associated with the construction of the proposed project.
- Long-term (operational) increases in regional criteria air pollutants will be quantitatively assessed for area source, mobile sources, and stationary sources. The ARB-approved

CalEEMod computer model will be used to estimate emissions associated with the proposed project. Exposure to odorous or toxic air contaminants will be assessed through a screening method as recommended by the Air District.

- Local mobile-source CO concentrations will be assessed through a CO screening method as recommended by the Air District. Mobile source CO concentrations are modeled for signalized intersections expected to operate at unacceptable levels of service (i.e., LOS E or worse). If the screening method indicates that modeling is necessary, upon review of the traffic analysis, CO concentrations will be modeled using the Caltrans-approved CALINE4 computer model.

We will incorporate the results of the HRA and CalEEMod modeling identified in a previous task. This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts on air quality. Greenhouse Gases and Climate Change will be addressed in a separate chapter.

#### BIOLOGICAL RESOURCES

We plan to utilize the Wildland Resource Manager’s Wetland Report and Biological Review to prepare this section of the EIR. This task does not include any new analysis, but will include a reconnaissance level survey by a qualified biologist to verify site conditions.

#### ENERGY

De Novo will prepare an Energy analysis pursuant to the requirements of CEQA. This will include an evaluation of the energy consumption (electricity, oil, and natural gas) and a review of the project related to the Title 24, Part 6 of the California Code of Regulations, known as the Building Energy Efficiency Standards (Standards), including the CALGreen standards. In order to ensure that energy implications are considered in project decisions, Appendix F of the CEQA Guidelines requires that EIRs include a discussion of the potential energy impacts of proposed projects, with particular emphasis on avoiding or reducing inefficient, wasteful and unnecessary consumption of energy. The goal of conserving energy implies the wise and efficient use of energy.

Per Appendix G of the State CEQA Guidelines, the proposed project would result in a significant impact on energy use if it would result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation; or conflict with or obstruct a state or local plan for renewable energy or energy efficiency.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts associated with energy consumption.

#### GREENHOUSE GASES AND CLIMATE CHANGE

De Novo will prepare a Green House Gas Emissions analysis pursuant to the requirements of federal, state, regional, and local laws and regulations related to thresholds and methodology for this analysis. The analysis will follow the California Air Pollution Control Officers Association (CAPCOA) white paper methodology and recommendations presented in Climate Change & CEQA, which was prepared in coordination with the California Air Resources Board and the Governor’s Office of Planning and Research as a common platform for public agencies to ensure that GHG emissions are appropriately considered and addressed under CEQA. This analysis will consider a regional approach toward determining whether GHG emissions are significant, and will present mitigation measures to reduce impacts. The discussion and analysis will include quantification of GHGs generated by the project using ARB-approved CalEEMod computer model as well as a qualitative discussion of the project’s consistency with any applicable state and local plans to reduce the impacts of climate change. The De Novo team will work with City staff to implement a methodology and mitigation strategy that meets all legal requirements and is consistent with current City policies and preferences.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts associated with greenhouse gas emissions.

#### LAND USE AND PLANNING

This section will include a detailed discussion of the project entitlements as it relates to the existing General Plan, Zoning Code, and other local regulations. We will discuss and map the existing and planned land uses and the character of the region. The local, regional, state, and federal jurisdictions potentially affected by the project will be identified, as well as their respective plans, policies, laws, and regulations (including zoning), and potentially sensitive land uses. We will evaluate the proposed project for consistency the General Plan, the Zoning Ordinance, and any other relevant planning document. Planned development and land use trends in the region will be identified based on currently available plans. Reasonably foreseeable future development projects within the region will be noted, and the potential land use impacts associated with the project will be presented.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to ensure consistency with the existing and planned land uses.

#### NOISE

The De Novo Team includes Saxelby Acoustics, an acoustical consulting firm with exceptional local knowledge and experience. The scope of work includes the preparation of a noise study described in a previous task, which will be incorporated into this section of the EIR.

This EIR section will include a full discussion of any existing noise environment, an analysis of station noise generated by the project, including proposed loading docks, on-site truck circulation, parking lots, and any proposed mechanical equipment. We will also conduct an analysis of the noise and vibration impacts associated with construction of the project at existing sensitive receptors in the project vicinity. This section will include a comprehensive mitigation plan to address any potentially significant impacts identified.

#### TRANSPORTATION AND CIRCULATION

We plan to utilize the KD Anderson traffic report and addendum to prepare this section of the EIR. The Traffic Report includes a VMT analysis to comply with the requirements of SB 743, and a LOS Analysis for compliance with General Plan policy. This task does not include any new analysis.

#### UTILITIES AND SERVICES SYSTEMS

We understand that an engineer has been retained by the applicant to perform civil engineering for this project, and that the applicant’s plan has been engineered to City specifications. We will utilize the engineering design/calculations performed by the engineer in the preparation of this section of the EIR. This section will focus on wastewater, water, and storm drainage infrastructure, as well as other utilities (i.e. solid waste, gas, electric, etc.) that are needed to serve the proposed project. It is noted that these topics are relevant to the Utilities Department and the appropriate level of coordination will be performed to confirm that the plans are acceptable and that the project description is accurate. This section will provide an analysis, including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts associated with utilities and service systems.

#### CUMULATIVE IMPACT SUMMARY

De Novo will analyze the environmental impacts of the project when viewed in combination with other known, approved, or reasonably foreseeable projects in the region. The cumulative analysis will address each topic covered in the environmental analysis and will identify appropriate mitigation measures for any significant impacts identified. This cumulative analysis will be based on a list of known projects in the region as well as forecasts.

#### ALTERNATIVES

De Novo will coordinate with City staff to formulate up to four (4) alternatives for analysis in the EIR as required by the CEQA Guidelines. Our efforts will result in an EIR that will include an examination of a range of reasonable alternatives that could feasibly achieve the basic objectives of the project.

The CEQA Guidelines require that a “No Project” alternative be analyzed among the range of alternatives. An alternative location must also be analyzed unless it is determined by the lead

agency that a feasible alternative location does not exist. If the lead agency determines that an alternative location does not exist, it must disclose the reasons for this conclusion in the EIR.

The alternatives section will provide a description and comparison of the alternatives. Finally, an environmental superior alternative will be selected. From our experience with similar EIRs, we will provide suggested alternatives for City staff to consider. Once the alternatives are initially formulated, they will be presented at the public scoping meeting and refined based on public input. (*Note: We do not anticipate the need to analyze the alternatives at an equal level to the proposed project.*)

#### OTHER CEQA REQUIREMENTS

The section will include the other required CEQA sections including issues previously determined to be less than significant, growth-inducing impacts, significant irreversible environmental effects, and a summary of significant and unavoidable impacts.

#### REPORT PREPARERS AND REFERENCES

This section will provide a list of all persons, agencies, and references used to prepare the EIR.

*Deliverables: One (1) electronic copy of the Admin Draft EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK F – PUBLIC DRAFT EIR AND NOTICE OF COMPLETION

Comments received from City staff regarding the Administrative Draft EIR will be incorporated into the Draft EIR for public circulation. De Novo will generate a “Screen-check” Draft EIR for a final staff review before we produce the document for public review. After the document is finalized, we will publish the document and distribute it with the proper notices to the State Clearinghouse. We will provide City staff with a copy to be filed by the City with the County Clerk, and a newspaper of regional circulation.

*Deliverables: One (1) electronic copy of the Public Draft EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK G – ADMINISTRATIVE FINAL EIR

Upon completion of the public review period De Novo will coordinate with City staff and prepare a written response to the public comments, and where necessary the appropriate revisions will be made to the EIR text. Any additional text will be marked in underline format and any deleted text will be marked in ~~strikeout~~ format. All responses will be prepared pursuant to Section 15088 of the State CEQA Guidelines and provided to Stockton staff for review.

We anticipate 20 or fewer comment letters, two to three pages in length. Excessively long comment letters, or those that are complicated and require a significant effort and/or additional analysis to respond to are considered outside the scope of work and cost estimate.

*Deliverables: One (1) electronic copy of the Admin Final EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK H – FINAL EIR AND MMRP

Comments received from City staff regarding the Administrative Draft Final EIR will be incorporated into the Final EIR for public circulation. De Novo will generate a “Screen-check” Final EIR for a final staff review before we produce the document for public review. After the document is finalized, we will produce the document and deliver it to the City for distribution.

This task will also include the preparation of a Mitigation Monitoring and Reporting Program (MMRP) pursuant to Section 21081.6 of the Public Resources Code. The MMRP will consolidate information contained in the environmental analysis, including the specific mitigation measure, the party responsible for implementation, the party responsible for monitoring, the time frame for implementation, and a section for confirmation of implementation.

*Deliverables: One (1) electronic copy of the Public Final EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK I – FINDINGS OF FACT/ OVERRIDING CONSIDERATIONS

De Novo will prepare the required CEQA Findings of Fact and Statement of Overriding Considerations pursuant to requirements of Sections 15091 and 15093 of the State CEQA Guidelines. These findings shall be prepared using Stockton’s format and will be provided to City staff for an administrative review. Comments received from staff regarding the Administrative Findings will be incorporated into a final version of the Findings for use by the City at the public hearings.

*Deliverables: One (1) electronic copy of the findings.*

#### TASK J – ATTENDANCE AT PLANNING COMMISSION/CITY COUNCIL MEETINGS

De Novo will attend up to two (2) public hearings, which includes a Planning Commission and City Council hearing. De Novo will be responsible for preparing any exhibits that may be necessary for display at these meetings, presentations, and responses to public comment. We anticipate that the Project Manager or Senior Planner will be required for each meeting. Technical support from other members is not included, but can be accommodated on a time and material basis.

#### TASK K – NOTICE OF DETERMINATION

Upon certification of the EIR De Novo will prepare a Notice of Determination for filing with the State Clearinghouse. The applicant will be responsible for paying the CDFW filing fees, which are approximately \$3,445.25, but are anticipated to increase on January 1, 2022. The applicant will also be responsible for paying the Clerk fee, which is \$50.

*Deliverables: One (1) electronic copy of the NOD.*

TASK L-ADMINISTRATION/PROJECT MANAGEMENT

This task includes time to administer the contract, invoicing, coordination with the City staff, developer team, and consultant team.

# BUDGET AND SCHEDULE OF CHARGES

Task #	Task Description	McMurtry Project Manager \$175	Ritchie Prin. Planner \$175	Carroll Senior Planner \$135	Smith Senior Planner \$135	De Novo Support/GIS \$105	Direct Costs	Totals
<b>A Project Initiation</b>								
	Refine scope/schedule	0.75						0.8
	Conference Call w/ City re: communication, deliverables	0.50						0.5
	Research and Collect background documents			1.00		0.50	\$ 28.75	1.5
	<i>Subtotal</i>	1.25	0.00	1.00	0.00	0.50		2.8
	<b>Task A</b>	\$ 218.75	\$ -	\$ 135.00	\$ -	\$ 52.50	\$ 28.75	\$ 435.00
<b>B Prepare Project Description</b>								
	See Task E							0.0
	<i>Subtotal</i>	0.0	0.0	0.0	0.0	0.0		0.0
	<b>Task B</b>	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
<b>C NOP</b>								
	See Task E							0.0
	<i>Subtotal</i>	0.0	0.0	0.0	0.0	0.0		0.0
	<b>Task C</b>	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
<b>D Additional Technical Studies</b>								
	Health Risk Assessment, AQ Modeline, Noise Study				10.00		51.00	61.0
	<i>Subtotal</i>	0.00	0.00	0.00	10.00	0.00		10.0
	<b>Task D</b>	\$ -	\$ -	\$ -	\$ 1,350.00	\$ -	\$ 8,745.00	\$ 10,095.00
<b>E Prepare Administrative Draft EIR</b>								
	Document Preparation/Management	3.0		18.0			\$ 82.50	21.0
	<i>Subtotal</i>	3.0	0.0	18.0	0.0	0.0		21.0
	<b>Task E</b>	\$ 525.00	\$ -	\$ 2,430.00	\$ -	\$ -	\$ 82.50	\$3,037.50
<b>F Prepare Public Draft EIR/NOC</b>								
F.1	Document Revisions	12.0	8.0	8.0	8.0	2.0		38.0
F.2	Document Preparation/Management	6.0		1.0		4.0		11.0
	<i>Subtotal</i>	18.0	8.0	9.0	8.0	6.0		49.0
	<b>Task F</b>	\$ 3,150.00	\$ 1,400.00	\$ 1,215.00	\$ 1,080.00	\$ 630.00	\$ -	\$7,475
<b>G Prepare Admin Final EIR</b>								
G.1	Introduction	1.0						1.0
G.2	Overview of Comments Received	4.0						4.0
G.3	Response to Comments	12.0	12.0	16.0	16.0			56.0
G.4	Produce Admin. Final EIR	1.0		4.0		4.0		9.0
G.5	Document Preparation/Management	1.0		1.0		2.0		4.0
	<i>Subtotal</i>	19.0	12.0	21.0	16.0	6.0		74.0
	<b>Task G</b>	\$ 3,325.00	\$ 2,100.00	\$ 2,835.00	\$ 2,160.00	\$ 630.00	\$ -	\$11,050
<b>H Prepare Final EIR and MMRP</b>								
H.1	Document Revisions	6.0		12.0				18.0
H.2	Document Preparation	1.0		4.0	1.0	8.0		14.0
H.3	MMRP	1.0		10.0		1.0		12.0
	<i>Subtotal</i>	8.0	0.0	26.0	1.0	9.0		44.0
	<b>Task H</b>	\$ 1,400.00	\$ -	\$ 3,510.00	\$ 135.00	\$ 945.00	\$ -	\$5,990
<b>I Findings/Overriding Considerations</b>								
I.1	Prepare Admin. Findings/Overriding Considerations	6.0		18.0				24.0
I.2	Prepare Final Findings/Overriding Considerations			1.0				1.0
	<i>Subtotal</i>	6.0	0.0	19.0	0.0	0.0		25.0
	<b>Task I</b>	\$ 1,050.00	\$ -	\$ 2,565.00	\$ -	\$ -	\$ -	\$ 3,615.00
<b>J Attendance at Hearings for Final EIR</b>								
J.1	Planning Commission	10.0					\$ 200.00	10.0
J.2	City Council	10.0					\$ 200.00	10.0
	<i>Subtotal</i>	20.0	0.0	0.0	0.0	0.0		20.0

Task #	Task Description	McMurtry Project Manager \$175	Ritchie Prin. Planner \$175	Carroll Senior Planner \$135	Smith Senior Planner \$135	De Novo Support/GIS \$105	Direct Costs	Totals
	<i>Task J</i>	\$ 3,500.00	\$ -	\$ -	\$ -	\$ -	\$ 400.00	\$ 3,900.00
<b>K Notice of Determination</b>								
K.1	Prepare Notice of Determination	1.0						1.0
K.2	File with State Clearinghouse	1.0					\$ 75.00	1.0
	<i>Subtotal</i>	2.0	0.0	0.0	0.0	0.0		2.0
	<i>Task K</i>	\$ 350.00	\$ -	\$ -	\$ -	\$ -	\$ 75.00	\$ 425.00
<b>L Administration/Project Management</b>								
L.1	Project Management/Coordination	17.0	2.0	8.0	8.0			35.0
L.2	Administration	10.0	1.0	8.0	8.0	4.0		31.0
	<i>Subtotal</i>	27.0	3.0	16.0	16.0	4.0		66.0
	<i>Task L</i>	\$ 4,725.00	\$ 525.00	\$ 2,160.00	\$ 2,160.00	\$ 420.00	\$ -	\$ 9,990.00
<b>De Novo Project Subtotals</b>								
	Project Subtotal Hours	104.3	23.0	110.0	51.0	25.5		313.75
	Project Subtotal Cost	\$ 18,243.75	\$ 4,025.00	\$ 14,850.00	\$ 6,885.00	\$ 2,677.50	\$ 9,331.25	\$56,012.50
<b>Total Project Cost</b>								<b>\$56,012.50</b>

## WORK SCHEDULE

<i>Project Task</i>	<i>Time Period (days)</i>	<i>Start</i>	<i>Finish</i>
<b>Task A and B– Project Initiation and Project Description</b>			
Notice to Proceed	-	15-Apr-21	15-Apr-21
Kickoff Call	2	15-Apr-21	17-Apr-21
Draft Project Description	1	15-Apr-21	16-Apr-21
<i>Staff Administrative Review</i>	<i>1</i>	<i>16-Apr-21</i>	<i>17-Apr-21</i>
Prepare Revised Project Description	1	17-Apr-21	18-Apr-21
<b>Task C – NOP</b>			
NOP	3	15-Apr-21	18-Apr-21
<i>Staff Administrative Review</i>	<i>2</i>	<i>18-Apr-21</i>	<i>20-Apr-21</i>
Complete Public NOP	1	20-Apr-21	21-Apr-21
<i>Staff Screencheck Review</i>	<i>1</i>	<i>21-Apr-21</i>	<i>22-Apr-21</i>
Statutory 30-day Public Review Period	30	22-Apr-21	22-May-21
Public Scoping Meeting	1	TBD (estimated 5/11/22)	TBD (estimated 5/11/22)
<b>Task D and E – Tech Reports and Admin Draft EIR</b>			
Health Risk Assessment	20	15-Apr-21	5-May-21
CalEEMod Modeling	20	15-Apr-21	5-May-21
Noise Report	20	15-Apr-21	5-May-21
Administrative Draft EIR	25	15-Apr-21	10-May-21
<i>Staff Administrative Review of Draft EIR</i>	<i>10</i>	<i>10-May-21</i>	<i>20-May-21</i>
<b>Task F – Public Draft EIR/NOC</b>			
Screen-check Draft EIR	5	20-May-21	25-May-21
<i>Staff Screencheck Review of Draft EIR</i>	<i>1</i>	<i>25-May-21</i>	<i>26-May-21</i>
Complete Public Draft EIR	3	26-May-21	29-May-21
<b>Statutory 45-day Public Review Period</b>	<b>45</b>	<b>29-May-21</b>	<b>13-Jul-21</b>
<b>Task G – Admin Final EIR</b>			
Complete Administrative Final EIR	21	13-Jul-21	3-Aug-21
<i>Staff Administrative Review</i>	<i>7</i>	<i>3-Aug-21</i>	<i>10-Aug-21</i>
<b>Task H – Final EIR and MMRP</b>			
Screen-check Final EIR and MMRP	7	10-Aug-21	17-Aug-21
<i>Staff Screen check Review of Final EIR</i>	<i>14</i>	<i>17-Aug-21</i>	<i>31-Aug-21</i>
Complete Final EIR and MMRP	2	17-Aug-21	19-Aug-21
Send Final EIR to all Commentors	2	19-Aug-21	21-Aug-21
<b>Task I – Findings of Fact/Overriding Considerations</b>			
Admin Findings / Overriding Considerations	7	21-Aug-21	28-Aug-21
<i>Staff Review Findings / Overriding Considerations</i>	<i>7</i>	<i>28-Aug-21</i>	<i>4-Sep-21</i>
Complete Findings / Overriding Considerations	7	4-Sep-21	11-Sep-21
<b>Task J – Public Hearings for Draft EIR</b>			
Public Hearing	10	11-Sep-21	21-Sep-21
<b>Task K – Notice of Determination</b>			
File NOD with SCH/County Clerk	5	21-Sep-21	26-Sep-21

## SAMPLE WORK PRODUCT

The link below will take you to a recent Draft EIR and Appendices for the Lumina at Machado Ranch Project in the City of Manteca, San Joaquin County. I have also provided you with a link to the State Clearinghouse CEQAnet database for all documents prepared for this project. Additional samples can be provided at your request.

[Planning Division Documents | City of Manteca](#)

[Lumina Ranch \(ca.gov\)](#)

## INSURANCE

We have reviewed the insurance requirements and will provide an insurance certification upon a contract award in accordance with the insurance specification.

## CONSULTANT AGREEMENT

The following are contractual text edits requested to the City Professional Services Agreement provided in Attachment 2 of the RFP.

- Paragraph 1.3 – Add *“Satisfactory work shall mean work that follows customary good professional standards. The CITY will inform CONSULTANT of any unsatisfactory work within 30-days of receipt of the work being delivered to CITY”*
- Paragraph 6.8 – Modify as follows: *“If Consultant is not a design professional performing “design professional” services under this Agreement, as that term is defined in Civil Code Section 2782.8, Consultant agrees to defend, indemnify, hold free and harmless the City, its elected and appointed officials, officers, agents and employees, at Consultant’s sole expense, from and against ~~any and~~ all claims, demands, actions, suits or other legal proceedings brought against the City, its elected and appointed officials, officers, agents and employees arising out of the performance of the Consultant, its employees, and/or authorized subcontractors, of the work undertaken pursuant to this Agreement. The defense obligation provided for hereunder shall apply whenever any claim, action, complaint or suit asserts liability against the City, its elected and appointed officials, officers, agents and employees based upon the work performed by the Consultant, its employees, and/or authorized subcontractors under this Agreement, whether or not the Consultant, its employees, and/or authorized subcontractors are specifically named or otherwise asserted to be liable. Notwithstanding the foregoing, the Consultant shall not be liable for the defense or indemnification of the City for claims, actions, complaints or suits arising out of the ~~sole active~~ negligence or willful misconduct of the City. This provision shall supersede and replace all other indemnity provisions contained either in the City’s specifications or Consultant’s Proposal, which shall be of no force and effect.*  
*Notwithstanding the foregoing, for any claim alleging the negligent performance of work by Consultant, the Consultant has no immediate obligation to provide the defense of the City. The Consultant will reimburse indemnified parties their reasonable defense costs ultimately determined to have been caused by the negligence of the Consultant and proportionate to the degree of fault of the Consultant.”*
- Paragraph 6.15 Responsibility for Errors – Add *“To the extent that any errors or omissions are attributable to the City, the City’s representatives, or information provided by others to the City, the Consultant shall not be responsible.”*



# De Novo Planning Group

A Land Use Planning, Design, and Environmental Firm

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De Novo is committed to practices that demonstrate sustainability and stewardship. Our company provides a working environment that enables our team members to make contributions to improving the environment in which we live.

## Sacramento Area Office

1020 Suncast Lane, Suite 106 | El Dorado Hills, CA 95762  
info@denovoplanning.com | TEL 916-580-9818

## Southern California Office

180 East Main Street # 108 | Tustin, CA 92780  
info@denovoplanning.com | TEL 714-453-7711

**CITY OF FORT BRAGG  
PROFESSIONAL SERVICES AGREEMENT  
WITH  
DE NOVO PLANNING GROUP**

THIS AGREEMENT is made and entered into this \_\_\_\_ day of April, 2022 (“Effective Date”), by and between the CITY OF FORT BRAGG, a municipal corporation, 416 N. Franklin Street, Fort Bragg, California 95437 (“City”), and DE NOVO PLANNING GROUP, a California corporation, 1020 Suncastr Lane, Suite 106, El Dorado Hills, California 95762 (“Consultant”).

**WITNESSETH:**

A. WHEREAS, City proposes to utilize the services of Consultant as an independent contractor to prepare an Environmental Impact Report for the proposed Grocery Outlet at 825, 848, and 851 South Franklin Street, Fort Bragg, as more fully described herein; and

B. WHEREAS, City and Consultant desire to contract for the specific services described in Exhibit “A” (the “Project”) and desire to set forth their rights, duties and liabilities in connection with the services to be performed; and

C. WHEREAS, no official or employee of City has a financial interest, within the provisions of Sections 1090-1092 of the California Government Code, in the subject matter of this Agreement.

D. WHEREAS, the legislative body of the City on April 25, 2022 by Resolution No. [REDACTED] authorized execution of this Agreement on behalf of the City in accordance with Chapter 3.20 of the City Municipal Code and/or other applicable law;

NOW, THEREFORE, for and in consideration of the mutual covenants and conditions contained herein, the parties hereby agree as follows:

**1.0. SERVICES PROVIDED BY CONSULTANT**

1.1. Scope of Work. Consultant shall provide the professional services described in the Consultant’s Proposal (“Proposal”), attached hereto as **Exhibit A** and incorporated herein by this reference.

1.2. Professional Practices. All professional services to be provided by Consultant pursuant to this Agreement shall be provided by personnel experienced in their respective fields and in a manner consistent with the standards of care, diligence and skill ordinarily exercised by professional consultants in similar fields and circumstances in accordance with sound professional practices. Consultant also warrants that it is familiar with all laws that may affect its performance of this Agreement and shall advise City of any changes in any laws that may affect Consultant’s performance of this Agreement. Consultant shall keep itself informed of State and Federal laws and regulations which in any manner affect those employed by it or in any way affect the performance of its service pursuant to this Agreement. The Consultant shall at all times observe and comply with all such laws and regulations. City officers and employees shall not be liable at law or in equity occasioned by failure of the Consultant to comply with this section.

1.3. Performance to Satisfaction of City. Consultant agrees to perform all the work to the complete satisfaction of the City as hereinafter specified. Evaluations of the work will be done by the City Manager or his or her designee. If the quality of work is not satisfactory, City in its

discretion has the right to:

- (a) Meet with Consultant to review the quality of the work and resolve the matters of concern;
- (b) Require Consultant to repeat the work at no additional fee until it is satisfactory; and/or
- (c) Terminate the Agreement as hereinafter set forth.

Satisfactory work shall mean work that follows customary good professional standards. The City will inform Consultant of any unsatisfactory work within 30-days of receipt of the work being delivered to City.

1.4. Warranty. Consultant warrants that it shall perform the services required by this Agreement in compliance with all applicable Federal and California employment laws, including, but not limited to, those laws related to minimum hours and wages; occupational health and safety; fair employment and employment practices; workers' compensation insurance and safety in employment; and all other Federal, State and local laws and ordinances applicable to the services required under this Agreement. Consultant shall indemnify and hold harmless City from and against all claims, demands, payments, suits, actions, proceedings, and judgments of every nature and description including attorneys' fees and costs, presented, brought, or recovered against City for or on account of any liability under any of the above-mentioned laws, which may be incurred by reason of Consultant's performance under this Agreement. To the extent that this Agreement may be funded by fiscal assistance from another governmental entity, Consultant and any subcontractors shall comply with all applicable rules and regulations to which City is bound by the terms of such fiscal assistance program.

1.5. Non-discrimination. In performing this Agreement, Consultant shall not engage in, nor permit its agents to engage in, discrimination in employment of persons because of their race, religion, color, national origin, ancestry, age, physical handicap, medical condition, marital status, sexual gender, sexual orientation, or disability except as permitted pursuant to Section 12940 of the Government Code. Such actions shall include, but not be limited to the following: employment, upgrading, demotion or transfer, recruitment or recruitment advertising, layoff or termination, rates of pay or other forms of compensation and selection for training, including apprenticeship. Consultant agrees to post in conspicuous places, available to employees and applicants for employment, a notice setting forth provisions of this non-discrimination clause.

Consultant shall, in all solicitations and advertisements for employees placed by, or on behalf of Consultant, state that all qualified applicants will receive consideration for employment without regard to race, religion, color, national origin, ancestry, age, physical handicap, medical condition, marital status, sexual gender, sexual orientation, or disability. Consultant shall cause the paragraphs contained in this Section to be inserted in all subcontracts for any work covered by the Agreement, provided that the foregoing provisions shall not apply to subcontracts for standard commercial supplies or raw materials.

1.6. Non-Exclusive Agreement. Consultant acknowledges that City may enter into agreements with other consultants for services similar to the services that are subject to this Agreement or may have its own employees perform services similar to those services contemplated by this Agreement.

1.7. Delegation and Assignment. This is a personal service contract, and the duties set forth herein shall not be delegated or assigned to any person or entity without the prior written

consent of City. Consultant may engage a subcontractor(s) as permitted by law and may employ other personnel to perform services contemplated by this Agreement at Consultant's sole cost and expense. All insurance requirements contained in this Agreement are independently applicable to any and all subcontractors that Consultant may engage during the term of this Agreement.

1.8. Confidentiality. Employees of Consultant in the course of their duties may have access to financial, accounting, statistical, and personnel data of private individuals and employees of City. Consultant covenants that all data, documents, discussion, or other information developed or received by Consultant or provided for performance of this Agreement are deemed confidential and shall not be disclosed by Consultant without written authorization by City. City shall grant such authorization if disclosure is required by law. All City data shall be returned to City upon the termination of this Agreement. Consultant's covenant under this Section shall survive the termination of this Agreement.

## **2.0. COMPENSATION AND BILLING**

2.1. Compensation. Consultant's total compensation shall not exceed **Fifty-six Thousand Thirteen Dollars (\$56,013.00)**.

2.2. Additional Services. Consultant shall not receive compensation for any services provided outside the scope of work specified in the Consultant's Proposal or which is inconsistent with or in violation of the provisions of this Agreement unless the City or the Project Manager for this Project, prior to Consultant performing the additional services, approves such additional services in writing. It is specifically understood that oral requests and/or approvals of such additional services or additional compensation shall be barred and are unenforceable. Should the City request in writing additional services that increase the hereinabove described "Scope of Work," an additional fee based upon the Consultant's standard hourly rates shall be paid to the Consultant for such additional services. The City Manager may approve contract change orders not exceeding a total of 10% of the approved contract or up to the contingency amount whichever amount is less for any one project.

2.3. Method of Billing. Consultant may submit invoices to the City for approval on a progress basis, but not more often than monthly. Said invoice shall be based on the total of all Consultant's services which have been completed to City's sole satisfaction. City shall pay Consultant's invoice within forty-five (45) days from the date City receives said invoice. Each invoice shall describe in detail, the services performed, the date of performance, and the associated time for completion. Any additional services approved and performed pursuant to this Agreement shall be designated as "Additional Services" and shall identify the number of the authorized change order, where applicable, on all invoices.

2.4. Records and Audits. Records of Consultant's services relating to this Agreement shall be maintained in accordance with generally recognized accounting principles and shall be made available to City or its Project Manager for inspection and/or audit at mutually convenient times for a period of three (3) years from the date of final payment.

## **3.0. TIME OF PERFORMANCE**

3.1. Commencement and Completion of Work. The professional services to be performed pursuant to this Agreement shall commence within five (5) days from the issuance of Notice to Proceed. Said services shall be performed in strict compliance with the schedule set forth in the Scope of Work attached hereto as **Exhibit A**. Consultant will complete the services in accordance with this Agreement by **December 30, 2022**. The Time of Completion may only be

modified by a written amendment of the Agreement signed by both the City and the Consultant and in accordance with its terms.

3.2. Excusable Delays. Neither party shall be responsible for delays or lack of performance resulting from acts beyond the reasonable control of the party or parties. Such acts shall include, but not be limited to, acts of God, fire, strikes, material shortages, compliance with laws or regulations, riots, acts of war, or any other conditions beyond the reasonable control of a party. If a delay beyond the control of the Consultant is encountered, a time extension may be mutually agreed upon in writing by the City and the Consultant. The Consultant shall present documentation satisfactory to the City to substantiate any request for a time extension.

#### 4.0. TERM AND TERMINATION

4.1. Term. This Agreement shall commence on the Effective Date and expire on **March 30, 2023** unless previously terminated as provided herein or as otherwise agreed to in writing by the parties.

4.2. Notice of Termination. The City reserves and has the right and privilege of canceling, suspending or abandoning the execution of all or any part of the work contemplated by this Agreement, with or without cause, at any time, by providing at least ten (10) days prior written notice to Consultant. The termination of this Agreement shall be deemed effective upon receipt of the notice of termination. In the event of such termination, Consultant shall immediately stop rendering services under this Agreement unless directed otherwise by the City. If the City suspends, terminates or abandons a portion of this Agreement, such suspension, termination or abandonment shall not make void or invalidate the remainder of this Agreement.

If the Consultant defaults in the performance of any of the terms or conditions of this Agreement, it shall have ten (10) days after service upon it of written notice of such default in which to cure the default by rendering a satisfactory performance. In the event that the Consultant fails to cure its default within such period of time, the City shall have the right, notwithstanding any other provision of this Agreement, to terminate this Agreement without further notice and without prejudice to any other remedy to which it may be entitled at law, in equity or under this Agreement.

The City shall have the right, notwithstanding any other provisions of this Agreement, to terminate this Agreement, at its option and without prejudice to any other remedy to which it may be entitled at law, in equity or under this Agreement, immediately upon service of written notice of termination on the Consultant, if the latter should:

- a. Be adjudged a bankrupt;
- b. Become insolvent or have a receiver of its assets or property appointed because of insolvency;
- c. Make a general assignment for the benefit of creditors;
- d. Default in the performance of any obligation or payment of any indebtedness under this Agreement;
- e. Suffer any judgment against it to remain unsatisfied or unbonded of record for thirty (30) days or longer; or
- f. Institute or suffer to be instituted any procedures for reorganization or

rearrangement of its affairs.

4.3. Compensation. In the event of termination, City shall pay Consultant for reasonable costs incurred and professional services satisfactorily performed up to and including the date of City's written notice of termination within thirty-five (35) days after service of the notice of termination. Compensation for work in progress shall be prorated based on the percentage of work completed as of the effective date of termination in accordance with the fees set forth herein. In ascertaining the professional services actually rendered hereunder up to the effective date of termination of this Agreement, consideration shall be given to both completed work and work in progress, to complete and incomplete drawings, and to other documents pertaining to the services contemplated herein whether delivered to the City or in the possession of the Consultant. City shall not be liable for any claim of lost profits.

4.4. Documents. In the event of termination of this Agreement, all documents prepared by Consultant in its performance of this Agreement including, but not limited to, finished or unfinished design, development and construction documents, data studies, drawings, maps and reports, shall be delivered to the City within ten (10) days of delivery of termination notice to Consultant, at no cost to City. Any use of uncompleted documents without specific written authorization from Consultant shall be at City's sole risk and without liability or legal expense to Consultant.

## **5.0. INSURANCE**

5.1. Minimum Scope and Limits of Insurance. Consultant shall obtain, maintain, and keep in full force and effect during the life of this Agreement all of the following minimum scope of insurance coverages with an insurance company admitted to do business in California, rated "A," Class X, or better in the most recent Best's Key Insurance Rating Guide, and approved by City:

- (a) Broad-form commercial general liability, in a form at least as broad as ISO form #CG 20 01 04 13, including premises-operations, products/ completed operations, broad form property damage, blanket contractual liability, independent contractors, personal injury or bodily injury with a policy limit of not less than One Million Dollars (\$1,000,000.00) per occurrence, Two Million Dollars (\$2,000,000.00) aggregate, combined single limits. If such insurance contains a general aggregate limit, it shall apply separately to this Agreement or shall be twice the required occurrence limit. If Consultant maintains higher limits than the specified minimum limits, City requires and shall be entitled to coverage for the high limits maintained by the Consultant.
- (b) Business automobile liability for owned vehicles, hired, and non-owned vehicles, with a policy limit of not less than One Million Dollars (\$1,000,000.00), combined single limits, each incident for bodily injury and property damage.
- (c) Workers' compensation insurance as required by the State of California and Employers Liability Insurance with a minimum limit of \$1,000,000 per accident for any employee or employees of Consultant. Consultant agrees to waive, and to obtain endorsements from its workers' compensation insurer waiving subrogation rights under its workers' compensation insurance policy against the City, its officials, officers, agents, employees, and volunteers for losses arising from work performed by Consultant for

the City and to require each of its subcontractors, if any, to do likewise under their workers' compensation insurance policies.

Before execution of this Agreement by the City, the Consultant shall file with the City Clerk the following signed certification:

I am aware of, and will comply with, Section 3700 of the Labor Code, requiring every employer to be insured against liability of Workers' Compensation or to undertake self-insurance before commencing any of the work.

The Consultant shall also comply with Section 3800 of the Labor Code by securing, paying for and maintaining in full force and effect for the duration of this Agreement, complete Workers' Compensation Insurance, and shall furnish a Certificate of Insurance to the City Clerk before execution of this Agreement by the City. The City, its officers and employees shall not be responsible for any claims in law or equity occasioned by failure of the consultant to comply with this section.

- (d) Professional errors and omissions ("E&O") liability insurance with policy limits of not less than Two Million Dollars (\$2,000,000.00), combined single limits, per occurrence and aggregate. Architects' and engineers' coverage shall be endorsed to include contractual liability. If the policy is written as a "claims made" policy, the retro date shall be prior to the start of the contract work. Consultant shall obtain and maintain said E&O liability insurance during the life of this Agreement and for three years after completion of the work hereunder.

Neither the City nor any of its elected or appointed officials, officers, agents, employees, or volunteers makes any representation that the types of insurance and the limits specified to be carried by Consultant under this Agreement are adequate to protect Consultant. If Consultant believes that any such insurance coverage is insufficient, Consultant shall provide, at its own expense, such additional insurance as Consultant deems adequate.

5.2. Endorsements. The commercial general liability insurance policy and business automobile liability policy shall contain or be endorsed to contain the following provisions:

- (a) Additional insureds: "The City of Fort Bragg and its elected and appointed boards, officers, officials, agents, employees, and volunteers are additional insureds with respect to: liability arising out of activities performed by or on behalf of the Consultant pursuant to its contract with the City; products and completed operations of the Consultant; premises owned, occupied or used by the Consultant; automobiles owned, leased, hired, or borrowed by the Consultant."
- (b) Notice: "Consultant shall provide immediate written notice if (1) any of the required insurance policies is terminated; (2) the limits of any of the required policies are reduced; or (3) the deductible or self-insured retention is increased. In the event of any cancellation or reduction in coverage or limits of any insurance, Consultant shall forthwith obtain and submit proof of substitute insurance. Should Consultant fail to immediately procure other insurance, as specified, to substitute for any canceled policy, the City may procure such insurance at Consultant's sole cost and expense."

- (c) Other insurance: "The Consultant's insurance coverage shall be primary insurance as respects the City of Fort Bragg, its officers, officials, agents, employees, and volunteers. Any other insurance maintained by the City of Fort Bragg shall be excess and not contributing with the insurance provided by this policy."
- (d) Any failure to comply with the reporting provisions of the policies shall not affect coverage provided to the City of Fort Bragg, its officers, officials, agents, employees, and volunteers.
- (e) The Consultant's insurance shall apply separately to each insured against whom claim is made or suit is brought, except with respect to the limits of the insurer's liability.

5.3. Deductible or Self-Insured Retention. If any of such policies provide for a deductible or self-insured retention to provide such coverage, the amount of such deductible or self-insured retention shall be approved in advance by City. No policy of insurance issued as to which the City is an additional insured shall contain a provision which requires that no insured except the named insured can satisfy any such deductible or self-insured retention.

5.4. Certificates of Insurance. Consultant shall provide to City certificates of insurance showing the insurance coverages and required endorsements described above, in a form and content approved by City, prior to performing any services under this Agreement. The certificates of insurance and endorsements shall be attached hereto as **Exhibit B** and incorporated herein by this reference.

5.5. Non-limiting. Nothing in this Section shall be construed as limiting in any way, the indemnification provision contained in this Agreement, or the extent to which Consultant may be held responsible for payments of damages to persons or property.

## 6.0. GENERAL PROVISIONS

6.1. Entire Agreement. This Agreement constitutes the entire agreement between the parties with respect to any matter referenced herein and supersedes any and all other prior writings and oral negotiations. This Agreement may be modified only in writing, and signed by the parties in interest at the time of such modification. The terms of this Agreement shall prevail over any inconsistent provision in any other contract document appurtenant hereto, including exhibits to this Agreement.

6.2. Representatives. The City Manager or his or her designee shall be the representative of City for purposes of this Agreement and may issue all consents, approvals, directives and agreements on behalf of the City, called for by this Agreement, except as otherwise expressly provided in this Agreement.

Consultant shall designate a representative for purposes of this Agreement who shall be authorized to issue all consents, approvals, directives and agreements on behalf of Consultant called for by this Agreement, except as otherwise expressly provided in this Agreement.

6.3. Project Managers. The Project Manager designated to work directly with Consultant in the performance of this Agreement will be **Heather Gurewitz**, Associate Planner. It shall be the Consultant's responsibility to assure that the Project Manager is kept informed of the

progress of the performance of the services and the Consultant shall refer any decision, which must be made by City, to the Project Manager. Unless otherwise specified herein, any approval of City required hereunder shall mean the approval of the Project Manager.

Consultant designates **Steve McMurtry**, Principal Planner, as its Project Manager, who shall represent it and be its agent in all consultations with City during the term of this Agreement and who shall not be changed by Consultant without the express written approval by the City. Consultant or its Project Manager shall attend and assist in all coordination meetings called by City.

6.4. Notices. Any notices, documents, correspondence or other communications concerning this Agreement or the work hereunder may be provided by personal delivery, facsimile or if mailed, shall be addressed as set forth below and placed in a sealed envelope, postage prepaid, and deposited in the United States Postal Service. Such communication shall be deemed served or delivered: a) at the time of delivery if such communication is sent by personal delivery; b) at the time of transmission if such communication is sent by facsimile; and c) 72 hours after deposit in the U.S. Mail as reflected by the official U.S. postmark if such communication is sent through regular United States mail.

IF TO CONSULTANT:  
Steve McMurtry  
De Novo Planning Group  
1020 Suncastr Lane, Suite 106  
El Dorado Hills, CA 95762  
Tel: 916-580-9818

IF TO CITY:  
City Clerk  
City of Fort Bragg  
416 N. Franklin St.  
Fort Bragg, CA 95437  
Tel: 707-961-2823  
Fax: 707-961-2802

6.5. Attorneys' Fees. In the event that litigation is brought by any party in connection with this Agreement, the prevailing party shall be entitled to recover from the opposing party all costs and expenses, including reasonable attorneys' fees, incurred by the prevailing party in the exercise of any of its rights or remedies hereunder or the enforcement of any of the terms, conditions, or provisions hereof.

6.6. Governing Law. This Agreement shall be governed by and construed under the laws of the State of California without giving effect to that body of laws pertaining to conflict of laws. In the event of any legal action to enforce or interpret this Agreement, the parties hereto agree that the sole and exclusive venue shall be a court of competent jurisdiction located in Mendocino County, California. Consultant agrees to submit to the personal jurisdiction of such court in the event of such action.

6.7. Assignment. Consultant shall not voluntarily or by operation of law assign, transfer, sublet or encumber all or any part of Consultant's interest in this Agreement without City's prior written consent. Any attempted assignment, transfer, subletting or encumbrance shall be void and shall constitute a breach of this Agreement and cause for termination of this Agreement. Regardless of City's consent, no subletting or assignment shall release Consultant of Consultant's obligation to perform all other obligations to be performed by Consultant hereunder for the term of this Agreement.

6.8. Indemnification and Hold Harmless.

If Consultant is not a design professional performing "design professional" services under this Agreement, as that term is defined in Civil Code Section 2782.8, Consultant agrees to defend, indemnify, hold free and harmless the City, its elected and appointed officials, officers, agents

and employees, at Consultant's sole expense, from and against any and all claims, demands, actions, suits or other legal proceedings brought against the City, its elected and appointed officials, officers, agents and employees arising out of the performance of the Consultant, its employees, and/or authorized subcontractors, of the work undertaken pursuant to this Agreement. The defense obligation provided for hereunder shall apply whenever any claim, action, complaint or suit asserts liability against the City, its elected and appointed officials, officers, agents and employees based upon the work performed by the Consultant, its employees, and/or authorized subcontractors under this Agreement, whether or not the Consultant, its employees, and/or authorized subcontractors are specifically named or otherwise asserted to be liable. Notwithstanding the foregoing, the Consultant shall not be liable for the defense or indemnification of the City for claims, actions, complaints or suits arising out of the sole active negligence or willful misconduct of the City. This provision shall supersede and replace all other indemnity provisions contained either in the City's specifications or Consultant's Proposal, which shall be of no force and effect.

If Consultant is a design professional performing "design professional" services under this Agreement, as that term is defined in Civil Code Section 2782.8, Consultant agrees to defend, indemnify, hold free and harmless the City, its elected and appointed officials, officers, agents and employees, at Consultant's sole expense, from and against any and all claims, demands, actions, suits or other legal proceedings arising out of, pertaining to, or relating to the negligence, recklessness, or willful misconduct of Consultant. The defense obligation provided for hereunder shall apply whenever any claim, action, complaint or suit asserts liability against the City, its elected and appointed officials, officers, agents and employees based upon the negligence, recklessness, or willful misconduct of the Consultant, its employees, and/or authorized subcontractors under this Agreement, whether or not the Consultant, its employees, and/or authorized subcontractors are specifically named or otherwise asserted to be liable. Notwithstanding the foregoing, the Consultant shall not be liable for the defense or indemnification of the City for claims, actions, complaints or suits arising out of the sole active negligence or willful misconduct of the City. This provision shall supersede and replace all other indemnity provisions contained either in the City's specifications or Consultant's Proposal, which shall be of no force and effect.

6.9. Independent Contractor. Consultant is and shall be acting at all times as an independent contractor and not as an employee of City. Consultant shall have no power to incur any debt, obligation, or liability on behalf of City or otherwise act on behalf of City as an agent. Neither City nor any of its agents shall have control over the conduct of Consultant or any of Consultant's employees, except as set forth in this Agreement. Consultant shall not, at any time, or in any manner, represent that it or any of its or employees are in any manner agents or employees of City. Consultant shall secure, at its sole expense, and be responsible for any and all payment of Income Tax, Social Security, State Disability Insurance Compensation, Unemployment Compensation, and other payroll deductions for Consultant and its officers, agents, and employees, and all business licenses, if any are required, in connection with the services to be performed hereunder. Consultant shall indemnify and hold City harmless from any and all taxes, assessments, penalties, and interest asserted against City by reason of the independent contractor relationship created by this Agreement. Consultant further agrees to indemnify and hold City harmless from any failure of Consultant to comply with the applicable worker's compensation laws. City shall have the right to offset against the amount of any fees due to Consultant under this Agreement any amount due to City from Consultant as a result of Consultant's failure to promptly pay to City any reimbursement or indemnification arising under this paragraph.

6.10. PERS Eligibility Indemnification. In the event that Consultant or any employee, agent, or subcontractor of Consultant providing services under this Agreement claims or is

determined by a court of competent jurisdiction or the California Public Employees Retirement System (PERS) to be eligible for enrollment in PERS as an employee of the City, Consultant shall indemnify, defend, and hold harmless City for the payment of any employee and/or employer contributions for PERS benefits on behalf of Consultant or its employees, agents, or subcontractors, as well as for the payment of any penalties and interest on such contributions, which would otherwise be the responsibility of City.

Notwithstanding any other agency, state or federal policy, rule, regulation, law or ordinance to the contrary, Consultant and any of its employees, agents, and subcontractors providing service under this Agreement shall not qualify for or become entitled to, and hereby agree to waive any claims to, any compensation, benefit, or any incident of employment by City, including but not limited to eligibility to enroll in PERS as an employee of City and entitlement to any contribution to be paid by City for employer contribution and/or employee contributions for PERS benefits.

6.11. Cooperation. In the event any claim or action is brought against City relating to Consultant's performance or services rendered under this Agreement, Consultant shall render any reasonable assistance and cooperation which City might require.

6.12. Ownership of Documents. All findings, reports, documents, information and data including, but not limited to, computer tapes or discs, preliminary notes, working documents, files and tapes furnished or prepared by Consultant or any of its subcontractors in the course of performance of this Agreement, shall be and remain the sole property of City. Consultant agrees that any such documents or information shall not be made available to any individual or organization without the prior consent of City, but shall be made available to the City within ten (10) days of request or within ten (10) days of termination. Any use of such documents for other projects not contemplated by this Agreement, and any use of incomplete documents, shall be at the sole risk of City and without liability or legal exposure to Consultant. City shall indemnify and hold harmless Consultant from all claims, damages, losses, and expenses, including attorneys' fees, arising out of or resulting from City's use of such documents for other projects not contemplated by this Agreement or use of incomplete documents furnished by Consultant. Consultant shall deliver to City any findings, reports, documents, information, data, preliminary notes and working documents, in any form, including but not limited to, computer tapes, discs, files audio tapes or any other Project related items as requested by City or its authorized representative, at no additional cost to the City. Consultant or Consultant's agents shall execute such documents as may be necessary from time to time to confirm City's ownership of the copyright in such documents.

6.13. Public Records Act Disclosure. Consultant has been advised and is aware that this Agreement and all reports, documents, information and data, including, but not limited to, computer tapes, discs or files furnished or prepared by Consultant, or any of its subcontractors, pursuant to this Agreement and provided to City may be subject to public disclosure as required by the California Public Records Act (California Government Code Section 6250 *et seq.*). Exceptions to public disclosure may be those documents or information that qualify as trade secrets, as that term is defined in the California Government Code Section 6254.7, and of which Consultant informs City of such trade secret. The City will endeavor to maintain as confidential all information obtained by it that is designated as a trade secret. The City shall not, in any way, be liable or responsible for the disclosure of any trade secret including, without limitation, those records so marked if disclosure is deemed to be required by law or by order of the Court.

6.14. Conflict of Interest. Consultant and its officers, employees, associates and subconsultants, if any, will comply with all conflict of interest statutes of the State of California applicable to Consultant's services under this agreement, including, but not limited to, the Political

Reform Act (Government Code Sections 81000, *et seq.*) and Government Code Section 1090. During the term of this Agreement, Consultant and its officers, employees, associates and subconsultants shall not, without the prior written approval of the City Representative, perform work for another person or entity for whom Consultant is not currently performing work that would require Consultant or one of its officers, employees, associates or subconsultants to abstain from a decision under this Agreement pursuant to a conflict of interest statute.

6.15. Responsibility for Errors. Consultant shall be responsible for its work and results under this Agreement. Consultant, when requested, shall furnish clarification and/or explanation as may be required by the City's representative, regarding any services rendered under this Agreement at no additional cost to City. In the event that an error or omission attributable to Consultant occurs, then Consultant shall, at no cost to City, provide all necessary design drawings, estimates and other Consultant professional services necessary to rectify and correct the matter to the sole satisfaction of City and to participate in any meeting required with regard to the correction. To the extent that any errors or omissions are attributable to the City, the City's representatives, or information provided by others to the City, the Consultant shall not be responsible.

6.16. Prohibited Employment. Consultant will not employ any regular employee of City while this Agreement is in effect.

6.17. Order of Precedence. In the event of an inconsistency in this Agreement and any of the attached Exhibits, the terms set forth in this Agreement shall prevail. If, and to the extent this Agreement incorporates by reference any provision of any document, such provision shall be deemed a part of this Agreement. Nevertheless, if there is any conflict among the terms and conditions of this Agreement and those of any such provision or provisions so incorporated by reference, the conflict shall be resolved by giving precedence in the following order, if applicable: This Agreement, the City's Request for Proposals, the Consultant's Proposal.

6.18. Costs. Each party shall bear its own costs and fees incurred in the preparation and negotiation of this Agreement and in the performance of its obligations hereunder except as expressly provided herein.

6.19. No Third Party Beneficiary Rights. This Agreement is entered into for the sole benefit of City and Consultant and no other parties are intended to be direct or incidental beneficiaries of this Agreement and no third party shall have any right in, under or to this Agreement.

6.20. Headings. Paragraph and subparagraph headings contained in this Agreement are included solely for convenience and are not intended to modify, explain or to be a full or accurate description of the content thereof and shall not in any way affect the meaning or interpretation of this Agreement.

6.21. Construction. The parties have participated jointly in the negotiation and drafting of this Agreement. In the event an ambiguity or question of intent or interpretation arises with respect to this Agreement, this Agreement shall be construed as if drafted jointly by the parties and in accordance with its fair meaning. There shall be no presumption or burden of proof favoring or disfavoring any party by virtue of the authorship of any of the provisions of this Agreement.

6.22. Amendments. Only a writing executed by the parties hereto or their respective successors and assigns may amend this Agreement.

6.23. Waiver. The delay or failure of either party at any time to require performance or

compliance by the other of any of its obligations or agreements shall in no way be deemed a waiver of those rights to require such performance or compliance. No waiver of any provision of this Agreement shall be effective unless in writing and signed by a duly authorized representative of the party against whom enforcement of a waiver is sought. The waiver of any right or remedy in respect to any occurrence or event shall not be deemed a waiver of any right or remedy in respect to any other occurrence or event, nor shall any waiver constitute a continuing waiver.

6.24. Severability. If any provision of this Agreement is determined by a court of competent jurisdiction to be unenforceable in any circumstance, such determination shall not affect the validity or enforceability of the remaining terms and provisions hereof or of the offending provision in any other circumstance. Notwithstanding the foregoing, if the value of this Agreement, based upon the substantial benefit of the bargain for any party, is materially impaired, which determination made by the presiding court or arbitrator of competent jurisdiction shall be binding, then both parties agree to substitute such provision(s) through good faith negotiations.

6.25. Counterparts. This Agreement may be executed in one or more counterparts, each of which shall be deemed an original. All counterparts shall be construed together and shall constitute one agreement.

6.26. Corporate Authority. The persons executing this Agreement on behalf of the parties hereto warrant that they are duly authorized to execute this Agreement on behalf of said parties and that by doing so the parties hereto are formally bound to the provisions of this Agreement.

6.27. Use of Recycled Paper Products. In the performance of this Agreement, Consultant shall use paper products and printing and writing paper that meets Federal Trade Commission recyclability standards as defined in 16 CFR 260.12.

**IN WITNESS WHEREOF**, the parties hereto have caused this Agreement to be executed by and through their respective authorized officers, as of the date first above written.

CITY

CONSULTANT

By: \_\_\_\_\_

David Spaur

Its: City Manager

By: \_\_\_\_\_

Steve McMurtry

Its: Principal Planner

ATTEST:

By: \_\_\_\_\_

June Lemos, MMC

City Clerk

APPROVED AS TO FORM:

By: \_\_\_\_\_

Keith F. Collins

City Attorney

## SCOPE OF WORK

### TASK A – PROJECT INITIATION

Within one week of project commencement, the De Novo team will meet with City Staff to discuss the following:

- Refinement of project work scope and schedule,
- City preferences for point of contact, method of communication, meeting responsibilities, project updates, etc.
- Collection of relevant background documents (adopted documents, reports, and studies), and
- Project deliverables.

### TASK B – PREPARE PROJECT DESCRIPTION

De Novo will prepare a detailed description including text and graphics utilizing the information provided to the City by the applicant and applicant’s engineer. The project description will include a regional and local setting, project history and land uses, past ownership, objectives, characteristics, important project features including discretionary actions and entitlements, consistency with the General Plan and zoning designations, a list of responsible and other agencies expected to use the product document in decision making, and a list of approvals for which the product document will be used. We will provide the draft project description to the City staff for review and comment. Upon receipt of comments from the City staff we will finalize the project description for use in the NOP/Initial Study as described in Task C, which will involve a second review of the Project Description along with the Initial Study.

### TASK C – NOP/INITIAL STUDY

De Novo will prepare an Initial Study and Notice of Preparation (NOP) in an administrative draft form for City staff to review. Comments received will be incorporated into the Initial Study and Notice of Preparation for public circulation. After the document is “Screen Checked” by City staff, we will finalize the document. The public draft will be published and distributed with the proper notices to the State Clearinghouse. We will provide City staff with a copy to be filed by the City with the County Clerk, and a newspaper of regional circulation. The results of the Initial Study and NOP will be presented at a public scoping meeting in coordination with City staff.

The intent of the Initial Study/NOP and public scoping meeting is to narrow the focus of the environmental analysis in the EIR to the most pertinent and relevant environmental issues. The Initial Study will review the following topics: Aesthetics, Agricultural Resources, Air Quality, Biological Resources, Cultural Resources, Energy, Greenhouse Gas Emissions, Hazards and Hazardous Materials, Hydrology and Water Quality, Land Use Planning, Mineral Resources, Population, Public Services, Noise, Recreation, Traffic, Tribal Resources, and Utilities. Once the Initial Study is completed, we may find that the project will not have significant adverse impacts

on certain topics and a cursory level of analysis is all that would be needed in the EIR for that particular topic. For other topics, we would require a full EIR analysis of the topic.

From our initial review of the project relative, we believe that the following topics will be scoped out of the EIR: Aesthetics/Visual Resources, Agricultural Resources, Cultural and Tribal Resources, Geology/Soils/Seismicity, Hazards and Hazardous Materials, Hydrology and Water Quality, Mineral Resources, Population and Housing, Public Services/Recreation, and Wildfires. If the above topics are scoped out during the NOP phase of the project, we would have the following topics be the focus of the EIR: Air Quality, Biological Resources, Energy, Greenhouse Gas Emissions, Land Use Planning, Noise, Traffic, and Utilities. This Scope of Work is based on these assumptions. Additional topics may need to be added to the scope after the NOP phase concludes, which would require additional budget.

The Initial Study and Scoping Meeting will also serve as an opportunity to define and solidify the project alternatives in a public forum. This will be an important step during this project task in an effort to streamline the Environmental Review process. We will review all available community information to assist in the development of up to four (4) project alternatives to be addressed in the EIR.

*Deliverables: One (1) electronic copy of the Admin Draft NOP with appendices, in MS Word and PDF format. One (1) electronic copy of the Final NOP with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK D – TECHNICAL STUDIES

The following discusses the individual technical studies that will be prepared for the project.

#### HEALTH RISK ASSESSMENT (HRA)

The objective of the HRA is to determine the public health risks from existing emissions from nearby rail and other toxic air sources, combined with the new public health risks from project-related traffic. The HRA consists of two main tasks: 1. Exposure Assessment, and 2. Risk Calculations.

- *Task 1 Exposure Assessment:* The main steps involved in exposure assessment includes estimating the emission rates of toxic air pollutants and running an air dispersion model to calculate the concentration of a toxic air pollutant at each location in the modeling domain. The modeling domain would include proposed project site, portions of adjacent roadways, and any other known source(s) of toxic air pollutants proximate to the site. The main toxic air contaminant (TAC) associated with industrial uses is diesel particulate matter (DPM), although various manufacturing uses also emit TACs. The emission rates of DPM will be calculated on the basis of site-specific characteristics. We propose to use the AERMOD air dispersion model with 5 years of meteorological data collected at the closest monitoring station. The proposed dispersion model is recommended by the Air District.

- *Task 2 Risk Calculations:* We propose to calculate the residential (70 year) cancer health risks by multiplying the concentration of DPM by its unit risk factor. The current unit risk factor recommended by the Office of Environmental Health Hazard Assessment is  $4.15 \times 10^{-4}$  (ug/m<sup>3</sup>)-1. The resulting cancer risks will be plotted on the base map.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts on air quality.

#### RUN CALCEEMOD

The project's unmitigated and mitigated NO<sub>x</sub> and PM<sub>10</sub> emissions will be modelled with the CalEEMod. The model will incorporate project details as provided to the City by the Project Applicant. We will confer with the City and Project Applicant regarding the feasibility of a wide variety of project NO<sub>x</sub> and PM<sub>10</sub> emissions reduction (mitigation) measures.

#### NOISE STUDY

The De Novo team includes Saxelby Acoustics to prepare a Noise and Vibration Study. The following outlines the scope of work for this study:

1. 1A. Existing Noise Environment:
  - **Traffic Noise:** Existing noise levels due to nearby transportation noise sources will be quantified. Saxelby Acoustics uses the Federal Highway Administration (FHWA) traffic noise prediction model for the prediction of traffic noise levels. Direct inputs to the traffic model will include traffic data provided by the project traffic consultant, existing posted speed limits, truck count information, and 24-hour traffic split data collected by Saxelby Acoustics.
  - **Community Noise Survey:** Saxelby Acoustics will conduct a noise survey within the project site to quantify existing background noise levels. The noise survey will consist of short-term noise level measurements and continuous noise level measurements for a minimum period of 24-hours.
2. **Analysis of Transportation Noise Environment:** Saxelby Acoustics will evaluate increased traffic noise levels at existing sensitive receptors in the project vicinity. This task will be performed using traffic volumes provided by the traffic engineer. We anticipate providing traffic noise levels for existing, existing plus project, cumulative, and cumulative plus project scenarios. However, should additional scenarios be included in the traffic study, we will also evaluate those scenarios. We will also calculate exterior and interior traffic noise levels on the proposed residential uses. If necessary, we will evaluate any required exterior or interior noise control measures needed to achieve compliance with the City noise level standards.
3. **Analysis of Stationary Noise Environment:** Saxelby Acoustics will provide an analysis of the noise and vibration impacts associated with construction of the project at existing

sensitive receptors in the project vicinity. It is expected that this analysis will follow the assumptions used in the project air quality analysis.

4. Report Preparation: Saxelby Acoustics will provide a draft report which details our findings, methodology, and noise reduction measures (if required). The report will be prepared to meet the requirements of the City and CEQA.
5. Response to Comments: Saxelby Acoustics will respond to comments on the draft technical report. After comments are received, a final report will be provided.

#### TASK E –ADMINISTRATIVE DRAFT EIR

De Novo will prepare the project-level EIR for the project in an administrative draft form for City staff to review. The EIR will be intended to provide the information and environmental analysis necessary to assist public agency decision-makers in considering approval of the project.

The EIR will consider the potential environmental effects of the project to determine the level of significance and will analyze these potential effects to the detail necessary to make these determinations on significance. Each section will include GIS graphics and figures to create an easy to comprehend document that is user-friendly. It is noted that the scope of the EIR is focused on the following topics: Air Quality, Biological Resources, Energy, Greenhouse Gas Emissions, Land Use Planning, Noise, Traffic, and Utilities.

The EIR will consist of the following sections:

#### EXECUTIVE SUMMARY

This section will provide a concise description of the project, the potential areas of controversy, issues to be resolved, project alternatives, and a summary of impacts and mitigation measures. The intent of this section is to provide the City and the public with a simple and easy to understand overview of the project and related issues, which will be analyzed and discussed much more thoroughly in the contents of the EIR.

#### INTRODUCTION

The Introduction will serve as an overview of the EIR, describing its purpose and relevant environmental review procedures, the document organization, and the methodology used.

#### PROJECT DESCRIPTION

The Project Description section will consist of a detailed description of the project (See Task B), including the proposed actions, the project goals and objectives, and the relationship of the project to other regional plans and projects. This section will also present the City's and other agency involvement in the project, and the use of the EIR by other agencies, including permits and approvals. This section will be consistent with the requirements of State CEQA Guidelines Section 15124.

De Novo will prepare the Project Description prior to including it in the Initial Study and Notice of Preparation. The Project Description will be provided in an administrative draft form for City staff to review. Comments received will be incorporated into the revised Project Description and included in the Initial Study and Notice of Preparation. After the document is “Screen Checked” by City staff, we will finalize the Project Description for public release.

#### ENVIRONMENTAL SETTING, IMPACTS, AND MITIGATION MEASURES

The Environmental Setting, Impacts, and Mitigation Measures section will present a detailed discussion of each individual environmental topic. Each discussion will include the following:

- An environmental setting and environmental baseline conditions (including figures and GIS graphics);
- The applicable local, state, and federal regulatory setting;
- The threshold of significance used for each impact determination;
- The methodology used for conducting the environmental analysis and making significance determinations;
- An analysis of all identified direct and indirect impacts associated with project;
- An analysis of the cumulative impacts associated with the project;
- Identification of mitigation measures to reduce impacts; and
- A determination of the significance of each impact after mitigation.

De Novo will work closely with City staff to formulate the appropriate mitigation measure language and timing that is appropriate for inclusion in the EIR. Each EIR section will be organized concisely for ease of use and future reference.

#### AIR QUALITY

The project will include an assessment of short-term construction-related emissions and long-term operational emissions, primarily attributable to emissions from vehicle trips and from energy consumption by the uses. We will consult with the local Air District regarding the project’s potential to cause impacts, and the applicability of the Air District’s Rules and Regulations. The Air Quality analysis will include the following:

- Regional air quality and local air quality in the vicinity of the project site will be described. Meteorological conditions in the vicinity of the project site that could affect air pollutant dispersal or transport will be described. Applicable air quality regulatory framework, standards, and significance thresholds will be discussed.
- Short-term (i.e., construction) increases in regional criteria air pollutants will be quantitatively assessed. The ARB-approved CalEEMod computer model will be used to estimate regional mobile source and particulate matter emissions associated with the construction of the proposed project.
- Long-term (operational) increases in regional criteria air pollutants will be quantitatively assessed for area source, mobile sources, and stationary sources. The ARB-approved

CalEEMod computer model will be used to estimate emissions associated with the proposed project. Exposure to odorous or toxic air contaminants will be assessed through a screening method as recommended by the Air District.

- Local mobile-source CO concentrations will be assessed through a CO screening method as recommended by the Air District. Mobile source CO concentrations are modeled for signalized intersections expected to operate at unacceptable levels of service (i.e., LOS E or worse). If the screening method indicates that modeling is necessary, upon review of the traffic analysis, CO concentrations will be modeled using the Caltrans-approved CALINE4 computer model.

We will incorporate the results of the HRA and CalEEMod modeling identified in a previous task. This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts on air quality. Greenhouse Gases and Climate Change will be addressed in a separate chapter.

#### BIOLOGICAL RESOURCES

We plan to utilize the Wildland Resource Manager’s Wetland Report and Biological Review to prepare this section of the EIR. This task does not include any new analysis, but will include a reconnaissance level survey by a qualified biologist to verify site conditions.

#### ENERGY

De Novo will prepare an Energy analysis pursuant to the requirements of CEQA. This will include an evaluation of the energy consumption (electricity, oil, and natural gas) and a review of the project related to the Title 24, Part 6 of the California Code of Regulations, known as the Building Energy Efficiency Standards (Standards), including the CALGreen standards. In order to ensure that energy implications are considered in project decisions, Appendix F of the CEQA Guidelines requires that EIRs include a discussion of the potential energy impacts of proposed projects, with particular emphasis on avoiding or reducing inefficient, wasteful and unnecessary consumption of energy. The goal of conserving energy implies the wise and efficient use of energy.

Per Appendix G of the State CEQA Guidelines, the proposed project would result in a significant impact on energy use if it would result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation; or conflict with or obstruct a state or local plan for renewable energy or energy efficiency.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts associated with energy consumption.

#### GREENHOUSE GASES AND CLIMATE CHANGE

De Novo will prepare a Green House Gas Emissions analysis pursuant to the requirements of federal, state, regional, and local laws and regulations related to thresholds and methodology for this analysis. The analysis will follow the California Air Pollution Control Officers Association (CAPCOA) white paper methodology and recommendations presented in Climate Change & CEQA, which was prepared in coordination with the California Air Resources Board and the Governor’s Office of Planning and Research as a common platform for public agencies to ensure that GHG emissions are appropriately considered and addressed under CEQA. This analysis will consider a regional approach toward determining whether GHG emissions are significant, and will present mitigation measures to reduce impacts. The discussion and analysis will include quantification of GHGs generated by the project using ARB-approved CalEEMod computer model as well as a qualitative discussion of the project’s consistency with any applicable state and local plans to reduce the impacts of climate change. The De Novo team will work with City staff to implement a methodology and mitigation strategy that meets all legal requirements and is consistent with current City policies and preferences.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts associated with greenhouse gas emissions.

#### LAND USE AND PLANNING

This section will include a detailed discussion of the project entitlements as it relates to the existing General Plan, Zoning Code, and other local regulations. We will discuss and map the existing and planned land uses and the character of the region. The local, regional, state, and federal jurisdictions potentially affected by the project will be identified, as well as their respective plans, policies, laws, and regulations (including zoning), and potentially sensitive land uses. We will evaluate the proposed project for consistency the General Plan, the Zoning Ordinance, and any other relevant planning document. Planned development and land use trends in the region will be identified based on currently available plans. Reasonably foreseeable future development projects within the region will be noted, and the potential land use impacts associated with the project will be presented.

This section will provide an analysis including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to ensure consistency with the existing and planned land uses.

#### NOISE

The De Novo Team includes Saxelby Acoustics, an acoustical consulting firm with exceptional local knowledge and experience. The scope of work includes the preparation of a noise study described in a previous task, which will be incorporated into this section of the EIR.

This EIR section will include a full discussion of any existing noise environment, an analysis of station noise generated by the project, including proposed loading docks, on-site truck circulation, parking lots, and any proposed mechanical equipment. We will also conduct an analysis of the noise and vibration impacts associated with construction of the project at existing sensitive receptors in the project vicinity. This section will include a comprehensive mitigation plan to address any potentially significant impacts identified.

#### TRANSPORTATION AND CIRCULATION

We plan to utilize the KD Anderson traffic report and addendum to prepare this section of the EIR. The Traffic Report includes a VMT analysis to comply with the requirements of SB 743, and a LOS Analysis for compliance with General Plan policy. This task does not include any new analysis.

#### UTILITIES AND SERVICES SYSTEMS

We understand that an engineer has been retained by the applicant to perform civil engineering for this project, and that the applicant’s plan has been engineered to City specifications. We will utilize the engineering design/calculations performed by the engineer in the preparation of this section of the EIR. This section will focus on wastewater, water, and storm drainage infrastructure, as well as other utilities (i.e. solid waste, gas, electric, etc.) that are needed to serve the proposed project. It is noted that these topics are relevant to the Utilities Department and the appropriate level of coordination will be performed to confirm that the plans are acceptable and that the project description is accurate. This section will provide an analysis, including the methodology, thresholds of significance, a consistency analysis, cumulative impact analysis, and a discussion of feasible mitigation measures that should be implemented to reduce impacts associated with utilities and service systems.

#### CUMULATIVE IMPACT SUMMARY

De Novo will analyze the environmental impacts of the project when viewed in combination with other known, approved, or reasonably foreseeable projects in the region. The cumulative analysis will address each topic covered in the environmental analysis and will identify appropriate mitigation measures for any significant impacts identified. This cumulative analysis will be based on a list of known projects in the region as well as forecasts.

#### ALTERNATIVES

De Novo will coordinate with City staff to formulate up to four (4) alternatives for analysis in the EIR as required by the CEQA Guidelines. Our efforts will result in an EIR that will include an examination of a range of reasonable alternatives that could feasibly achieve the basic objectives of the project.

The CEQA Guidelines require that a “No Project” alternative be analyzed among the range of alternatives. An alternative location must also be analyzed unless it is determined by the lead

agency that a feasible alternative location does not exist. If the lead agency determines that an alternative location does not exist, it must disclose the reasons for this conclusion in the EIR.

The alternatives section will provide a description and comparison of the alternatives. Finally, an environmental superior alternative will be selected. From our experience with similar EIRs, we will provide suggested alternatives for City staff to consider. Once the alternatives are initially formulated, they will be presented at the public scoping meeting and refined based on public input. (*Note: We do not anticipate the need to analyze the alternatives at an equal level to the proposed project.*)

#### OTHER CEQA REQUIREMENTS

The section will include the other required CEQA sections including issues previously determined to be less than significant, growth-inducing impacts, significant irreversible environmental effects, and a summary of significant and unavoidable impacts.

#### REPORT PREPARERS AND REFERENCES

This section will provide a list of all persons, agencies, and references used to prepare the EIR.

*Deliverables: One (1) electronic copy of the Admin Draft EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK F – PUBLIC DRAFT EIR AND NOTICE OF COMPLETION

Comments received from City staff regarding the Administrative Draft EIR will be incorporated into the Draft EIR for public circulation. De Novo will generate a “Screen-check” Draft EIR for a final staff review before we produce the document for public review. After the document is finalized, we will publish the document and distribute it with the proper notices to the State Clearinghouse. We will provide City staff with a copy to be filed by the City with the County Clerk, and a newspaper of regional circulation.

*Deliverables: One (1) electronic copy of the Public Draft EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK G – ADMINISTRATIVE FINAL EIR

Upon completion of the public review period De Novo will coordinate with City staff and prepare a written response to the public comments, and where necessary the appropriate revisions will be made to the EIR text. Any additional text will be marked in underline format and any deleted text will be marked in ~~strikeout~~ format. All responses will be prepared pursuant to Section 15088 of the State CEQA Guidelines and provided to Stockton staff for review.

We anticipate 20 or fewer comment letters, two to three pages in length. Excessively long comment letters, or those that are complicated and require a significant effort and/or additional analysis to respond to are considered outside the scope of work and cost estimate.

*Deliverables: One (1) electronic copy of the Admin Final EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK H – FINAL EIR AND MMRP

Comments received from City staff regarding the Administrative Draft Final EIR will be incorporated into the Final EIR for public circulation. De Novo will generate a “Screen-check” Final EIR for a final staff review before we produce the document for public review. After the document is finalized, we will produce the document and deliver it to the City for distribution.

This task will also include the preparation of a Mitigation Monitoring and Reporting Program (MMRP) pursuant to Section 21081.6 of the Public Resources Code. The MMRP will consolidate information contained in the environmental analysis, including the specific mitigation measure, the party responsible for implementation, the party responsible for monitoring, the time frame for implementation, and a section for confirmation of implementation.

*Deliverables: One (1) electronic copy of the Public Final EIR with appendices, in MS Word and PDF format. Hard copies can be prepared at time and materials if requested.*

#### TASK I – FINDINGS OF FACT/ OVERRIDING CONSIDERATIONS

De Novo will prepare the required CEQA Findings of Fact and Statement of Overriding Considerations pursuant to requirements of Sections 15091 and 15093 of the State CEQA Guidelines. These findings shall be prepared using Stockton’s format and will be provided to City staff for an administrative review. Comments received from staff regarding the Administrative Findings will be incorporated into a final version of the Findings for use by the City at the public hearings.

*Deliverables: One (1) electronic copy of the findings.*

#### TASK J – ATTENDANCE AT PLANNING COMMISSION/CITY COUNCIL MEETINGS

De Novo will attend up to two (2) public hearings, which includes a Planning Commission and City Council hearing. De Novo will be responsible for preparing any exhibits that may be necessary for display at these meetings, presentations, and responses to public comment. We anticipate that the Project Manager or Senior Planner will be required for each meeting. Technical support from other members is not included, but can be accommodated on a time and material basis.

#### TASK K – NOTICE OF DETERMINATION

Upon certification of the EIR De Novo will prepare a Notice of Determination for filing with the State Clearinghouse. The applicant will be responsible for paying the CDFW filing fees, which are approximately \$3,445.25, but are anticipated to increase on January 1, 2022. The applicant will also be responsible for paying the Clerk fee, which is \$50.

*Deliverables: One (1) electronic copy of the NOD.*

TASK L-ADMINISTRATION/PROJECT MANAGEMENT

This task includes time to administer the contract, invoicing, coordination with the City staff, developer team, and consultant team.

# BUDGET AND SCHEDULE OF CHARGES

Task #	Task Description	McMurtry Project Manager \$175	Ritchie Prin. Planner \$175	Carroll Senior Planner \$135	Smith Senior Planner \$135	De Novo Support/GIS \$105	Direct Costs	Totals
<b>A Project Initiation</b>								
	Refine scope/schedule	0.75						0.8
	Conference Call w/ City re: communication, deliverables	0.50						0.5
	Research and Collect background documents			1.00		0.50	\$ 28.75	1.5
	<i>Subtotal</i>	1.25	0.00	1.00	0.00	0.50		2.8
	<b>Task A</b>	\$ 218.75	\$ -	\$ 135.00	\$ -	\$ 52.50	\$ 28.75	\$ 435.00
<b>B Prepare Project Description</b>								
	See Task E							0.0
	<i>Subtotal</i>	0.0	0.0	0.0	0.0	0.0		0.0
	<b>Task B</b>	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
<b>C NOP</b>								
	See Task E							0.0
	<i>Subtotal</i>	0.0	0.0	0.0	0.0	0.0		0.0
	<b>Task C</b>	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
<b>D Additional Technical Studies</b>								
	Health Risk Assessment, AQ Modeline, Noise Study				10.00		51.00	61.0
	<i>Subtotal</i>	0.00	0.00	0.00	10.00	0.00		10.0
	<b>Task D</b>	\$ -	\$ -	\$ -	\$ 1,350.00	\$ -	\$ 8,745.00	\$ 10,095.00
<b>E Prepare Administrative Draft EIR</b>								
	Document Preparation/Management	3.0		18.0			\$ 82.50	21.0
	<i>Subtotal</i>	3.0	0.0	18.0	0.0	0.0		21.0
	<b>Task E</b>	\$ 525.00	\$ -	\$ 2,430.00	\$ -	\$ -	\$ 82.50	\$3,037.50
<b>F Prepare Public Draft EIR/NOC</b>								
F.1	Document Revisions	12.0	8.0	8.0	8.0	2.0		38.0
F.2	Document Preparation/Management	6.0		1.0		4.0		11.0
	<i>Subtotal</i>	18.0	8.0	9.0	8.0	6.0		49.0
	<b>Task F</b>	\$ 3,150.00	\$ 1,400.00	\$ 1,215.00	\$ 1,080.00	\$ 630.00	\$ -	\$7,475
<b>G Prepare Admin Final EIR</b>								
G.1	Introduction	1.0						1.0
G.2	Overview of Comments Received	4.0						4.0
G.3	Response to Comments	12.0	12.0	16.0	16.0			56.0
G.4	Produce Admin. Final EIR	1.0		4.0		4.0		9.0
G.5	Document Preparation/Management	1.0		1.0		2.0		4.0
	<i>Subtotal</i>	19.0	12.0	21.0	16.0	6.0		74.0
	<b>Task G</b>	\$ 3,325.00	\$ 2,100.00	\$ 2,835.00	\$ 2,160.00	\$ 630.00	\$ -	\$11,050
<b>H Prepare Final EIR and MMRP</b>								
H.1	Document Revisions	6.0		12.0				18.0
H.2	Document Preparation	1.0		4.0	1.0	8.0		14.0
H.3	MMRP	1.0		10.0		1.0		12.0
	<i>Subtotal</i>	8.0	0.0	26.0	1.0	9.0		44.0
	<b>Task H</b>	\$ 1,400.00	\$ -	\$ 3,510.00	\$ 135.00	\$ 945.00	\$ -	\$5,990
<b>I Findings/Overriding Considerations</b>								
I.1	Prepare Admin. Findings/Overriding Considerations	6.0		18.0				24.0
I.2	Prepare Final Findings/Overriding Considerations			1.0				1.0
	<i>Subtotal</i>	6.0	0.0	19.0	0.0	0.0		25.0
	<b>Task I</b>	\$ 1,050.00	\$ -	\$ 2,565.00	\$ -	\$ -	\$ -	\$ 3,615.00
<b>J Attendance at Hearings for Final EIR</b>								
J.1	Planning Commission	10.0					\$ 200.00	10.0
J.2	City Council	10.0					\$ 200.00	10.0
	<i>Subtotal</i>	20.0	0.0	0.0	0.0	0.0		20.0

Task #	Task Description	McMurtry Project Manager \$175	Ritchie Prin. Planner \$175	Carroll Senior Planner \$135	Smith Senior Planner \$135	De Novo Support/GIS \$105	Direct Costs	Totals
	<i>Task J</i>	\$ 3,500.00	\$ -	\$ -	\$ -	\$ -	\$ 400.00	\$ 3,900.00
<b>K Notice of Determination</b>								
K.1	Prepare Notice of Determination	1.0						1.0
K.2	File with State Clearinghouse	1.0					\$ 75.00	1.0
	<i>Subtotal</i>	2.0	0.0	0.0	0.0	0.0		2.0
	<i>Task K</i>	\$ 350.00	\$ -	\$ -	\$ -	\$ -	\$ 75.00	\$ 425.00
<b>L Administration/Project Management</b>								
L.1	Project Management/Coordination	17.0	2.0	8.0	8.0			35.0
L.2	Administration	10.0	1.0	8.0	8.0	4.0		31.0
	<i>Subtotal</i>	27.0	3.0	16.0	16.0	4.0		66.0
	<i>Task L</i>	\$ 4,725.00	\$ 525.00	\$ 2,160.00	\$ 2,160.00	\$ 420.00	\$ -	\$ 9,990.00
<b>De Novo Project Subtotals</b>								
	Project Subtotal Hours	104.3	23.0	110.0	51.0	25.5		313.75
	Project Subtotal Cost	\$ 18,243.75	\$ 4,025.00	\$ 14,850.00	\$ 6,885.00	\$ 2,677.50	\$ 9,331.25	\$56,012.50
<b>Total Project Cost</b>								<b>\$56,012.50</b>



CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY) 04/14/2022

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement.

PRODUCER: Cummins Insurance Agency, Inc. License # OC42488. CONTACT NAME: Cummins Insurance Agency. PHONE: 916-961-6000. FAX: 916-961-3046. INSURED: De Novo Planning Group, Steve McMurtry.

COVERAGES CERTIFICATE NUMBER: REVISION NUMBER:

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES.

Table with columns: INSR LTR, TYPE OF INSURANCE, ADDL INSD, SUBR WVD, POLICY NUMBER, POLICY EFF, POLICY EXP, LIMITS. Rows include Commercial General Liability, Automobile Liability, Umbrella Liab, Workers Compensation, and Professional Liab.

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

Re: EIR for Grocery Outlet Project
Additional Insured: City of Fort Bragg, its elected and appointed boards, officers, officials, agents, employees, and volunteers as per the attached.

CERTIFICATE HOLDER: CITYF-2, City of Fort Bragg, 416 N. Franklin Street. CANCELLATION: SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS. AUTHORIZED REPRESENTATIVE: Denise B...



## Primary and Non-Contributory Insurance

---

This endorsement, effective 4/8/2022 attaches to and forms a part of Policy Number FEI-ECC-14846-08. This endorsement changes the Policy. Please read it carefully.

---

### SCHEDULE

1. Name and Address of Person or Organization (Additional Insured):

**City of Fort Bragg, its elected and appointed boards, officers, officials,  
agents, employees and volunteers  
416 N. Franklin St.  
Fort Bragg, CA 95437**

2. Additional Premium: \$Applied

This insurance is primary with respect to the coverage afforded to the Additional Insured shown in the Schedule above by the following endorsement: ECC-535-0712

Any other insurance which the Additional Insured may have is excess and non-contributory.



## Waiver Of Subrogation Endorsement

---

This endorsement, effective 4/8/2022 attaches to and forms a part of Policy Number FEI-ECC-14846-08. This endorsement changes the Policy. Please read it carefully.

---

In consideration of an additional premium of \$Applied, this endorsement modifies insurance provided under the following:

### COMMERCIAL GENERAL LIABILITY COVERAGE PART CONTRACTORS POLLUTION LIABILITY COVERAGE PART

#### SCHEDULE

Name and Address of Person or Organization:

**City of Fort Bragg, its elected and appointed boards, officers, officials,  
agents, employees and volunteers  
416 N. Franklin St.  
Fort Bragg, CA 95437**

The Company waives any right of recovery it may have against the person or organization shown in the above Schedule because of payments the Company makes for injury or damage arising out of the *insured's* work done under a contract with that person or organization. The waiver applies only to the person or organization in the above Schedule.

Under no circumstances shall this endorsement act to extend the policy period, change the scope of coverage or increase the Aggregate Limits of Insurance shown in the Declarations.



## Additional Insured – Owners, Lessees or Contractors

---

This endorsement, effective 4/8/2022 attaches to and forms a part of Policy Number FEI-ECC-14846-08. This endorsement changes the Policy. Please read it carefully.

---

In consideration of an additional premium of \$Applied, this endorsement modifies insurance provided under the following:

### COMMERCIAL GENERAL LIABILITY COVERAGE PART CONTRACTORS POLLUTION LIABILITY COVERAGE PART

#### SCHEDULE

Name and Address of Person or Organization:

**City of Fort Bragg, its elected and appointed boards, officers, officials,  
agents, employees and volunteers  
416 N. Franklin St.  
Fort Bragg, CA 95437**

The person or organization shown in this Schedule is included as an insured, but only with respect to that person's or organization's vicarious liability arising out of your ongoing operations performed for that insured.



## Amendment of Cancellation Notice Endorsement

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This endorsement, effective 4/8/2022 attaches to and forms a part of Policy Number FEI-ECC-14846-08. This endorsement changes the Policy. Please read it carefully.

---

In consideration of an additional premium of \$Applied, this endorsement modifies insurance provided under the following:

Notwithstanding the appropriate provision of this policy, in the event cancellation of this policy is instigated by the Company for any reason except nonpayment of premium, the Company will endeavor to provide 30 days advance notice of such cancellation to the following:

**City of Fort Bragg, its elected and appointed boards,  
officers, officials, agents, employees and volunteers  
416 N. Franklin St.  
Fort Bragg, CA 95437**

**THIS ENDORSEMENT CHANGES THE POLICY. PLEASE READ IT CAREFULLY.**

## **DESIGNATED INSURED FOR COVERED AUTOS LIABILITY COVERAGE**

This endorsement modifies insurance provided under the following:

AUTO DEALERS COVERAGE FORM  
BUSINESS AUTO COVERAGE FORM  
MOTOR CARRIER COVERAGE FORM

With respect to coverage provided by this endorsement, the provisions of the Coverage Form apply unless modified by this endorsement.

This endorsement identifies person(s) or organization(s) who are "insureds" for Covered Autos Liability Coverage under the Who Is An Insured provision of the Coverage Form. This endorsement does not alter coverage provided in the Coverage Form.

### **SCHEDULE**

<b>Name Of Person(s) Or Organization(s):</b>
City of Fort Bragg
Information required to complete this Schedule, if not shown above, will be shown in the Declarations.

Each person or organization shown in the Schedule is an "insured" for Covered Autos Liability Coverage, but only to the extent that person or organization qualifies as an "insured" under the Who Is An Insured provision contained in Paragraph **A.1.** of Section **II** – Covered Autos Liability Coverage in the Business Auto and Motor Carrier Coverage Forms and Paragraph **D.2.** of Section **I** – Covered Autos Coverages of the Auto Dealers Coverage Form.

**THIS ENDORSEMENT CHANGES THE POLICY. PLEASE READ IT CAREFULLY.**

## **ADDITIONAL INSURED – PRIMARY AND NON-CONTRIBUTORY ENDORSEMENT**

This endorsement modifies insurance provided under the following:

AUTO DEALERS COVERAGE FORM  
BUSINESS AUTO COVERAGE FORM

With respect to coverage provided by this endorsement, the provisions of the Coverage Form apply unless modified by the endorsement.

### **SCHEDULE**

Name of Person(s) or Organization(s):

City of Fort Bragg

(If no entry appears above, information required to complete this endorsement will be shown in the Declarations as applicable to the endorsement.)

A. **Who Is An Insured** for **COVERED AUTOS LIABILITY COVERAGE** is amended to include as an "insured" for Covered Autos Liability Coverage:

Each person or organization shown in the Schedule, but only to the extent that person or organization qualifies as an "insured". The "accident" must arise out of ongoing operations performed for the Named Insured.

B. **Changes in CONDITIONS**

The following is added to the **Other Insurance** Condition and supersedes any provision to the contrary:

### **Primary And Noncontributory Insurance**

This insurance is primary to and will not seek contribution from any other insurance available to the person or organization named in the Schedule under your policy provided that:

- (1) The person or organization is a Named insured under such other insurance; and
- (2) You have agreed in writing in a contract or agreement that this insurance would be primary and would not seek contribution from any other insurance available to them.

**THIS ENDORSEMENT CHANGES THE POLICY. PLEASE READ IT CAREFULLY.**

## **WAIVER OF TRANSFER OF RIGHTS OF RECOVERY AGAINST OTHERS TO US (WAIVER OF SUBROGATION)**

This endorsement modifies insurance provided under the following:

AUTO DEALERS COVERAGE FORM  
BUSINESS AUTO COVERAGE FORM  
MOTOR CARRIER COVERAGE FORM

With respect to coverage provided by this endorsement, the provisions of the Coverage Form apply unless modified by the endorsement.

### **SCHEDULE**

<b>Name(s) Of Person(s) Or Organization(s):</b>  City of Fort Bragg
Information required to complete this Schedule, if not shown above, will be shown in the Declarations.

The **Transfer Of Rights Of Recovery Against Others To Us** condition does not apply to the person(s) or organization(s) shown in the Schedule, but only to the extent that subrogation is waived prior to the "accident" or the "loss" under a contract with that person or organization.



**THIS ENDORSEMENT CHANGES THE POLICY. PLEASE READ IT CAREFULLY.**

**WAIVER OF OUR RIGHT TO RECOVER FROM  
OTHERS ENDORSEMENT - CALIFORNIA**

**Policy Number:** 57 WEC ZO3688

**Endorsement Number:**

**Effective Date:** 04/29/21

Effective hour is the same as stated on the Information Page of the policy.

**Named Insured and Address:** DE NOVO PLANNING GROUP, INC.

1020 SUNCAST LN STE 106  
EL DORADO HILLS CA 95762

We have the right to recover our payments from anyone liable for an injury covered by this policy. We will not enforce our right against the person or organization named in the Schedule. (This agreement applies only to the extent that you perform work under a written contract that requires you to obtain this agreement from us.)

You must maintain payroll records accurately segregating the remuneration of your employees while engaged in the work described in the Schedule.

The additional premium for this endorsement shall be 2 % of the California workers' compensation premium otherwise due on such remuneration.

**SCHEDULE**

**Person or Organization**

**Job Description**

Any person or organization for whom you are required by written contract or agreement to obtain this waiver of rights from us

Countersigned by \_\_\_\_\_  
Authorized Representative

**From:** [Jacob Patterson](#)  
**To:** [Lemos, June](#)  
**Cc:** [Spaur, David](#); [sarah mccormick](#); [Smith, John](#); [O'Neal, Chantell](#)  
**Subject:** Public Comment -- 4/25/22 CC Mtg., Item No. 8B, GrocOut EIR Contract  
**Date:** Thursday, April 21, 2022 10:25:20 AM

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City Council,

See below for my thoughts on 8B. This total contract price seems a little low for an EIR and that may be due, in part, to an ill-advised reliance on existing studies that were a major part of what was allegedly wrong with the prior MND. I recommend some reconsideration or this contract will likely be back for cost-overruns and change orders. That said, I generally agree with the pre-identified study areas and the list of impact areas that are likely not going to involve more detailed analysis (hydrology might need additional review though).

One of the omissions from the prior MND was analysis of the safety impacts of pedestrians accessing the site and crossing the nearby streets, which might be included within the transportation section. IMO, that needs to be evaluated and was not but I don't see it identified in the intended project tasks. The special condition concerning the 4-way stop and the sidewalks across the street from the project is logically connected to this project impact and it should be incorporated specifically as a mitigation measure not just a special condition. (There is case law to back up this required area of study so you may want to check with Jones & Mayer for any relevant advice.)

Overall this proposal firm seems like a reasonable choice but a lot will depend on improved mitigation measures compared to the first round, the additional alternatives section, and much better analysis throughout--literally rewrite the whole thing rather than trying to revise the defective MND and convert it to an EIR in hopes that will pass muster.

Regards,

--Jacob

----- Forwarded message -----

**From:** **Jacob Patterson** <[jacob.patterson.esq@gmail.com](mailto:jacob.patterson.esq@gmail.com)>  
**Date:** Wed, Apr 20, 2022 at 2:45 PM  
**Subject:** Draft De Novo Contract  
**To:** Spaur, David <[dspaur@fortbragg.com](mailto:dspaur@fortbragg.com)>, McCormick, Sarah <[SMcCormick@fortbragg.com](mailto:SMcCormick@fortbragg.com)>  
**Cc:** Smith, John <[jsmith@fortbragg.com](mailto:jsmith@fortbragg.com)>, O'Neal, Chantell <[coneal@fortbragg.com](mailto:coneal@fortbragg.com)>

Dave,

The following excerpts from their proposal are huge red flags to me for the EIR contract for the S. Franklin Street Grocery Outlet Project. IMO, this is setting the City up for a likely challenge because this is one of the areas that received the most attention in public and responsible agency comments because of the obvious defects in the existing study

A) "BIOLOGICAL RESOURCES We plan to utilize the Wildland Resource Manager's Wetland Report and Biological Review to prepare this section of the EIR. This task does not

include any new analysis, but will include a reconnaissance level survey by a qualified biologist to verify site conditions."

This should be addressed prior to moving forward or you are likely structuring this agreement and the intended scope of work to set the City up for failure. The wetland and wildlife reports were demonstrably defective. In fact, there was substantial evidence in the record (IMO) that demonstrated an issue with wetlands in particular. A new study should be done on a day like today or soon thereafter to adequately evaluate the conditions. There is documented evidence that was presented during the prior review but which is inconsistent with the cited report the City intends to reuse that there are wetland conditions on the site due to soil structure in a study location that was omitted from the existing study as well as wetland-indicating plants around that area.

B) "TRANSPORTATION AND CIRCULATION We plan to utilize the KD Anderson traffic report and addendum to prepare this section of the EIR. The Traffic Report includes a VMT analysis to comply with the requirements of SB 743, and a LOS Analysis for compliance with General Plan policy. This task does not include any new analysis."

The existing report and addendum do not include adequate VMT analysis, instead they just state that no detailed VMT analysis is necessary. It should likely be updated, although this is a relatively minor task that could probably be done by a planner using basic math skills rather than a formal traffic engineer.

C) "(Note: We do not anticipate the need to analyze the alternatives at an equal level to the proposed project.)"

This is somewhat troubling to me as I anticipate that the environmentally superior alternative will not be the proposed project and will, in fact, be a reconfigured site plan and building layout. If that is the case, the environmentally superior alternative

D) Although not an actual quote from the proposal, there are numerous references with working closely with City staff to develop various components of the EIR (e.g., the development of mitigation measure language). We do not have any qualified City staff who have demonstrated an ability to perform these tasks. For example and IMO, current planning staff tend to write special conditions and mitigation measures that are not written to actually be effective nor have prior project reviews included the necessary evaluation of alternative mitigation measures to ensure that the City selects the most effective mitigation measures. The consultant should not plan on receiving much useful assistance or input from current City staff and may need to adjust their project budget accordingly.

Regards,

--Jacob

**From:** [SCR AM](#)  
**To:** [Lemos, June](#)  
**Subject:** Comment for 8B on April 25th City Council Meeting  
**Date:** Thursday, April 21, 2022 11:08:15 AM  
**Attachments:** [HELIX.pdf](#)  
[M-Group.pdf](#)

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Please add the attached proposals as public comments for 8B on the agenda for April 25, 2022, which were omitted from the agenda materials. Bids from other consultants should be considered as context in any decision like this one, particularly when there is such a large budget disparity between the proposals.

S.C.R.A.M. offers the following comments concerning the award to the EIR consultant contract for the Grocery Outlet and cautions the City Council to consider the ramifications of the City going with the "low-ball" proposal for this environmental review, which was also an issue with the selection of the environmental consultant for the prior Grocery Outlet development proposed for Todd's Point. The City may award this contract only to find that the budget is wholly inadequate. An EIR requires detailed work and the recommended consultant's bid is alarmingly low compared to most EIR budgets but both of the other proposals are in line with reasonable budgets for an EIR for a project of this scope. These comparisons show up in the referenced projects by each proposing firm listed in their respective proposals.

An even more significant consideration is the likely quality of the work for the environmental review, which has already been challenged once in court and led to this subsequent review using an EIR. A consultant who dramatically underbids on a project indicates that they might not understand the complexity of the issues, which is likely to impact the quality of the finished product. In addition, unlike Negative Declarations or MNDs, an EIR is required to not only include timely written comments on the DEIR but is also required to include written responses to each point raised in a submitted written comment. That additional requirement in the review process is likely to be much more involved than in being anticipated in the proposal from De Novo, which will surely impact the project expenses relative to budget.

Proposal to Prepare an

**Environmental Impact Report Pursuant to the California Environmental Quality Act for the Proposed Grocery Outlet**

March 29, 2022



Prepared for:

**City of Fort Bragg**

416 North Franklin Street  
Fort Bragg, CA 95437

Prepared by:

**HELIX Environmental Planning, Inc.**

11 Natoma Street, Suite 155  
Folsom, CA 95630

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### Appendices

- Appendix A - Key Personnel Resumes
- Appendix B - Budget and Rate Sheet

March 25, 2022

Heather Gurewitz, AICP, Associate Planner  
Community Development Department  
City of Fort Bragg  
416 North Franklin Street  
Fort Bragg, CA 95437

**Subject: Proposal for an Environmental Impact Report Pursuant to the California Environmental Quality Act for the Proposed Grocery Outlet**

Dear Ms. Gurewitz:

The City of Fort Bragg (City) deserves to be supported by the most knowledgeable and proficient environmental consulting firm to process the necessary environmental compliance documentation for the proposed Grocery Outlet project, as outlined in the Request for Proposal (RFP) dated March 15, 2022. Selecting HELIX Environmental Planning, Inc. (HELIX) to prepare the California Environmental Quality Act (CEQA) documentation will result in the certification of a comprehensive CEQA document that fully vets all required environmental topics, will withstand the anticipated intense public scrutiny and possible legal challenge, and presents the City's decision-makers with clear and concise analyses.

Since its inception, HELIX has developed an unparalleled reputation of providing high quality CEQA services to local government and public agency clients throughout California. This reputation includes extensive experience in preparing CEQA documents for controversial projects that not only withstand public scrutiny but have been awarded accolades from professional planning associations. We understand that the City is under intense pressure to prepare and circulate an Environmental Impact Report (EIR) based upon sound science with conclusions supported by substantial evidence. With our project team of tried and tested planning professionals and scientists, we will prepare project-specific air quality /greenhouse gas (GHG)/energy analyses, noise and vibration technical studies, geotechnical investigation from our local teaming partner (SHN Engineers & Geologists) and a cultural resources evaluation.

The following are key aspects of our proposal that provide direct benefit to the City:

- We have reviewed the Initial Study/Mitigated Negative Declaration (ISMND) previously prepared for the proposed project and believe that the project applicant and City were correct in vacating the adopted document as it was likely not legally defensible. Many conclusions drawn in the ISMND were not based upon substantial evidence, a key component of a legally defensible CEQA document. HELIX will rely upon technical studies prepared in support of the ISMND to the maximum extent practicable (i.e., biological review, wetland report, and traffic impact analysis) along with pending technical documents being prepared by the project applicant once vetted/approved by the City. As outlined in this proposal, HELIX will conduct additional technical study to ensure that CEQA conclusions are based upon science and fact, not unsubstantiated conjecture.
- We will use information and analysis previously prepared (in support of the ISMND) to the maximum extent practicable and supplement this information with stand-alone technical studies as warranted and outlined in this proposal. Our preliminary review of the ISMND analyses indicates that additional stand-alone technical analysis is needed for Air Quality/Greenhouse Gas Emissions (including a Health Risk Assessment), Noise, and Cultural/Tribal Cultural Resources. HELIX has the in-house experts on staff and identified for this project to successfully complete these required studies. Additional or pending technical studies to be provided by the

project applicant (and reviewed/approved by the City) will also be incorporated by reference and integrated into the EIR.

- HELIX has the expertise, depth of staff, and commitment necessary to provide excellent service to the City for the proposed project. The EIR will likely face similar legal challenge from local opponents and must be prepared strictly in accordance with the CEQA Guidelines for content, noticing, circulation, and approval consideration. HELIX will employ our knowledge and experience to ensure that the City's document reflects this commitment to correct process and procedure.
- The HELIX Team will be led by the Project Manager and CEQA Lead, Robert Edgerton, who has 25 years of experience in environmental documentation and regulatory compliance, including extensive experience working with controversial development projects throughout rural Counties in California. He will manage the contract and be the primary point of contact for the City. As Principal-in-Charge, Joanne Dramko's experience includes coastal development projects as well air quality/noise analyses. She will provide technical oversight and Quality Assurance (QA)/Quality Control (QC) to ensure document quality and readability. HELIX senior resource experts are included to prepare the necessary technical information to be integrated into the CEQA document, Lesley Owing for senior environmental planning support, Clarus Backes for cultural resources investigations, and John DeMartino for geographic information systems (GIS). We are equally pleased to be teamed with SHN, a local, Mendocino Coast consultant, to provide needed geotechnical investigation services. Biographical sketches and full resumes of key staff are included in this proposal.
- We are pleased to recently have been awarded a contract with the City for the proposed Waste Hauling Transfer Station ISMND. Most of our internal HELIX Team members identified for the Grocery Outlet EIR are also supporting this project and are intimately knowledgeable about the baseline conditions within the City for their individual discipline areas. Both projects would be managed by the same project manager, thereby ensuring continuity and consistency across both contracts and CEQA documents.

Please contact our Project Manager, Robert Edgerton, at [RobertE@helixepi.com](mailto:RobertE@helixepi.com) or his direct phone number at 916.365.8713 during the evaluation period with any questions about our proposal or qualifications. Thank you for considering HELIX and we look forward to hearing from you.

Sincerely,



Joanne M. Dramko, AICP  
Environmental Planning Discipline Leader

Distribution: Digital proposal on flash drive and (2) two hardcopies

## Firm Description

HELIX is an employee-owned, California Corporation and a leader in environmental planning, design, and natural resource sustainability. Established in 1991 and with offices in Sacramento and Placer Counties, as well as San Diego, Orange, Los Angeles, and Riverside Counties, we provide a broad range of environmental compliance services throughout California. With 30 years of extensive experience providing environmental planning services, HELIX has developed a proven approach to environmental compliance that produces legally defensible documents in an efficient, cost-effective manner. We devote the utmost attention to quality which has given our clients the confidence to bring their most sensitive and controversial projects to us, time and time again.

In-house services provided by HELIX include CEQA and National Environmental Policy Act (NEPA) compliance; biological and aquatic resource studies; acoustical/noise studies; air quality (AQ)/GHG/energy analyses; cultural resources investigations; historic resource evaluations; visual resource assessments; arboriculture; land use and planning; public involvement; agricultural resources; community impact studies; regulatory permitting; mitigation monitoring and compliance; landscape architecture; and GIS.

HELIX has completed environmental documents for virtually every project type, including commercial, industrial, residential, mixed-use, and transit-oriented development projects. We have worked on general, community, and specific plans as well as documents for educational, medical, institutional, and correctional facilities. In addition, we have supported a variety of public infrastructure projects from pipelines and water treatment plants, to fire stations, parks, roads, bridges, and highways. We proactively assist agencies in determining the appropriate environmental documentation and review process for proposed projects. Creative problem-solving, a high level of commitment to clients, and high-quality service and products are the hallmark of our success.

We have assembled a team with the depth and breadth of experience in place to lead preparation of this EIR through all of the environmental requirements and offer innovative and fresh approaches along the way. Furthermore, HELIX's Folsom and Roseville office location staff includes approximately 50 professionals, with an additional 140 employees located in the firm's other California offices, should additional staff resources be needed.

## Relevant Experience

HELIX has extensive experience with environmental compliance documentation supporting rural, coastal, and/or underserved communities. The following are summaries of recent projects highlight relevant experience, as well as the HELIX team members who were active participants in these projects. In addition to the necessary experience with CEQA documentation for development projects, our staff are also familiar with the resources in north coastal California, having recently completed resources assessment and CEQA documentation for over 50 cannabis industry projects in Humboldt, Mendocino, and Trinity Counties.

### Vacant Lands Inventory EIR | Inyo County | 2021

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HELIX is preparing an EIR and technical studies for a vacant lands inventory and zoning review of properties located throughout rural Inyo County. This information will be used to identify land that might be appropriate for zone changes to promote housing opportunities primarily by increasing the allowable residential density. The review will consider increasing the amount of multi-family zoning in the County, lowering some of the minimum lot size requirements, and adding zoning areas with principal permitting for mobile home parks. The review of the County's current zoning will also focus on commercial zones for opportunities for residential infill development. Areas near public transportation and other services will be considered prime, but due to the County's rural nature, other properties located in remote communities without these services might also be identified for zone changes. A primary component of this work includes public outreach meetings and communication with property owners. HELIX prepared biological resources and cultural resources technical studies. The project is funded through a grant provided by the California Department of Housing and Community Development. **HELIX Team Members: Edgerton, Project Manager; Owning and Gustafson, Environmental Planners; Backes, Archaeologist; and DeMartino, GIS.**

### North Torrey Pines Living and Learning Neighborhood EIR | San Diego County | 2018

HELIX prepared the EIR in the coastal zone for the housing and academic project that would redevelop a 13-acre surface parking lot on the west side of the UC San Diego campus, located southeast of the intersection of Muir College Drive and North Torrey Pines Road. The project proposed the construction of six buildings positioned around central community open space areas. Three of the buildings would be primarily residential, and three would contain a mix of educational, community, and residential uses. The project would provide approximately 2,000 beds for undergraduate students.



Residential support spaces and amenities would be provided throughout the student housing areas, including meeting, study, music, and multi-purpose rooms; gathering space; dining space; bicycle storage; an underground parking garage; and other support amenities. The project incorporates utilities improvements, and the buildings would be designed to meet U.S. Green Building Council Leadership in Energy and Environmental Design Platinum standards. The project EIR was tiered from the UC San Diego 2004 Long Range Development Plan EIR. This project was awarded the 2021 Construction Management Project Achievement Award - Buildings \$25M+. **HELIX Team Members: Dramko, Project Manager and Principal-in-Charge (PIC).**

### Aramis Solar Energy Generation and Storage Project EIR | 2020

HELIX prepared an EIR for the construction and operation of a solar energy generation and storage project on 410 acres in unincorporated North Livermore, Alameda County. In support of the EIR, HELIX prepared biological resources, cultural and historic resources evaluation, AQ/GHG, and noise technical reports and managed a thorough Administrative Record in anticipation of a legal challenge.

The preparation of the biological resources technical report included conducting general biological surveys, an aquatics delineation report, protocol surveys for the federally listed as threatened California red-legged frog, and rare plant surveys for the utility-scale solar project. The preparation of the cultural resources technical report included conducting a pedestrian survey, records search, and historic evaluation of nearby ranching structures that were determined to be eligible for listing with the National Register of Historic Places (NRHP), California Register of Historical Resources (CRHR), and the local County register.

In support of County staff, HELIX staff supported the preparation of numerous County reports including a Statement of Overriding Considerations, Written Findings of Significant Effects, Use Permit findings, and the East County Board of Zoning Adjustments staff report. HELIX's Project Manager presented the project and key environmental issue areas to the public and Board members at numerous public scoping meetings and hearings. The project was approved and the EIR was certified by the East County Board of Zoning Adjustments in November 2020.

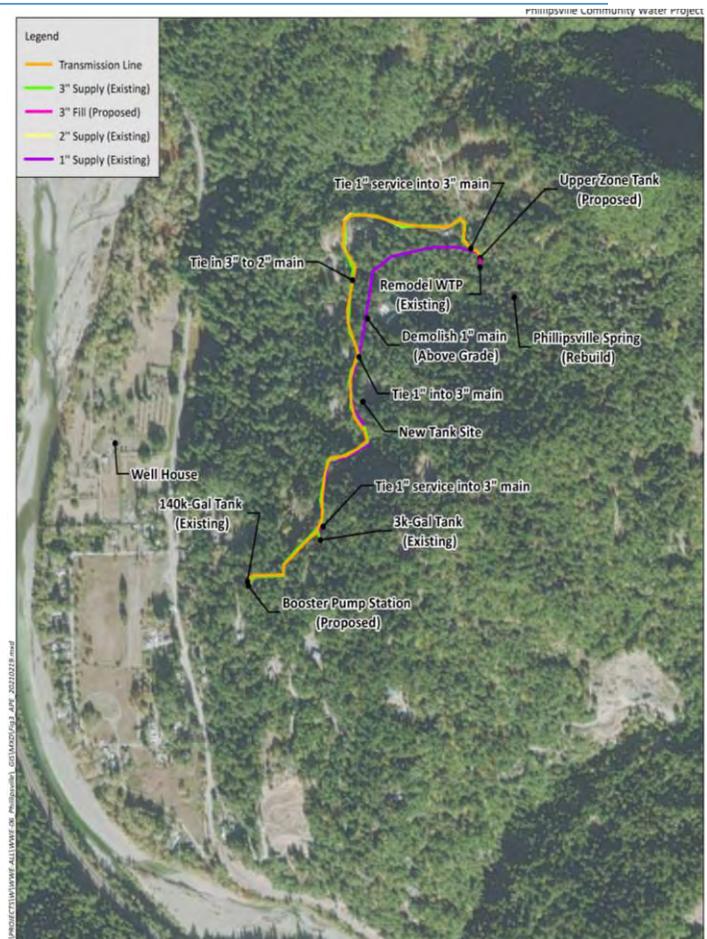
Project approval was appealed by the local Save North Livermore, Friends of Livermore, and Friends of Open Space and Vineyards groups primarily regarding concerns about the project's consistency with the voter-approved Measure D of the East County Area Plan and potential impacts to special-status species. HELIX provided further support to the County and project applicant through the appeal process by drafting appeal response letters and supporting exhibits for the Board of Supervisors staff report. The Board's action on the project was "de novo," and the project was approved and the EIR was certified by the County Board of Supervisors in March 2021. Work was performed for Alameda County on behalf of Intersect Power, LLC. **HELIX Team Members: Owning, Project Manager; Gustafson, Environmental Planner; Backes, Archaeologist; Dramko, Air Quality and Noise; and DeMartino, GIS.**

### City of Bishop Downtown Specific Plan & Mixed-Use Zoning Code Update EIR | 2021 - 2022

HELIX is supporting the City of Bishop with preparation of an EIR for the proposed Bishop Downtown Specific Plan. If adopted, the proposed project would increase residential and commercial density in the downtown core of this rural and underserved City located in the Eastern Sierra Nevada region. Alta Planning + Design and the City will prepare the Specific Plan, including visual simulations, and leading the public outreach campaign with support from HELIX. HELIX has prepared several technical studies in support of the program-level EIR, including air quality/GHG/energy analysis, noise and vibration studies, biological resource evaluation, cultural resource assessment, and visual resource analysis. HELIX is teamed with Tom Kear Transportation Planning and Management, Inc. to prepare a qualitative vehicle miles traveled (VMT) analysis based in part on substantial input from local citizens and residents. The Notice of Preparation for this project was circulated in spring 2021 and the public review draft document will be circulated for public comment in summer 2022. **HELIX Team Members: Edgerton, Project Manager; Owning and Gustafson, Environmental Planners; Backes, Archaeologist; and DeMartino, GIS.**

### Phillipsville Community Services District Water System Improvements ISMND | 2020 - 2021

The Phillipsville Community Services District (PCSD) serves approximately 300 residents through 66 service connections in rural Humboldt County. A treatment system for the supply spring and a 140,000-gallon storage tank was installed approximately 8 years ago; the installed treatment system has deficiencies due to improper design and installation. The PCSD is currently under a boil water notice for inadequate filtration and not meeting sufficient chlorine contact time requirements. In addition, the spring source is in jeopardy of potential land movement and, at times during the summer months, is inadequate to supply its customers. This project is needed to assess the current condition of the spring source and evaluate potential improvements to address system deficiencies and redundancy, storage, chlorine contact time, and adequate supply. This project required Water Works Engineers, as the consulting engineer, to evaluate and develop solutions for the problems associated with the spring source. HELIX is supporting the project through the preparation of stand-alone technical studies (biological and cultural resource evaluations) and development of CEQA-Plus documentation to meet state/federal environmental compliance needs per the State Revolving Loan Fund program as administer by the State Water Resources Control Board (SWRCB). The CEQA-Plus document will be an ISMND with SWRCB as the CEQA Lead Agency. **HELIX Team Members: Edgerton, Project Manager; Gustafson, Environmental Planners; Backes, Archaeologist; and DeMartino, GIS.**

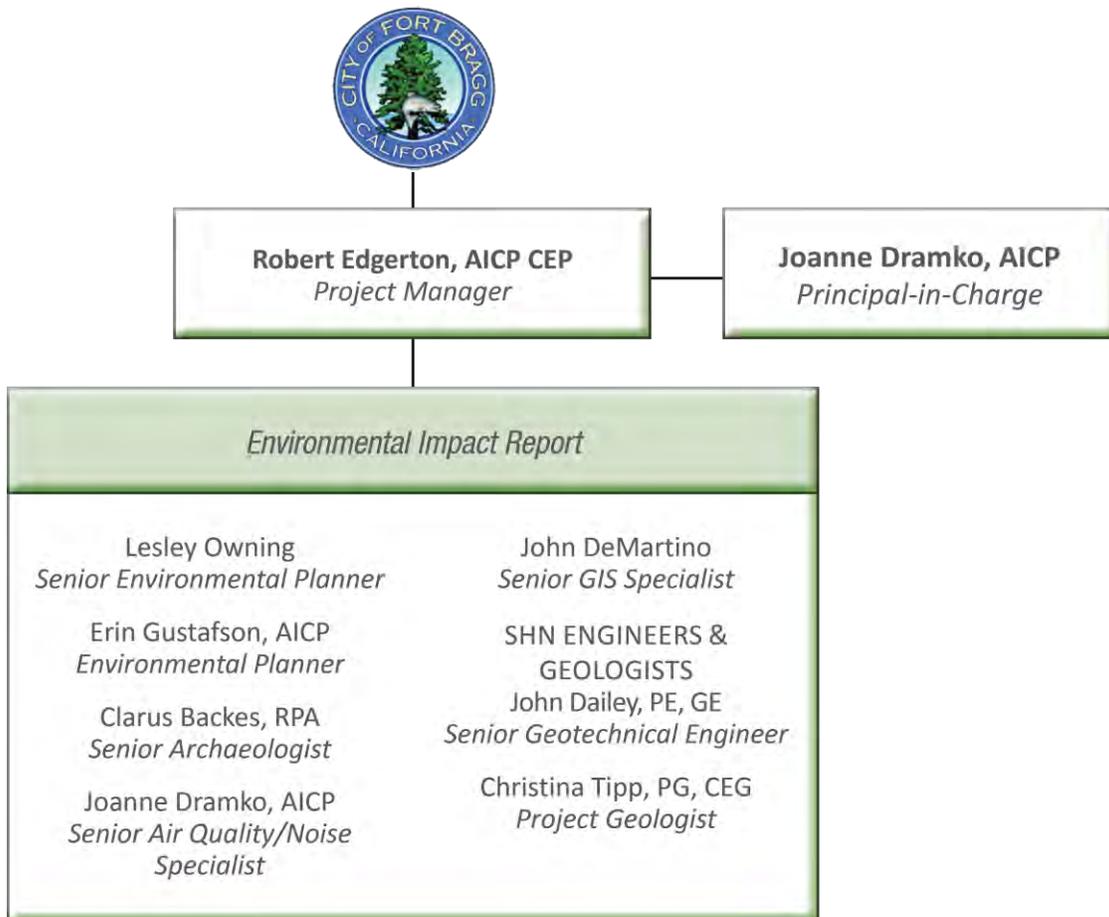


## Key Personnel

HELIX has assembled a highly qualified team to prepare the required CEQA documentation. As identified on the organizational chart below, our in-house team includes resource experts for archaeology, AQ, GHG, and noise with subconsultants for specialized technical studies. Robert Edgerton, AICP CEP will serve as our Project Manager. He is a proven Project Manager with 25+ years of CEQA experience and will serve as the City’s primary point of contact. He will be responsible for the management of the agreed-upon scope of work, deliverable schedule, and project budget and actively participate in project meetings with the City’s Project Manager and project team. He will direct the day-to-day activities of the project team in coordination with key staff and hold regular team meetings to coordinate tasks requiring multiple staffing resources. Mr. Edgerton will be assisted by Ms. Dramko, AICP, for implementation of our QA/QC program for all project deliverables in advance of submittal to the City.

## Organizational Chart

The following organizational chart identifies key personnel who will work on this project and their roles.



## Key Personnel Qualifications

Following are introductions to key personnel which includes an overview of experience in their respective specialty. Full resumes with additional relevant project experience, education, and certifications are provided in Appendix A.

### Robert Edgerton, AICP CEP | Project Manager | HELIX

Mr. Edgerton draws upon his experience as both a project manager and a senior environmental planner to aid private companies, governmental agencies, and non-governmental organizations in the planning, entitlement, and permitting of infrastructure improvement projects. Calling upon 25 years of experience in the environmental and land use planning industry, his work focuses on CEQA and NEPA compliance, and he has successfully processed more than 100 environmental compliance documents such as EIRs, Environmental Impact Statements (EIS), ISMND, and Environmental Assessments (EA). He has also prepared, consulted on, and processed federal, state, and local permits in support of projects with the US Army Corps of Engineers, US Fish and Wildlife Service, Bureau of Land Management, State and Regional Water Quality Control Boards, and California Department of Fish and Wildlife. Mr. Edgerton is an accredited member of the American Institute of Certified Planners (AICP) with an advance Certificate in Environmental Planning (CEP) accreditation.

#### Key Projects Managed

- Bishop Downtown Specific Plan & Mixed-Use Zoning Code Update EIR
- Inyo County Vacant Lands Inventory EIR
- Inyo County Renewable Energy General Plan Amendment and Program EIR
- City of Folsom, On-Call Environmental Services (70+ projects)
- City of Fort Bragg, Waste Hauling Transfer Station ISMND (pending)



### Joanne Dramko | Principal & Senior Air Quality/Noise Specialist | HELIX

Ms. Dramko has over 20 years of experience preparing CEQA and NEPA environmental documents, including Programmatic EIRs (PEIR), ISMNDs, and EAs for a variety of project types, including residential, commercial, recreational, transportation, water/wastewater, and utilities. Her focus is climate change and sustainability within the context of CEQA. She has conducted noise and air quality analyses using survey equipment such as the American National Standards Type II noise level meter, computer models such as the California Emission Estimator Model (CalEEMod), and the Federal Highway Administration Traffic Noise Model. Ms. Dramko is also skilled at communicating technical information to general audiences in public forums, and engaging members of the public in the environmental planning process. She is also an AICP Certified Planner and Greenhouse Gas Lead Verifier (#H-18-041).

#### Key Projects Managed

- North Torrey Pines Living and Learning Neighborhood EIR
- La Jolla Innovation Center EIR
- Marisol Coastal Resort Specific Plan Initiative
- Mesa Housing Nuevo West and East EIR

#### Role

Ms. Dramko will prepare air quality, GHG, energy, and noise impact analyses in support of CEQA documentation, as needed. She will also serve as Principal-in-Charge to provide signatory authority and provide overall quality control of the document.



### Lesley Owning | Senior Environmental Planner | HELIX

Ms. Owning is a Senior Environmental Planner with ten years of experience in the provision of all types of CEQA and NEPA documentation for a variety of project types including, residential, commercial, and other land development; infrastructure improvement; renewable energy; transportation; cannabis cultivation, operation, and processing facilities; and other planning and public works projects. Ms. Owning also develops CEQA review and strategy plans for public and private clients early in the project planning phase and prepares project site constraints analyses from a CEQA and permitting perspective. Ms. Owning has managed controversial land development projects and provided public agency support through project appeal hearings. She has led responses to comments for projects that have generated hundreds of public comments and maintained thorough administrative records for projects that are anticipated to face legal challenges. Ms. Owning also oversees the implementation of mitigation monitoring and reporting programs (MMRPs) throughout project construction.

#### Key Projects

- Oakmont Senior Living EIR
- Aramis Solar Energy Generation and Storage Project EIR
- Inyo County Vacant Lands Inventory EIR

#### Role

Ms. Owning will be primary author for many technical sections of the EIR and perform duties as deputy project manager (as needed).



### Erin Gustafson | Environmental Planner | HELIX

Ms. Gustafson has nine years of environmental planning experience and assists clients in successful completion of the environmental review process. She is skilled in preparing ISMNDs and EIRs under the CEQA, and EAs and EISs under NEPA. She has also assisted clients with managing responses to public comments on complex and high-profile projects. Ms. Gustafson has coordinated multidisciplinary teams, worked closely with staff from public agencies, and integrated input from a variety of stakeholders on residential and commercial land use development; water; transportation; and renewable energy projects.

#### Key Projects

- Bishop Downtown Specific Plan & Mixed-Use Zoning Code Update EIR
- Inyo County Vacant Lands Inventory EIR
- Aramis Solar Energy Generation and Storage Project EIR

#### Role

Ms. Gustafson will be a technical author for many sections of the EIR, including project alternatives.



### Clarus Backes, RPA | Senior Archaeologist | HELIX

Mr. Backes is an archaeologist and cultural resources manager with over 20 years of professional experience throughout California and the western Great Basin. He has conducted and supervised numerous projects in support of compliance with Sections 106 and 110 of the National Historic Preservation Act, CEQA, and NEPA. He is also well versed in criteria for CRHR and NRHP evaluations. He has participated in a wide range of projects involving archaeological survey, testing, data recovery, monitoring, laboratory analysis, and the development of mitigation and treatment plans, and has over 15 years of experience in a decision-making capacity on cultural resources projects in California. He is a Registered Professional Archaeologist (#1673640), holds a Bureau of Land Management Statewide Cultural Resource Use Permit (#CA-18-35), and meets the U.S. Secretary of the Interior's Professional Qualifications Standards for prehistoric and historic archaeology.



#### Key Projects

- Aramis Solar Energy Generation and Storage Project EIR
- Inyo County Vacant Lands Inventory EIR
- Bishop Downtown Specific Plan & Mixed-Use Zoning Code Update EIR

#### Role

Mr. Backes will serve as the lead archaeologist for the cultural resources evaluations and provide senior level oversight of all cultural resources-related reports, maps, and evaluation. He will also support the City in the facilitation of tribal consultations Assembly Bill 52 (AB 52) and conduct searches of the Sacred Lands File as owned/maintained by the Native American Heritage Commission (NAHC), as-needed.

### John DeMartino | Senior GIS Specialist | HELIX

Mr. DeMartino is a GIS professional with an extensive background in applying GIS applications and workflows in support of biological, cultural, conservation, public works, water, and engineering projects, with an emphasis CEQA/NEPA documentation, and regulatory permitting. He is proficient in the latest GIS software and technologies, including ArcGIS Desktop, ArcGIS Server, ArcGIS Online, ArcGIS Collector, Trimble TerraSync and Trimble Pathfinder Office global positioning system (GPS) software, ERDAS Imagine and ERDAS StereoAnalyst, SketchUp, and several ArcGIS extensions. Mr. DeMartino has senior-level expertise both performing and supervising key GIS practices, including GIS data development, GPS data collection, CAD data integration, impact and overlay analysis, spatial modeling, mapping, and QA/QC of final deliverables.



#### Key Projects

- Aramis Solar Energy Generation and Storage Project EIR
- Inyo County Vacant Lands Inventory EIR
- Bishop Downtown Specific Plan & Mixed-Use Zoning Code Update EIR
- Oakmont Senior Living EIR

#### Role

Mr. DeMartino will lead GIS mapping and prepare report graphics.

### John Daily, PE, GE | Senior Geotechnical Engineer | SHN Engineering

Mr. Dailey has more than 40 years of experience in geotechnical, civil, and environmental engineering while working with federal, state, and local regulatory agencies as well as the private sector. His experience includes project management, subsurface geotechnical and environmental investigations, site remediation, plan and procedure development, specification and bid preparation, permitting, and subcontractor selection. Field experience includes excavation and drilling for geotechnical and environmental investigations, including soil and groundwater sampling, and field and laboratory soil testing.

#### Key Projects

- Santa Clara County Courthouse
- Fisherman’s Terminal Building, Eureka
- PG&E Unit 21 Geothermal Powerplant, Geysers

#### Role

Mr. Daily will lead preparation of the geotechnical investigation of the project site.



### Christina Tipp, PG, CEG | Certified Engineering Geologist | SHN Engineering

Ms. Tipp has more than 14 years of professional experience in engineering geology and geotechnical engineering. She specializes in evaluating geologic risk, geotechnical and geologic drilling and sampling, geologic inspections, and geotechnical investigations. Ms. Tipp is skilled in a multitude of subsurface exploration techniques, geologic mapping, review of LIDAR and aerial photography, gathering geologic research for a project site, and communicating project progress and relaying results to the project team. She has “hands-on” experience in geologic and geotechnical field investigations, addressing development in geologic hazard zones, and providing practical insights throughout the life of the project.

#### Key Projects

- Geotechnical Investigation and Geologic Hazard Report for Proposed New Classroom and Library, Redwood Elementary, Fort Bragg
- Geotechnical Assessment of Subsurface Soil Void, Sherwood Oaks Health Center, Fort Bragg
- Orr Creek Common Housing Development, Ukiah

#### Role

Ms. Tipp will lead preparation of the Geologic Hazards Evaluation.



## References

We welcome you to contact our references to attest to the quality of our work and to confirm delivery of projects in a timely and cost-efficient manner. Refer to the Relevant Experience section above for description of work performed and team members involved.

- Scott Johnson, AICP, Planning Manager, City of Folsom, Community Development Department, 916.461.6206.
- Elaine Kabala, Associate Planner, City of Bishop, Public Works Department, 760.873.8458.
- Albert Lopez, Planning Director, Alameda County, Planning Department, 510.670.5426.

## Project Understanding, Approach, and Scope of Work

### *Project Understanding and Approach*

The project applicant (BRR Architecture) is proposing to construct a Grocery Outlet (retail store) on a 1.63-acre site located at 825, 845, and 851 S. Franklin Street as identified by Assessor's Parcel Numbers 018-120-47, -48, and -49. The project would require the merger of three existing parcels to create one 71,002 square foot (1.63 acres) parcel. The project site is located in the Coastal Zone with a land use designation of Highway Visitor Commercial (CH) (2008) and a zoning designation of Highway Visitor Commercial (CH) per the City of Fort Bragg Zoning Map (2016). No changes to the site's land use designation or zoning designations are proposed.

The project includes the demolition of an existing 16,436 square-foot vacant office building and associated 47-space parking lot, and the construction and operation of a 16,157 square-foot, one-story, retail store with a 53-space parking lot and associated improvements and infrastructure. The project would be operated by 15 to 25 full-time staff and two managers and would be open from 9:00 AM to 10:00 PM, 7 days per week. Associated improvements and infrastructure on-site would include a loading dock and trash enclosure, a parking area with 53-parking spaces, an internal system of walkways and crosswalks, two bicycle racks, two driveways, a new fire connection, replacement of an existing sewer connection, connection to underground utilities, landscaping for stormwater capture and treatment, illuminated signage, and landscaping.

The proposed project would include 51,650 square feet (1.18 acres) of hardscape areas for the proposed store, parking lot, accessways or sidewalks, and driveways. Approximately 19,265 square feet (0.44 acres) of the site would be landscaped and permeable to stormwater as the project would be designed to capture stormwater and pre-treat it on-site to remove dirt, oil, and heavy metals using bioretention basins.

Anticipated City permits and approvals include, but may not be limited to, a coastal development permit, parcel merger, encroachment permit, zoning clearance, design review, and grading/building permits.

### *Scope of Work*

#### **Task 1: Project Management**

The HELIX Project Manager, Robert Edgerton, will be responsible for and oversee all aspects of HELIX's work, including the provisions of QA/QC of work products along with Ms. Dramko. He will supervise the sole sub-consultant (SHN) and will assure work products from the HELIX Team are accomplished within budget and per the project schedule. He will attend coordination meetings virtually or via telephone during preparation of the EIR.

**Deliverables:** N/A

#### **Task 2: Kick-off Meeting**

The HELIX Project Manager will meet with City staff virtually or via telephone to review the scope of work and project schedule. Careful consideration will be placed on pending technical studies to be prepared by the project applicant (along with associated review times by the City).

**Deliverables:** N/A

#### **Task 3: Notice of Preparation/Scoping Meeting**

HELIX will prepare a Notice of Preparation (NOP) that will include a brief summary of the proposed project, including regional and vicinity maps of the project location, and a listing of the environmental issues anticipated to be addressed in the EIR. The NOP will also include notice of a public scoping meeting (as described below). No Initial Study is envisioned as part of the NOP, as we suggest that the City proceed directly with an EIR. Should an Initial Study be requested, HELIX could prepare one with additional authorization by the City.

The entire NOP is anticipated to be up to five pages in length. HELIX will provide City staff with an electronic copy (in Microsoft Word and pdf format) of the NOP for review. HELIX will revise and finalize the NOP based on minor comments from the City. The City shall distribute the NOP to the State Clearinghouse electronically and to interested stakeholders and surrounding property owners as necessary for a 30-day public review period. The City shall be responsible for coordinating, scheduling, and managing the NOP scoping meeting; HELIX will attend the scoping meeting in support of the City.

The City shall arrange for publication of the NOP/scoping meeting notice in one weekday issue of the Fort Bragg Advocate-News and post the meeting notice on its website. At the scoping meeting, the City shall present an overview of the project; HELIX will prepare a brief Microsoft PowerPoint presentation to introduce the environmental process to meeting participants. The City shall provide the facility and virtual platform as needed (i.e., Zoom connection). It is assumed that all public comments will be submitted in writing and an official transcript of the scoping meeting recorded by a court reporter will not be required. Within two weeks of close of the 30-day scoping period, HELIX will prepare a memorandum summarizing comments and issues raised during the scoping period. Advertisement costs for public scoping meeting announcement shall be borne by the City.

**Deliverables:** Notice of Preparation; NOP Presentation Materials for Public Scoping Meeting; NOP Public Scoping Meeting Summary Report (notes).

#### Task 4: Evaluate Existing Technical Studies

HELIX will review documents previously prepared in support of the proposed project and advise the City as to their adequacy and usefulness. Our preliminary assessment suggests that the following technical studies are valid for use in the EIR: Biological Review (Wildland Resources, August 2019); Wetland Report (Wildland Resources, March 2021); and Traffic Impact Analysis (KD Anderson, June 2021). These studies may require minor revision due to their shelf life and dependence on dated database searches. Technical studies that we have preliminarily determined to be inadequate for reuse in the EIR include air quality/GHG analysis (LACO Associates, 2020), noise assessment (LACO Associates, 2020), and Cultural Resources Inventory Survey (Genesis Society, August 2019). We further understand that pending technical studies may be provided by the project applicant (following review and approval by the City). These studies, including a Phase 1 Environmental Site Assessment, will be incorporated by reference and/or integrated directly into the EIR as necessary. Modification to existing technical studies, as deemed necessary by the City, shall be at the responsibility of the project applicant.

**Deliverables:** Existing Technical Studies assessment memo (letter format).

#### Task 5: Technical Evaluation of Issues

##### *5.1: Geotechnical Report*

HELIX will rely upon the professional services of our local teaming partner, SHN, to conduct the necessary geotechnical investigation of the project site. The purpose of SHN's geotechnical investigation will be to characterize the surface and subsurface conditions at the project site to develop geotechnical engineering criteria for design and construction of the proposed project. SHN's work will include a geologic evaluation of the potential hazards in the vicinity of the proposed building, including:

- Review of published geologic and geologic hazard maps in the immediate vicinity of the project site.
- Perform site reconnaissance by certified engineering geologists to observe existing site conditions, mark the exploration areas for Underground Service Alert (USA), and subcontract with a private utility locator to check the drilling locations for underground utilities.
- Submit a Drilling Permit application to the Mendocino County Department of Environmental Health and pay associated County fees.
- SHN will drill four borings near the proposed building and parking lot. Three borings will be advanced to depths ranging from approximately 15 to 20 feet below the ground surface. One boring will be advanced to a

depth of 50 feet below the ground surface, or 10 feet into bedrock (whichever occurs first), to address the liquefaction potential beneath the site. Samples from each boring will be collected at suitable intervals, using standard penetration test (SPT) and modified California split spoon samplers. A drilling subcontractor would be retained to complete the borings. Soil cuttings will be placed in drums and removed from the project site.

- Samples collected will be returned to SHN's soils testing laboratory for geotechnical analysis. Anticipated tests include dry density and moisture content, percent passing the #200 sieve, shear strength, Atterburg limits, R-value, and corrosivity testing. Specific tests may be added or eliminated depending on the materials encountered at the project site.
- Assessment of potential earthquake-related geologic/geotechnical hazards (for example, strong earthquake ground shaking, surface fault rupture, liquefaction, and differential settlement), and other potential geologic/geotechnical hazards, as needed.
- Provide seismic design parameters in accordance with the applicable portions of the 2019 California Building Code and the American Society of Civil Engineers 7-16 Standard, including site soil classification, seismic design category, and spectral response accelerations.
- Perform engineering analyses in order to provide a report presenting conclusions and recommendations regarding: a.) earthwork, including project site and subgrade preparation, fill material specifications, and fill compaction requirements; b.) discussion of appropriate foundation options, including allowable bearing capacities, estimates of settlement (total and differential), minimum footing depth, and allowable lateral capacities; c.) support of concrete slabs-on-grade; and, d.) recommendations for observation of site preparation and grading, observation of foundation installation, and other geotechnical construction considerations.

**Deliverables:** Geotechnical Investigation and Geologic Hazards Evaluation letter report.

### *5.2: Cultural Resources Assessment Report*

HELIX will conduct a records search at the Northwest Information Center (NWIC) located at Sonoma State University. The records search will include reviews of U.S. Geological Survey topographic maps where archaeological sites are mapped; Department of Parks and Recreation (DPR) archaeological site records; data from previous surveys and research reports; historic maps; the Historic Property Data File; the NRHP; CRHR; and listings of California Historical Landmarks and Points of Historical Interest. The records search will capture all documented cultural resources within 0.25-mile of the project area. This proposal assumes that NWIC fees will not exceed \$500.

HELIX will request that the NAHC search their Sacred Lands File for Native American sites or resources that may be within or near the project area. Using the Native American representatives list provided by the NAHC, letters will be sent to each tribal representative requesting additional information or concerns they may have about the proposed project. These letter requests are for informational purposes only and are not part of the AB 52 consultation process. Note: HELIX assumes the City shall be responsible for management and operation of the AB 52 tribal consultation process. Information derived from AB 52 consultation will be included in the EIR as provided by the City.

HELIX archaeologists will conduct an intensive pedestrian survey of the project area to characterize any extant archaeological sites, artifacts, or structures more than 45 years old. The survey will consist of a pedestrian walk-over of all areas where ground disturbance is proposed using 15-meter parallel transects.

HELIX assumes that no previously recorded archaeological or architectural resources will be located within the project area and that no undocumented resources will be encountered during the survey. However, if cultural resources are identified within the project area, additional funds may be required for field documentation and reporting; if these resources cannot be avoided during construction, additional funds may be required to evaluate the eligibility of these resources to the CRHR. If documentation is necessary the resources will be recorded on the

appropriate DPR forms, and these forms will be presented as an appendix to the Cultural Resources Assessment Letter Report.

A Cultural Resources Assessment letter report will be prepared upon completion of the survey. The report will include the results of the records search and Native American outreach, cultural survey findings, and maps depicting all areas surveyed. The report will also include recommendations for further study, avoidance, or mitigation of any cultural resources that may be impacted by the proposed project. Once finalized, a copy of the report and any new or updated site records will be filed with the NWIC.

**Deliverables:** Cultural Resources Assessment letter report.

### *5.3: Air Quality/Greenhouse Gas Emissions/Energy/Health Risk Assessment*

HELIX will prepare an air quality, GHG emissions, and energy technical report in accordance with the requirements of CEQA and guidelines from the Mendocino County Air Quality Management District (MCAQMD). HELIX will rely on a listing of quantified information needs from the project applicant relative to project phasing, construction methods and timing, export and import of soils and materials, anticipated energy and water use, project design features that will reduce energy use and GHG emissions, and other data relative to air quality and GHG emissions. Data relative to trip generation and trip length will be incorporated from the project traffic study. HELIX will estimate the emissions of criteria pollutants and GHGs using CalEEMod. HELIX will analyze the proposed project's air quality impacts, addressing the issues described in the CEQA Guidelines Appendix G and in accordance with significance criteria adopted by the MCAQMD. It is expected that the proposed project would not cause severe congestion at a major intersection resulting in a local carbon monoxide (CO) "hotspot;" therefore, no dispersion modeling is included in this scope of work for CO analysis. Odor impacts will be addressed qualitatively. Additionally, the analysis will include a determination of project conformity with the MCAQMD Particulate Matter (PM) Attainment Plan and applicable portions of the State Implementation Plan for ozone and PM attainment. If potential significant impacts are identified, HELIX will recommend appropriate mitigation measures.

HELIX will prepare a health risk assessment (HRA) to analyze impacts to nearby sensitive receptors in accordance with applicable portions of the Office of Environmental Health Hazard Assessment Air Toxics Hot Spots Program Risk Assessment Guidelines (February 2015). Short-term construction and long-term operational emissions of toxic air contaminants, including diesel particulate matter emissions from off-road and on-road diesel equipment, as well as emissions from stationary sources, will be estimated using CalEEMod supplemented with emission factors calculated from the California Air Resources Board's (CARB) emissions inventory database, as needed. Dispersion modeling will be conducted that will include both emission and meteorological inputs using the US Environmental Protection Agency's AERMOD. Risks will be estimated by post-processing the AERMOD results using CARB's Hot Spots Analysis and Reporting Program tools. The results, in terms of incremental cancer and non-carcinogenic health risks, will be compared with MCAQMD's adopted thresholds.

HELIX will analyze potential GHG emission impacts by describing the methodology used to estimate GHG emissions, assessing potential impacts, and identifying mitigation measures, as appropriate and necessary. Significance of GHG emissions will be assessed based on MCAQMD recommended thresholds, with consideration of statewide post 2020 GHG reduction mandates. Significance will also be assessed by considering whether implementation of the project would conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of GHGs, including the City's 2012 Climate Action Plan. If potential significant impacts are identified, HELIX will recommend appropriate mitigation measures.

HELIX will analyze the project's anticipated energy use and qualitatively discuss impacts related to wasteful, inefficient, or unnecessary use of energy during project construction and operation; and whether the project would conflict with applicable energy efficiency and/or energy use reduction plans and programs.

HELIX will provide the results of the air quality, HRA, GHG emissions, and energy analyses in a technical report. The report will include descriptions of existing air quality and applicable regulations and policies, as well as the results of the analyses described above, including a determination of the level of significance of impacts in accordance with CEQA Guidelines. This scope of work includes one round of review and editorial revisions to the technical report and assumes only minor revisions not to include modeling. Requests for additional modeling of alternatives or remodeling due to changes in the project assumptions (e.g., project description, traffic study, project construction schedule) can be accommodated with an augment.

**Deliverables:** Air Quality/Greenhouse Gas Emissions/Energy/Health Risk Assessment technical report.

#### *5.4: Noise Study*

HELIX will prepare a noise study in support of the CEQA analysis in the EIR. The analysis will address potential noise impacts related to project site construction, project operational on-site activities, and off-site changes in traffic noise. Analysis and preparation of the noise study will include:

- Provide a current brief overview of noise and related federal, state, and local regulations, including the Noise Element of the City of Fort Bragg Coastal General Plan, and the City municipal code;
- Conduct a field inspection to measure the current ambient traffic noise on the project site and identify other existing noise sources in the project vicinity. The site visit will include at least one 24-hour measurement and multiple short-term (10 to 15 minute) measurements, as required to document the existing noise environment;
- Briefly evaluate the site construction noise and groundborne vibration impacts;
- Using appropriate modeling and/or calculations, estimate operational noise levels from stationary sources on the site, including heating, ventilation, and air conditioning (HVAC) equipment, grocery store refrigeration systems, truck delivery areas, idling trucks with truck refrigeration units, and other machinery associated with operation of a grocery store. The City's General Plan Noise Element and municipal code will be used to assess impacts associated with on-site store operational noise;
- Estimate changes in off-site ambient traffic noise levels using the Federal Highway Administration's Traffic Noise Model;
- Develop mitigation measures to attenuate noise, if necessary, to reduce impacts at nearby noise-sensitive land uses. Mitigation measures would identify potential noise attenuation techniques and specific performance standards.

HELIX will provide the results of the noise analysis in a technical report. The report will include descriptions of existing noise environment and applicable regulations and policies, as well as the results of the analyses described above, including a determination of the level of significance of impacts in accordance with CEQA Guidelines. This scope of work includes one round of review and editorial revisions to the technical report and assumes only minor revisions not to include modeling. Requests for additional modeling of alternatives or remodeling due to changes in the project assumptions or public comments (e.g., project description, traffic study, project construction schedule) can be accommodated with an augment.

**Deliverables:** Noise Study technical report.

#### **Task 6: Prepare Administrative Draft EIR**

The Administrative Draft Environmental Impact Report (ADEIR) will contain the requisite components of an EIR, including an Executive Summary, Introduction, and Project Description as outlined in the CEQA Guidelines Section 15168. The Project Description will discuss the goals and objectives and describe the major features of the project. In order to assure that the analysis accurately reflects all aspects of the proposed project, the HELIX team will work closely with the City in the early stages of the ADEIR.

### Project Description

The EIR Project Description will be based on the information provided by the project applicant and City as well as input gathered during the public scoping period (Task 3). These data will be used to assist in development of the project's goals and objectives, as well as feed into development of the project alternatives. The Project Description will include a background section describing the history of the proposed project's entitlement application(s) as well as City required permits and approvals.

### Project Alternatives

After the proposed project has been defined and in consultation with City staff, HELIX will develop preliminary and feasible project alternatives. Alternatives would be based on City input, as well as stakeholder input during the public scoping period (Task 3) and will be developed based on the need to avoid or reduce the potentially significant impacts identified for the proposed project. We envision a total of three alternatives, including the proposed project, the no project alternative, and an environmentally superior project alternative (assuming that the no project alternative would be identified as being an environmentally superior alternative to the proposed project). Alternatives would be evaluated and presented in the EIR for public review and consideration by decision makers.

### Cumulative Impact Analysis

An EIR must discuss cumulative impacts if the incremental effect of a project, combined with the effects of other projects is "cumulatively considerable." Such incremental effects are to be viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects. Together, these projects comprise the cumulative scenario which forms the basis of the cumulative impact analysis. Both the severity of impacts and the likelihood of their occurrence are to be reflected in the discussion, but the discussion need not provide as great a level of detail as is provided for the effects attributable to the project alone.

For this project, we recommend working with the City to develop a list of past, present, and probable residential and/or commercial development projects while also looking at the development trends identified in City planning documents. Large projects that have recently undergone environmental review would provide a useful starting point to identify individual projects in Mendocino County that would contribute to cumulative effects. Other planning documents, as made identified and made available to HELIX by the City, would also be considered in the cumulative effects analysis in so far as they may help define future land use patterns in the City. Each area of concern (e.g., water resources, land use, etc.) will be addressed in its own section and will contain the five major components: Introduction; Affected Environment (including Existing Conditions and Regulatory Framework); Thresholds and Methodology; Impact Analysis/Mitigation Measures; and Cumulative Impacts/Mitigation Measures. This scope anticipates the integration of all individual areas of concern directly into the EIR (rather than preparation of stand-alone technical reports).

The following individual environmental topic areas would be assessed qualitatively and/or quantitatively and discussed in the ADEIR per Appendix G of the CEQA Guidelines:

- **Aesthetics/Visual Resources:** This section of the EIR will evaluate whether implementation of the project could result in significant alterations to viewsheds, visual character, and lighting and glare conditions of the City, especially as it pertains to the coastal environment. This will consist of reviewing current City policies and code provisions. Visual resource simulations, as provided by the project applicant at their discretion, would be considered and incorporated into the EIR. The extent of potential aesthetic impacts will be qualitatively described. We assume a potentially significant and unavoidable impact for this resource topic requiring preparation of a Findings of Fact/Statement of Overriding Considerations document (Task 11).
- **Agricultural Resources:** This section of the EIR will evaluate whether implementation of the project could result in adverse impacts or loss of agricultural resources (temporary and permanent) in the City. This will consist of reviewing current City policies and code provisions that address these resources, as well as

applicable state and federal provisions. Mitigation measures will be identified should a significant impact be identified.

- **Air Quality/Greenhouse Gas Emissions/Energy/HRA:** See Task 5.3 for a detailed scope of work.
- **Biological Resources:** HELIX will rely on previously prepared documentation by Wildland Resources for biological resource evaluation, including potential presence of wetland habitat. Documents will be incorporated by reference and directly integrated into the EIR.
- **Cultural Resources/Tribal Cultural Resources:** See Task 5.2 for a detailed scope of work.
- **Geology and Soils/Mineral Resources:** See Task 5.1 for a detailed scope of work.
- **Hazards/Hazardous Materials:** We understand that the project applicant will be preparing and submitting a Phase 1 Environmental Site Assessment addressing the potential presence of hazardous materials on the project site. The Phase 1 Environmental Site Assessment, once reviewed and approved by the City, will be incorporated by reference and directly integrated into the EIR.
- **Hydrology and Water Quality:** We understand that water resources are of critical importance to the City and local citizens as evidenced by public comments on the ISMND. HELIX will rely upon water resource information provided by the City to qualitatively address potential impact of the proposed project on local water resources. The EIR will reflect the estimated amount of water use for the proposed project based upon input provided by the project applicant's civil engineer. We will also consult with the City's Public Works Department and service providers regarding the condition of water resources, as well as current water quality and efforts to improve/protect water quality. This will include identification of land areas where groundwater resources are limited and additional demand may result in overdraft concerns. We will review and identify applicable federal, state, and City policies and regulations (e.g., implementation of National Pollutant Discharge Elimination System permit requirements to protect water quality). Mitigation measures will be identified as warranted.
- **Land Use and Planning:** This section of the EIR will address if implementation of the proposed project would result in conflicts with the City's General Plan and associated plans that could result in physical impacts to the environment. An analysis of the current General Plan land use and zoning designations will be presented in the EIR in tabular fashion. HELIX will determine the potential impacts to land use and planning qualitatively in the EIR. We will review and identify current City policies and code provisions that address compatibility, as well as applicable state and federal provisions. Mitigation measures will be identified as needed but are not envisioned.
- **Noise:** See Task 5.4 for a detailed scope of work.
- **Population and Housing:** The EIR will include a discussion of the potential for the proposed project to have a negative impact on population and housing in the City. No mitigation is envisioned.
- **Public Services/Recreation/Utilities:** The EIR will also address potential public service and utility demands (i.e., fire protection, law enforcement, water supply, etc.). We will coordinate with applicable service providers and the City to seek "will serve" letters for the proposed development. Potential conflicts with existing and planned recreation uses and activities will also be identified. Mitigation measures will be identified as required.
- **Transportation:** HELIX will rely on the Traffic Impact Analysis (TIA) prepared by KD Anderson (June, 2021) for the proposed project. The TIA identifies the average daily trips based upon proposed land use trip generation rates derived from the ITE manual, 10th Edition and also includes a qualitative evaluation of VMT.
- **Wildfire:** The EIR will consider and qualitatively evaluate the potential impact associated with wildfire from the potential placement of a commercial structure in the City limits.

HELIX has allocated approximately 372 hours for preparation of the ADEIR, including HELIX time to conduct informal consultation with State agencies as requested by the City. The City will review the ADEIR and submit one set of unified comments to HELIX. We will then prepare the public review Draft EIR (DEIR) suitable for public circulation (outlined below).

**Deliverables:** ADEIR submitted electronically in MS Word format.

### Task 7: Prepare Pre-Print DEIR

HELIX will prepare a pre-print Draft EIR (DEIR) for review by the City incorporating requested edits to the ADEIR in track changes mode to facilitate review of the pre-print DEIR. In addition, draft Notices of Completion (NOC) and Availability (NOA) will be produced and submitted to the City for review/comment. The pre-print DEIR shall also consist of a draft MMRP. HELIX has allocated approximately 150 hours of staff time for preparation of the pre-print DEIR and MMRP.

**Deliverables:** Pre-print DEIR, MMRP, NOC, and NOA submitted electronically in MS Word format.

### Task 8: Prepare DEIR

HELIX will prepare the DEIR for circulation and distribution by the City. The DEIR will incorporate minor revisions to the pre-print DEIR (not in track changes mode). In addition, the final NOC and NOA will be produced and submitted to the City. The NOC will be filed by the City with the State Clearinghouse and Mendocino County Clerk. The City shall be responsible for publishing the NOA in the Fort Bragg Advocate-News and posting it to the City's website. The DEIR will be circulated by the City via the State Clearinghouse using their electronic delivery system (direct upload via their website rather than paper hardcopies). The DEIR shall also consist of the final MMRP. HELIX has allocated approximately 24 hours of staff time for preparation of the DEIR and MMRP.

**Deliverables:** DEIR, MMRP, NOC, and NOA submitted electronically in MS Word format.

### Task 9: Prepare Draft of Responses to Comments and AFEIR

HELIX has opted to combine preparation of draft responses to comments and an administrative final EIR (AFEIR) into a single task. We understand that significant opposition to the proposed project may be voiced during the public comment period and we have nominally allocated approximately 60 hours of staff time to respond to comments and prepare the AFEIR. The AFEIR will be based upon the draft responses to comments and indicated in track changes mode. Additional hours needed beyond the amount allocated may be provided following additional authorization by the City. We envision working with City staff and perhaps the project applicant's legal advisors closely during Task 9.

**Deliverables:** Draft responses to comments (tabular format) and AFEIR submitted electronically in MS Word format.

### Task 10: Prepare Final EIR

Following a single round of consolidated comments from the City on the draft responses to comments and AFEIR, HELIX will revise the CEQA document and prepare the Final EIR (FEIR). The FEIR will include a list of all persons and organizations that commented on the DEIR, City responses to the comments, and revisions to the DEIR in track changes mode. If the City desires another format for the FEIR we are open to the request. The FEIR will be provided to the City in electronic format with all appendices included. HELIX has allocated approximately 40 hours in support of Task 10.

**Deliverables:** FEIR submitted electronically in MS Word and Adobe Acrobat formats.

### Task 11: Prepare Findings and Statement of Overriding Considerations

Assuming that one or more environmental topic areas result in a significant and unavoidable impact finding (as we believe it will for Aesthetics/Visual Resources), then a Findings (per Section 15091) and a Statement of Overriding Considerations (per Section 15093) shall be required. The Findings shall describe each significant impact anticipated to occur as a result of the proposed project and decide as to whether mitigation measures are available to reduce each significant impact to below a level of significance. In the event one or more significant impacts cannot be reduced to below a level of significance, a Statement of Overriding Considerations —identifying the social, economic, or other factors taken into consideration in the decision to approve the project despite unmitigated significant environmental impacts—would be necessary. HELIX will prepare a draft Findings/Statement

of Overriding Considerations for City review and comment. We will also prepare a draft resolution based upon a City provided template.

Following receipt of a single round of consolidated comments from the City on the draft Findings and Statement of Overriding Considerations, HELIX will prepare a final Findings/ Statement of Overriding Considerations package for electronic submittal to the City. HELIX has allocated approximately 60 hours for Task 11; hours needed beyond this estimate would require additional authorization by the City.

**Deliverables:** Draft/Final Findings and Statement of Overriding Considerations submitted electronically in MS Word format.

### Task 12: Planning Commission Public Hearing

HELIX will support presentation of the FEIR to the City Planning Commission either virtually (i.e., Zoom meeting) or in person, depending on current public health conditions at the time of the hearing. Although a presentation by HELIX is not envisioned for this task, our Project Manager will be available to answer questions about the methodology, approach, results, and/or conclusions of the FEIR. We have allocated approximately 24 hours for preparation, travel time, and meeting attendance at the single Planning Commission hearing. Additional meeting support may be provided with additional authorization by the City.

**Deliverables:** N/A

### Budget and Schedule of Charges

We have prepared a budget that details the hours and personnel by task, as well as other direct costs, on a time and materials basis with a not to exceed total. Due to the oversized page, please see Appendix B for this table. We also included a schedule of charges showing hourly rates should augments for additional work be required.

### Work Schedule

The City has identified a target processing time of 6-9 months for completion of the EIR (including consideration of adoption by the Planning Commission). As outlined in our proposed work schedule (below), HELIX has developed a feasible schedule that provides for completion of the EIR within approximately 8 months. Potential Planning Commission hearing dates are flexible and may be rescheduled as desired. The following schedule is based upon a Notice to Proceed date of April 1, 2022 and can be adjusted depending upon the actual date of NTP as issued by the City.

Task	Duration	Deliverable Date
HELIX receives Notice to Proceed	--	April 1, 2022
Task 1: Project Management	Ongoing	--
Task 2: Kick-off Meeting	1 day	April 1, 2022
Task 3: Notice of Preparation and Scoping Meeting		
Preparation of Draft NOP	2 weeks	April 15, 2022
City review of Draft NOP	1 week	April 21, 2022
Preparation of Final NOP	3 days	April 24, 2022
--NOP Review Period--	30 days	April 25, 2022 – May 24, 2022
Scoping Meeting	1 day	May 11, 2022
Task 4: Evaluate Existing Technical Studies	1 week	April 15, 2022
Task 5: Technical Evaluation of Issues		

Geotechnical Report	10 weeks	June 13, 2022
Cultural Resources Assessment Report	8 weeks	June 1, 2022
Air Quality/Greenhouse Gas/Energy/Health Risk Assessment	10 weeks	June 13, 2022
Noise Study	8 weeks	June 1, 2022
Task 6: Prepare Administrative Draft EIR		
Preparation and submittal of ADEIR	2 weeks	June 27, 2022
City review of ADEIR	2 weeks	July 11, 2022
Task 7: Prepare Pre-Print DEIR		
Preparation of and submittal of Pre-Print DEIR (including draft MMRP, NOC, and NOA)	2 weeks	July 25, 2022
City review of Pre-Print DEIR	1 week	August 1, 2022
Task 8: Prepare DEIR		
Preparation and submittal of DEIR (including final MMRP, NOC, and NOA)	2 weeks	August 8, 2022
City circulation of DEIR via State Clearinghouse	1 day	August 9, 2022
--DEIR Review Period--	45 days	August 9, 2022 – September 22, 2022
Task 9: Prepare Draft Responses to Comments and Administrative Final EIR		
Preparation of draft Responses to Comments (tabular)	2 weeks	October 6, 2022
Preparation of Administrative FEIR	2 weeks	October 6, 2022
City review of draft Responses to Comments and AFEIR	2 weeks	October 20, 2022
Task 10: Prepare Final EIR	2 weeks	November 3, 2022
Task 11: Prepare Findings and Statement of Overriding Considerations		
Prepare and submit draft Findings and SOC	2 weeks	November 17, 2022
City review of draft Findings and SOC	2 weeks	December 1, 2022
Prepare and submit final Findings and SOC	2 weeks	December 15, 2022
Task 12: Planning Commission Public Hearing *	1 day	December 21, 2022

\*Planning Commission meets every second and fourth Wednesday of the month; this date may be moved as required.

## Sample Work Product

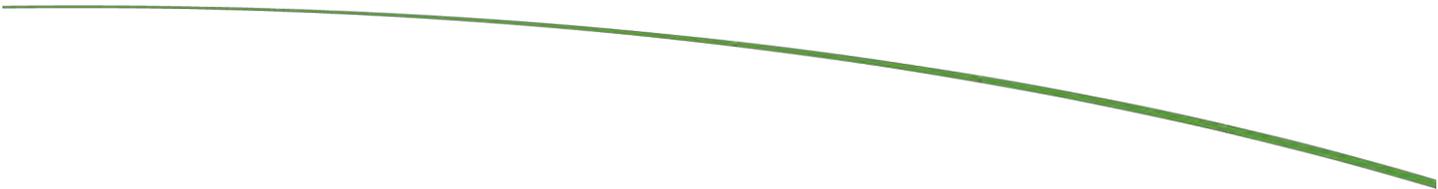
The enclosed flash drive includes a copy of this proposal as well as a copy of our sample work product. Our sample document is an EIR and associated technical documents for the County of Inyo Renewable Energy General Plan Amendment. This EIR received a *Planning Award of Merit for Innovation in Green Community Planning* from the American Planning Association (California Chapter) in 2015. It was prepared under the leadership and direction by our Project Manager, Robert Edgerton. A brief description of the project is included above in the Relevant Experience section.

## Insurance

HELIX maintains insurance coverage that meets the insurance limits required by the City. Upon award we are prepared to include the City of Fort Bragg, its elected and appointed officials, officers, etc., as named additional insureds. The cost of this insurance is accounted for through our hourly bill rates.

## Consultant Agreement

HELIX has reviewed the City's standard consultant services agreement as attached to the RFP and we do not have any issue with the provisions contained within.



*Appendix A*  
*Key Personnel Resumes*

### Summary of Qualifications



Mr. Edgerton draws upon his experience as both a project manager and a senior environmental planner to aid private companies, governmental agencies, and non-governmental organizations in the planning, entitlement, and permitting of land development and infrastructure improvement projects. Calling upon 28 years of experience in the environmental and land use planning industry, he has prepared, consulted on, and processed federal, state, and local permits in support of projects with the U.S. Army Corps of Engineers (USACE), U.S. Fish and Wildlife Services (USFWS), Bureau of Land Management (BLM), State and Regional Water Quality Control Board, and California Department of Fish and Wildlife (CDFW). Mr. Edgerton has worked both in the U.S. and abroad, and his project management skills are enhanced by his knowledge and use of Geographic Information Systems (GIS) for data analysis and presentation purposes, as well as principles and techniques of biological restoration. His work also focuses on California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA) compliance, and he has successfully processed more than 100 environmental compliance documents such as Environmental Impact Reports/Statements (EIR/EIS), Initial Study/Mitigated Negative Declaration (IS/MND), Environmental Assessments (EA). Mr. Edgerton is an accredited member of the American Institute of Certified Planners (AICP).

### Selected Project Experience

**Inyo County Renewable Energy General Plan Amendment and PEIR** (2013 - 2015). Project Manager assisting with preparation of a General Plan Amendment and Programmatic EIR (PEIR) to address State-mandated renewable energy demands and utility-scale renewable energy projects within the footprint of Inyo County. Responsible for the oversight of several subconsultants performing various technical analyses (such as a transmission corridor constraints assessment), preparation of a PEIR, and management of a proactive public involvement campaign. Nearly all technical environmental disciplines (e.g., air quality, biological resources, socioeconomic impacts, etc.) will be prepared using HELIX in-house resource experts with support from specialist sub-contractors. Primary issues of concern to County residents include the siting and placement of transmission lines, power delivery facilities, and solar/wind renewable energy infrastructure. HELIX is coordinating all actions directly with the County and County residents, California Energy Commission (CEC), CPUC, local Tribal governments, Los Angeles Department of Water and Power (LADWP), and applicable federal and state public resource agencies. Work performed for Inyo County from a grant administered by the CEC.

**Bishop Downtown Specific Plan & Mixed-Use Zoning Code Update EIR** (2021 - Present). Principal Planner for preparation of an EIR supporting proposed

### Education

Master of Science,  
Environmental  
Sciences, Colorado  
State University, 1999

Bachelor of Science,  
Natural Resource  
Management, San  
Diego State  
University, 1990

### Registrations/ Certifications

American Institute of  
Certified Planners,  
AICP Certified  
Planner No. 159640,  
2012

### Professional Affiliations

Association of  
Environmental  
Professionals  
American Planning  
Association, CCAPA

# Robert Edgerton, AICP CEP

## Project Manager/Principal Planner

redevelopment of downtown Bishop through development of a Specific Plan and mixed-use overlay. The primary purpose of the proposed project is to increase housing opportunities and density in the City's inner core while improving walkability, alternative transportation modes, and pedestrian amenities. The draft EIR is slated for public review in winter 2021 or spring 2022. Work performed for the City of Bishop in partnership with Alta Planning + Design.

**Inyo County Vacant Lands Inventory EIR** (2020 - Present). Principal Planner for preparation of an EIR and oversight of biological and cultural resources technical studies for a vacant lands inventory and zoning review of properties located throughout Inyo County. This information will be used to identify land that might be appropriate for zone changes to promote housing opportunities primarily by increasing the allowable residential density. The review will consider increasing the amount of multi-family zoning in the County, lowering some of the minimum lot size requirements, and adding zoning areas with principal permitting for mobile home parks. The review of the County's current zoning will also focus on commercial zones for opportunities for residential infill development. Areas near public transportation and other services will be considered prime, but due to the County's rural nature, other properties located in remote communities without these services might also be identified for zone changes. A primary component of this work includes public outreach meetings and communication with property owners. The project is funded through a grant provided by the California Department of Housing and Community Development. Work performed for Inyo County Planning Department.

**Victoria Crescent Initial Study and EIR Addendum** (2013 - 2014). Project Manager for construction of 46 residential homes on a 6.37-acre site in the City of Hercules, Contra Costa County. Managed an IS as an addendum to the EIR. This site was initially zoned as commercial flex and was never developed as part of the New Pacific Properties Project, a 206-acre development project. Work performed for City Ventures, LLC, with City of Hercules as the lead agency.

**Parkway Village H** (2013 - 2015). Project Manager for an IS/MND addressing an in-fill residential subdivision on a 5.44-acre site, as part of the City of Folsom Parkway Development Project in Sacramento County. Significant issues identified in the IS/MND included biological resources (elderberry shrubs and jurisdictional features), cultural resources, and traffic. Worked closely with the CEQA Lead Agency to identify feasible mitigation to offset adverse impacts associated with project implementation in a previously established community. The project was placed on hold by the client pending design changes to address the concerns of local residents. Work performed for the City of Folsom.

**Goddard School IS/MND** (2014 - 2014). Project Manager for a proposed private elementary school on an approximately 1.5-acre in-fill property within the City of Folsom. CEQA analysis identified the need for a sound barrier to protect school students from roadway noise associated with a nearby arterial highway. Minimal comment was received on the MND during the public comment period. The proposed project would accommodate 156 children with 20 staff members at full capacity, as well as construction of a child care center with other site improvements. Work performed for the City of Folsom.

**Veranda Subdivision** (2014 - 2015). Project Manager for an IS/MND for an in-fill residential subdivision project in the City of Folsom. The project proved highly controversial to local residents and was replaced with a less dense residential community. Significant public outreach occurred both prior to and during preparation of the draft IS/MND. Work performed for the City of Folsom.

### Summary of Qualifications



Ms. Dramko is the Environmental Planning Discipline Leader, Principal Planner, and Principal Air Quality/Noise Specialist at HELIX. She manages the production of environmental documents for a variety of project types, including residential, commercial, recreational, transportation, water/wastewater, and utilities. In her 22 years of experience with environmental reports under the California Environmental Quality Act (CEQA) and the National Environmental Policy Act (NEPA), Ms. Dramko has prepared environmental documentation for numerous planning projects, including Programmatic Environmental Impact Reports (PEIRs), Initial Study/Mitigated Negative Declaration (IS/MNDs), Environmental Assessments (EAs), Climate Action Plans (CAPs), Energy Action Plans, General Plans, and Specific Plans. In addition, Ms. Dramko is an accredited member of the American Institute of Certified Planners (AICP) and an accredited California Air Resources Board (CARB) Greenhouse Gas (GHG) Lead Verifier.

### Selected Project Experience

**North Torrey Pines Living and Learning Neighborhood EIR (2017 - 2018).** Project Manager for the housing and academic project that would redevelop a 13-acre surface parking lot on the west side of the UC San Diego campus within the Coastal Zone. Project includes a mix of educational, community, and residential uses. The project would provide approximately 2,000 beds for undergraduate students. The project was designed to meet U.S. Green Building Council Leadership in Energy and Environmental Design Platinum standards. The project EIR was tiered from the UC San Diego 2004 Long Range Development Plan EIR. Work performed for UC San Diego.

**La Jolla Innovation Center EIR (2019 - Present).** Project Manager for preparation of the EIR for the UC San Diego project that would develop a new building comprised of five levels of medical office and educational uses, two levels of above-grade parking, and two levels of subterranean parking at 8980 Villa La Jolla Drive, San Diego. Work involved preparation of an EIR and supporting technical studies (air quality/greenhouse gases, cultural resources), hosting online public hearings, and coordination between UC San Diego, UC Office of the President, and applicant teams. Work performed for UC San Diego.

**Marisol Specific Plan Initiative (2017 - 2018).** Principal Planner who provided quality control and assurance review for the technical reports prepared to support an EIR for a coastal resort in the City of Del Mar in the Coastal Zone. The Initiative consisted of a Specific Plan that provided the framework for a resort with 65 hotel guest rooms, 10 lower-cost shared visitor-serving accommodations, 31 villas (condos), and 22 affordable housing units intended for employees of the resort. Additional proposed facilities included restaurants, banquet facilities, spa/fitness center, meeting spaces, and parking facilities. Work performed for the City of Del Mar.

### Education

Master of Environmental Science and Management, University of California, Santa Barbara, 2000

Bachelor of Arts, Fine Arts, New College of Florida, Sarasota, 1991

### Registrations/Certifications

American Institute of Certified Planners, AICP Certified Planner No. 020810, 2006

California Air Resources Board, Greenhouse Gas Lead Verifier No. H-18-041, 2018

County of San Diego, Approved EIR (2007), Visual Impact (2007), Air Quality (2007) and Noise (2021) Report Preparer

### Professional Affiliations

American Planning Association

Association of Environmental Professionals

# Joanne Dramko, AICP

## Principal-in-Charge

**Mesa Housing Nuevo West and East EIR** (2016 - 2017). Principal-in-Charge for the EIR that analyzed two campus housing developments and a parking structure located within the East Campus Mesa Housing Neighborhood on the east side of campus. Key environmental issues included air quality, biological resources, hydrology/water quality, public services, traffic and circulation, and global climate change. The project was on an accelerated schedule to meet the housing goals set by the UC President's Student Housing Initiative. The project EIR was tiered from the UC San Diego 2004 Long Range Development Plan EIR. Work performed for UC San Diego.

**Sprouts Noise Assessment Study/P18-0111** (2018). Principal noise specialist for a noise technical report that analyzed a proposed Sprouts supermarket located west of East Vista Way between Arcadia Avenue and East Bobier Drive in the City of Vista. The analysis addressed potential noise impacts related to site construction and project operational activities including truck deliveries and ventilation equipment. Work conducted for the City of Vista.

**Camino Del Mar Bridge Replacement Environmental Documentation** (2018 - 2019). Principal Planner assisting the City of Del Mar with CEQA and Caltrans local assistance to replace a bridge that spans the San Dieguito Lagoon in Del Mar, California. Technical study areas included aesthetics/community character, air quality, biological resources, cultural resources, greenhouse gas, and noise, as well as a Section 4(f) Evaluation of parks and recreation facilities. Work performed as a subconsultant to Kleinfelder, with the City of Del Mar as the lead agency.

**1125 South Cleveland Street Residential IS/MND** (2015 - 2015). Senior Technical Specialist for a 15-unit residential townhome development within the Coastal Zone and adjacent to the LOSSAN rail corridor within the City of Oceanside. Key issues included noise and aesthetics. Provided technical review and quality control of the reports. Work performed as a subcontractor to Hallmark Communities, with the City of Oceanside as the lead agency.

**Phase III Recycled Water** (2013 - 2014). Project Manager for construction of new recycled water pipelines and support facilities in the City of Carlsbad, and initial expansion into neighboring water service agencies. Prepared the CEQA-Plus environmental review for the SWRCB Clean Water State Revolving Funds Program environmental review. Also prepared technical reports for air quality, biological resources, and cultural resources, and completed an environmental evaluation form to comply with federal requirements. Work performed for City of Carlsbad Municipal Water District.

**Balboa Mesa Shopping Center Project** (2013). Project Manager responsible for providing additional CEQA consulting services for the Balboa Mesa Shopping Center Project in the City of San Diego. Tasks included conducting a peer review of the Addendum to the MND and associated technical reports, and responding to public comments. Work performed for Regency Centers, with the City of San Diego as the lead agency.

**Coronado Strand Main Replacement** (2017). Senior Technical Specialist for the replacement of a water transmission pipeline from the intersection of 4th Street and Orange Avenue in the City of Coronado to the proposed transmission main realignment at the Coastal Campus Naval Base, and along Palm Avenue from Corvina Street to 13th Street in the City of Imperial Beach in the Coastal Zone. Work included technical oversight of the noise and air quality technical reports. Work performed as a subconsultant to Brown and Caldwell, with the City of Coronado as the lead agency.

### Summary of Qualifications



Ms. Owing is an Environmental Planning Group Manager and Senior Project Manager with 10 years of experience in the provision of all types of California Environmental Quality Act (CEQA)/National Environmental Policy Act (NEPA) documentation for a variety of project types including, but not limited to, residential/commercial/other land development, infrastructure improvement, renewable energy, cannabis cultivation/processing, transportation, and other planning/public works projects. Ms. Owing also develops CEQA review and strategy plans for public and private clients early on in the project planning phase and prepares project site constraints analyses from a CEQA/permitting perspective. Ms. Owing manages numerous extension of staff contracts with public agencies throughout northern California and provides environmental compliance support to public and private clients through overseeing the implementation of mitigation monitoring and reporting programs (MMRPs) throughout project construction. Additional skills include organizing public outreach meetings, data and spatial analysis using GIS, and word processing.

### Selected Project Experience

**Aramis Solar Energy Generation and Storage Project EIR (2018 - 2019).** Project Manager for preparation of an Environment Impact Report (EIR) for the construction and operation of a solar energy generation and storage project on 410 acres in unincorporated North Livermore, Alameda County. Led the preparation of the EIR and managed the preparation of the biological resources, cultural resources, air quality/greenhouse gas, and noise technical reports that were completed in-house as well as the traffic study prepared by a subconsultant. Other responsibilities included frequent coordination with the Alameda County Planning Department staff, project applicant, and multi-disciplinary project team as well as presenting and responding to public comments at the scoping meeting, public comment hearing, project approval hearing, and project appeal hearing. Work performed for Intersect Power, with the County of Alameda as the CEQA lead agency.

**Inyo County Vacant Lands Inventory EIR (2021 - 2022).** Deputy Project Manager for the preparation of an EIR analyzing the rezoning of vacant lands to promote housing opportunities in Inyo County. Project is funded through a Senate Bill 2 Planning Grant and intended to implement a process to streamline housing approvals by allowing housing development by right on the parcels identified. Responsible for the development of the project description and project alternatives analysis in close coordination with County staff. Other responsibilities include internal management of the preparation of the EIR and schedule tracking. Work is being performed for Inyo County.

### Education

Bachelor of Science, Environmental Economics & Policy; Minor-City/Regulatory Planning, University of California, Berkeley, 2013

Geographic Information System (GIS) Certified, San Francisco State University, 2015

### Registrations/Certifications

### Professional Affiliations

Association of Environmental Planners

# Lesley Owing

## Senior Environmental Planner

**Oakmont Senior Living (2020).** Deputy Project Manager for the preparation and processing of an EIR for a residential development project with age-restricted living. The proposed project includes a Specific Plan Amendment (SPA) for a previously approved entitlement in El Dorado Hills. The project applicant seeks to amend the SPA to allow for residential development, in addition to and in lieu of the previously approved commercial component. Responsible for the development of the project description and project alternatives in close coordination with the Project Manager and County staff. Work is being performed for Oakmont Senior Living in coordination with El Dorado County as the CEQA lead agency.

**Humboldt County As-Needed Environmental Compliance for Cannabis Operations (2017 - 2020).** Contract Manager responsible for reviewing the application materials and completing the ordinance conformance checklist, initiating contact with the applicant or consulting agent for outstanding information needs, coordinating with CDFW, CAL FIRE, and local tribes regarding project issues or concerns, and preparing the staff report for complete or near-complete applications. Services provided also included the provision of CEQA CEs and IS/MNDs for cannabis cultivation and operation projects. Work performed for the County of Humboldt.

**Flow Cannabis Institute Property (2020).** Project Manager for the preparation of a biological resources constraints analysis, cultural resources records search, and a CEQA due diligence assessment for a proposed cannabis cultivation operation in Mendocino County. Conducted a site visit, performed background research, and prepared a brief CEQA due diligence memorandum to inform the Client of the potential CEQA issues with the proposed project property using State CEQA Guidelines Appendix G as a tool to inform the memorandum. Work completed and performed for a private Client.

**Dumbarton Transit-Oriented Development, FMC Parcel C Project (2017 - 2019).** Project Manager for the preparation of an IS/Addendum for a medium/high density residential development project on a 17.4-acre formerly industrial project site requiring soil and groundwater remediation in Newark, Alameda County. Developed project description, drafted multiple resource sections, and managed multi-disciplinary team consisting of biologists, archaeologists, air quality/GHG specialists, and noise specialists. Attended Planning Commission and City Council hearings to provide environmental expertise for the IS/Addendum and assisted City staff in the implementation of the MMRP for this project. Work performed for Integral Communities.

**Mowry Villages Low-density Residential Development (2018 - 2019).** Project Manager for the preparation of an IS and technical studies for a low-density residential redevelopment project on an approximately 29-acre site in Newark, Alameda County. The project includes the demolition of an existing Pick-n-Pull auto scrap yard and development of a low-density residential project. Key environmental issues include hazards and hazardous materials, biological resources, cultural resources, and land use. Developed project description, managed subconsultants, drafted multiple resource sections, and attended numerous meetings with the City of Newark. Work performed for Integral Communities.

### Summary of Qualifications



Ms. Gustafson has nine years of environmental planning experience and assists clients in successful completion of the environmental review process. She is skilled in preparing Initial Studies/Mitigated Negative Declarations (IS/MNDs) and Environmental Impact Reports (EIRs) under the California Environmental Quality Act (CEQA), and Environmental Assessments (EAs) and Environmental Impact Studies (EISs) under the National Environmental Policy Act (NEPA). She has also assisted clients with managing responses to public comments on complex and high-profile projects. With seven years of experience on water, transportation, renewable energy, and residential, commercial, and other land use development projects, Ms. Gustafson has coordinated multidisciplinary teams, worked closely with staff from public agencies, and integrated input from a variety of stakeholders.

### Selected Project Experience

**Aramis Solar Energy Generation and Storage Project EIR (2020).** Deputy Project Manager for the preparation of an EIR for a 410-acre industrial solar energy generation and storage facility in Livermore. Managed the response to public comments on the Draft EIR. Assisted in the preparation of the Final EIR, including updates to the document in response to public comment. Work performed for Intersect Power, with the County of Alameda as the CEQA lead agency.

**Inyo County Vacant Lands Inventory EIR (2020 - 2022).** Environmental Planner for the preparation of an EIR analyzing the rezoning of vacant lands to promote housing opportunities in Inyo County. Analyzed potential environmental impacts under CEQA for a number of resource areas including land use, population and housing, utilities and service systems, and others and wrote associated sections of the EIR. Work performed for Inyo County.

**Willow Street Mixed-Use Project (2020 - 2021).** Environmental Planner for the preparation of an Initial Study (IS) for the FMC Willow and Grand Park mixed-use project within the Dumbarton Transit-Oriented Development (TOD) Specific Plan area in the City of Newark. Assisted with the preparation of an IS evaluating potential environmental impacts under CEQA. Work performed for The Willow Project Owner, LLC, with the City of Newark as the Lead Agency.

**Eagleheart Ranch Cannabis Cultivation CEQA Checklist (2021).** Project Manager for the preparation of an Appendix G Checklist for a cannabis cultivation project in Mendocino County. Conducted a site visit, performed background research, prepared project description, and managed the preparation of a project-specific Appendix G

### Education

Bachelor of Arts,  
Urban Studies and  
Planning, University  
of California, San  
Diego, 2013

### Registrations/ Certifications

American Institute of  
Certified Planners,  
AICP Certification,  
2019

### Professional Affiliations

Association of  
Environmental  
Professionals

American Planning  
Association

# Erin Gustafson, AICP

## Environmental Planner

Checklist analyzing the project's conformance with the County's Programmatic MND that was prepared for the adoption of the County's Medical Cannabis Cultivation Ordinance. Work performed for project applicant.

**Black Bart Trail Cannabis Cultivation CEQA Checklist (2021).** Project Manager for the preparation of two Appendix G Checklists for cannabis cultivation projects in Mendocino County. Conducted site visits, performed background research, prepared project descriptions, and managed the preparation of a project-specific Appendix G Checklists analyzing the projects' conformance with the County's Programmatic MND that was prepared for the adoption of the County's Medical Cannabis Cultivation Ordinance. Work performed for project applicant.

**UC Davis Sacramento Campus Long Range Development Plan (2020).** Project Coordinator and Environmental Planner for the preparation of a two-volume Supplemental EIR evaluating updates to the UC Davis Sacramento Campus's Long Range Development Plan (LRDP) and the Aggie Square development. Assisted senior project managers with the day-to-day management of the project including coordination of technical staff, subconsultants, and client staff to produce the Draft Supplemental EIR. Evaluated potential impacts to land use, population and housing, and recreation and wrote associated sections of both volumes of the Supplemental EIR. Work performed for the University of California, Davis.

**Sites Reservoir (2018 - 2020).** Project Coordinator and Environmental Planner for the preparation of an EIR/EIS evaluating the development of a 1.5 million acre-foot reservoir in Glenn and Colusa Counties. Assisted senior project managers with the day-to-day management of the project including coordination of technical staff and subconsultants to produce the document and coordination with agencies. Managed the response to public comments received on the Draft EIR/EIS. Work performed for the Sites Project Authority with the U.S. Bureau of Reclamation as the Lead Agency under NEPA.

**Manchester Cable Landing (2018 - 2019).** Environmental Planner for the preparation of an Initial Study/Mitigated Negative Declaration (IS/MND) evaluating a subsea fiberoptic cable landing site in Mendocino County. Worked closely with technical staff to assess potential environmental effects and authored the IS/MND. Work performed for RTI Infrastructure, Inc. with the California State Lands Commission as the Lead Agency.

**California WaterFix (2015 - 2018).** Project Coordinator and Environmental Planner for the preparation of a Final EIR/EIS and Supplemental EIR/EIS for physical and operational improvements to the State Water Project (SWP) system in the Sacramento-San Joaquin Delta. Assisted senior project managers with the day-to-day management of the project including document revisions by technical staff and subconsultants and coordination with agencies. Performed environmental impact analysis and authored several sections of the Supplemental EIR/EIS. Managed the response to over 30,000 public comments received on the Draft EIR/EIS and Recirculated EIR/EIS. Work performed for the California Department of Water Resources with the U.S. Bureau of Reclamation as the Lead Agency under NEPA.

### Summary of Qualifications



Mr. Backes is an archaeologist and cultural resources manager with 22 years of professional experience throughout California and the western Great Basin. He has conducted and supervised numerous projects in support of compliance with Sections 106 and 110 of the National Historic Preservation Act (NHPA), the California Environmental Quality Act (CEQA) and the National Environmental Policy Act (NEPA). He is also well versed in criteria for California

Register of Historical Resources (CRHR) and National Register of Historic Places (NRHP) evaluations. He has participated in a wide range of projects involving archaeological survey, testing, data recovery, monitoring, laboratory analysis, and the development of mitigation and treatment plans, and has over 17 years of experience in a decision-making capacity on cultural resources projects in California. His training and background meet the U.S. Secretary of the Interior's Professional Qualifications Standards for prehistoric and historic archaeology.

### Selected Project Experience

**Aramis Solar Energy Generation and Storage Project EIR (2018).** Principal Investigator responsible for supervising and conducting archival research, surveys, and Native American coordination in support of this proposed utility-scale solar project located on approximately 400 acres near the Contra Costa and Alameda county line north of the City of Livermore. Work included acting as primary author for the resulting Cultural Resources Technical Report and CEQA EIR section. Work performed for Intersect Power, with the County of Alameda as the CEQA lead agency.

**Avenida Senior Living (2020).** Senior Archaeologist for cultural resources services conducted in support of the CEQA IS/MND for the Avenida Senior Living facility in the City of Folsom. Project included archival research, Native American consultation, and an intensive pedestrian survey of the project area. The resulting Cultural Resources Assessment Report documented the findings of the study; assessed the potential for the project area to contain significant, undiscovered archaeological resources; and recommended avoidance and minimization measures to reduce potential impacts to unanticipated discoveries. Work performed for the City of Folsom.

**7-Eleven at 43 Middle Rincon Road (2019 - 2022).** Principal Investigator for an archaeological assessment and a historical resources evaluation for three properties in the City of Santa Rosa. The study includes archaeological and built-environment surveys, archival research, and Native American outreach, as well as preparing historic contexts, DPR 523 forms, and significance evaluations for a historic Craftsman house and warehouse. The resources are associated with a prominent

### Education

Master of Arts,  
Anthropology,  
California State  
University, Long  
Beach, 2009

Bachelor of Arts,  
Anthropology,  
California State  
University, Los  
Angeles, 2004

### Registrations/ Certifications

Bureau of Land  
Management  
Statewide Cultural  
Resource Use Permit  
(California), permit  
#CA-18-35,

Registered  
Professional  
Archaeologist,  
#1673640, 2009

### Professional Affiliations

Society for California  
Archaeology

Society for American  
Archaeology

American Rock Art  
Research Association

National Association  
of Environmental  
Professionals

# Clarus Backes, RPA

## Senior Archaeologist

local family and date to the early twentieth century. Work performed for TAIT & Associates with the City of Santa Rosa as the lead agency.

**7-Eleven Project and 1998 Whipple Rd** (2019 - 2020). Senior Archaeologist responsible for conducting archaeological surveys, archival research, and Native American coordination in support of the proposed development of a 7-Eleven gas station and convenience store at 1998 Whipple Road in the City of Union City. Acted as primary author for the project's Cultural Resources Assessment Report which presented the results of the assessment and provided recommendations for avoidance and mitigation during construction. Work performed for the City of Union City.

**Allison Drive Apartments** (2018). Principal Investigator responsible for conducting archaeological surveys, archival research, and Native American coordination in support of two proposed market rate apartment complexes on approximately 15 acres in the City of Vacaville in Solano County. Acted as primary author for the project's Cultural Resources Technical Report which presented the results of the assessment and provided recommendations for avoidance and mitigation during construction. Work performed for Guardian Commercial Real Estate, LLC.

**El Dorado County Bike Park** (2019 - ). Senior Archaeologist for cultural resources including delineation of an Area of Potential Effects (APE), extensive background research, Native American outreach, and an intensive pedestrian survey in support of CEQA IS/MND for a new bike park in El Dorado County. The project included a significance evaluation of several historic railroad features that seem to represent an early 20th century worker's camp associated with the Diamond and Caldor Railway's Diamond Springs facility. Work performed for the County of El Dorado.

**Dumbarton Transit-Oriented Development, FMC Parcel C Project** (2018). Principal Investigator responsible for addressing potential impacts to cultural resources by the development of 17.4 acres as part of the Dumbarton Transportation Oriented Development Specific Plan of the City of Newark, located adjacent to the Don Edwards San Francisco Bay Wildlife Refuge in Alameda County. Responsible for cultural resources compliance for the project, including archival research on early industrial development of Newark and the East Bay Area, Native American coordination, field surveys, and the National Register of Historic Places evaluation of a section of the Southern Pacific Railroad's historic Dumbarton Cutoff. Work performed for Integral Partners Funding, LLC.

**Mowry Villages Low-density Residential Development** (2019). Principal Investigator for a proposed low-density residential development on an approximately 29-acre project site in the City of Newark in Alameda County. The cultural resources assessment included an archival literature review, research related to Mowry's Landing, development of prehistoric and historic contexts for the project area, an intensive pedestrian survey of the project site, and completion of a Cultural Resources Assessment. The assessment determined that the area has a high sensitivity for buried prehistoric resources, and adapted mitigation measures from the Newark Areas 3 and 4 Specific Plan to address potential impacts to previously undiscovered cultural resources. Work performed for Integral Funding Partners, LLC.

### Summary of Qualifications



Mr. DeMartino is a GIS professional with 25 years of experience and an extensive background in applying GIS applications and workflows in support of biological, cultural, conservation, transportation, public works, municipal/ environmental planning, water, and engineering projects, with an emphasis on California Environmental Quality Act/National Environmental Policy Act (CEQA/NEPA) documentation, environmental permitting, and monitoring. He is proficient in the latest GIS software and technologies, including ArcGIS Desktop, ArcGIS Server, ArcGIS Online, ArcGIS Collector, Trimble TerraSync and Trimble Pathfinder Office GPS software, ERDAS Imagine and ERDAS StereoAnalyst, SketchUp, and several ArcGIS extensions, including Data Reviewer, Spatial Analyst, 3D Analyst, and Survey123 for ArcGIS. Mr. DeMartino has senior-level expertise both performing and supervising key GIS practices, including GIS data development, GPS data collection, CAD data integration, impact and overlay analysis, spatial modeling, mapping, and QA/QC of final deliverables.

### Education

Bachelor of Science,  
Geology, California State  
University, Northridge,  
2001

Bachelor of Science,  
Economics, Florida State  
University, 1992

### Selected Project Experience

**District56 Nature Area** (2019). Senior GIS Specialist responsible for GPS support for arborist survey team and development of final tree survey data and map for the approximately 30-acre District56 Nature Area in Elk Grove. This project encompasses the southern half of the City's new District 56/ Civic Center site. The Nature Area project centers around the conversion of a seasonal marsh to a 4.5-acre perennial pond with four islands. Work performed for the City of Elk Grove.

**Laguna Creek Trail and Bruceville Road Sidewalk Improvements** (2019). Senior GIS Specialist responsible for preparing mapping and analysis necessary to support NES and Aquatic Resource Delineation for a 29.45 acre trail improvement project in Sacramento County. Tasks included data creation, impact analysis, and cartography. Work performed for Mark Thomas & Company, with the City of Elk Grove Public Works Department as lead agency.

**Creekside Village Permitting Update** (2019 - Present). Senior GIS Specialist responsible for preparing mapping and analysis necessary to support Biological Resources Assessment and Aquatic Resources Delineation for a 240-acre commercial development site in Placer County. Tasks included data creation, impact analysis, and cartography. Work performed for Winn Ridge Investments, LLC.

**Duluth Road Warehouse** (2019 - Present). Senior GIS Specialist responsible for preparing mapping and analysis necessary to support BRA and Aquatic Resource delineation for a 7.5-acre commercial development site in Placer county. Tasks

# John DeMartino

## Senior GIS Specialist/Manager

included data creation, impact analysis, and cartography. Work performed for L&S Framing.

**Placer County Fuel Load Management Project** (2020 - Present). Senior GIS Specialist responsible for developing a mobile rapid fuel-load assessment survey for use by the Placer County Parks and Grounds Division on over 300 County-owned or maintained parcels. Developed a database utilizing ESRI's Survey123 for use by the County in tracking maintenance and assessments. Developed a customized Report to automate extraction of the data and helped train County staff on its use. Work performed for Placer County.

**Town of Loomis Tree Mitigation Master Plan** (2020 - Present). Senior GIS Specialist responsible for GIS support to conduct a tree preservation feasibility study to identify nearly 1,350 acres that are suitable for tree preservation, potentially through conservation easements or land acquisition. Aided in analyses by performing Image Classification to aid in mapping of land ownership, existing vegetation cover, and development. The data was used for in suitability analysis for potential land acquisition. Work performed for the Town of Loomis.

**City of Lincoln Open Space Consulting** (2019 - Present). (2015 - present) Senior GIS Specialist responsible for managing and updating all spatial data for the City's of Lincoln's open space preserves. Since 2015 HELIX has conducted annual monitoring and surveying in accordance with individual Operational & Management Plans for 12 open space preserves within the City of Lincoln. Surveys include conducting general inspections and biological surveys to evaluate the overall condition of the preserves. In addition to managing all the field data and providing annual mapping products has also developed survey collection forms to aid field staff in ESRI Collector and Survey123. Work performed for the City of Lincoln.

**Antonio Mountain Ranch Preserve** (2019 - Present). Senior GIS Specialist responsible for managing and updating all spatial data in support of annual preserve monitoring for this vernal pool, riparian, Swainson's hawk, and marsh mitigation bank in Placer County. Annual monitoring tasks performed by HELIX include floristic and hydrologic monitoring of created and restored vernal pools, riparian habitat monitoring, Swainson's hawk and tricolored blackbird nesting surveys and foraging habitat assessments, and conducting rangeland assessments using Residual Dry Matter (RDM) sampling at established monitoring plots. This contract also requires preparing an annual monitoring report according to the Interim Management Plan for the site as well as regular correspondence with the Interagency Review Team responsible for oversight of the Preserve. In addition to managing all the field data and providing annual mapping products, also developed survey collection forms to aid field staff in ESRI Collector and Survey123. Work performed for AKT Development Corporation.

**California High Speed Rail Construction Package 4** (2018 - 2019). GIS Manager for the California High Speed Rail Construction Package 4 – Fresno to Bakersfield, spanning a 22-mile stretch between the counties of Tulare and Kern. Responsible for GIS staff management and the analysis and cartographic products pertaining to environmental permitting. Products include Reexams, ITP amendments, trapping plans, monthly reports. Work performed for California Rail Builders with the State of California as the lead agency.

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### Distinguishing Qualifications

- Geotechnical Engineering
- Project Management
- Civil Engineering Design of Underground Structures
- Environmental Investigations
- Remediation Systems Design

### Years of Experience: 45

Years with SHN: 12

### Education

B.S., Civil Engineering,  
University of California, Davis;  
1975

Graduate Studies, University of  
California, Davis; 1975-1976

Graduate Studies, California  
State University, Fullerton;  
1977

Certificate in Environmental  
Site Assessment &  
Remediation, UC Berkeley  
Extension; 1998

### Professional Registrations

Registered Civil Engineer,  
California; No. 30345

Registered Geotechnical  
Engineer; California; No. 256

Professional Engineer / Civil  
and Geotechnical, Oregon; No.  
79564

Professional Civil Engineer,  
Washington, No. 46060

### Memberships

American Society of Civil  
Engineers

Association of Engineering  
Geologists

International Society of Soil  
Mechanics and Foundation  
Engineers

## John H. Dailey, PE, GE

### Senior Geotechnical Engineer



### Relevant Experience

Mr. Dailey has more than 40 years of experience in geotechnical, civil, and environmental engineering while working with federal, state, and local regulatory agencies as well as the private sector. His experience includes project management, subsurface geotechnical and environmental investigations, site remediation, plan and procedure development, specification and bid preparation, permitting, and subcontractor selection. Field experience includes excavation and drilling for geotechnical and environmental investigations, including soil and groundwater sampling, and field and laboratory soil testing.

### Representative Projects

**PG&E Unit 21 Geothermal Powerplant, Geysers, CA.** Project Manager /Engineer during geotechnical investigation for major geothermal powerplant.

**Santa Clara County Courthouse, San Jose, CA.** Project Manager/Engineer during geotechnical investigation for high rise structure with deep basement supported on driven pile foundation system.

**Calera Winery, Hollister, CA.** Project Engineer during geotechnical investigation and design of large underground wine cave complex in highly disturbed rock adjacent to the San Andreas Fault. Design included reinforced shotcrete tunnel liners and shotcrete/soil nail portal walls.

**City of Coos Bay Wastewater Treatment Plant, Coos Bay, OR.** Project Engineer during geotechnical investigation and develop design criteria for a below grade wastewater treatment facility.

**Fisherman's Terminal Building, Eureka, CA.** Project Engineer during geotechnical investigation and design consultation for large high one-story building constructed over bay mud and supported on a driven pile foundation system.

**Central Dock Development, Coos Bay, OR.** Project Engineer during geotechnical investigation for commercial/residential development along the waterfront and provide recommendations to minimize/mitigate consolidation and liquefaction conditions underlying site, including driven piles, stone columns and surcharge/wick drains.

**Buena Vista Winery, Sonoma, CA.** Project Manager/Engineer during geotechnical investigation and design consultation for structural renovation of 125-year-old, stone building (historical monument) and adjoining tunnels, for conversion into tasting rooms.

### Distinguishing Qualifications

- Subsurface investigations
- Geologic and geotechnical engineering
- Subcontractor oversight

### Years of Experience: 14

With SHN: 2

### Education

M.S., Geology, San Jose State University, San Jose, CA; 2017

B.A., Geology, Humboldt State University, Arcata, CA; 2007

### Professional Registrations and Affiliations

- Certified Engineering Geologist, CA, No. 2737
- Professional Geologist, CA, No. 9283
- OSHA 29 CFR 29 CFR Part 1910.120 Certified
- Hazwoper 40-Hr Certified
- Association of Engineering Geologists
- Geotechnical Extreme Event Reconnaissance

## Christina Tipp, PG, CEG Certified Engineering Geologist



### Relevant Experience

Christina Tipp has more than 14 years of professional experience in engineering geology and geotechnical engineering. She specializes in evaluating geologic risk, geotechnical and geologic drilling and sampling, geologic inspections, and geotechnical investigations. Ms. Tipp is skilled in a multitude of subsurface exploration techniques, geologic mapping, review of LIDAR and aerial photography, gathering geologic research for a project site, and communicating project progress and relaying results to the project team. She has “hands-on” experience in geologic and geotechnical field investigations, addressing development in geologic hazard zones, and providing practical insights throughout the life of the project.

### Representative Projects

**Geotechnical Investigation and Geologic Hazard Report for Proposed New Classroom and Library, Redwood Elementary, Fort Bragg, CA.** Engineering Geologist for a geologic hazard and geotechnical investigation of new classroom and library buildings at a public school in Fort Bragg, California.

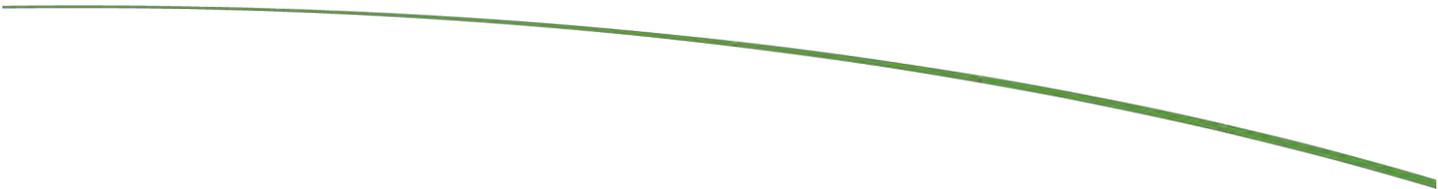
**Geotechnical Assessment of Subsurface Soil Void, Sherwood Oaks Health Center, Fort Bragg, CA.** Engineering Geologist to assess a soil void beneath a portion of the existing foundation at a health center and provide mitigation measures.

**Orr Creek Common Housing Development, Ukiah, CA.** Project Geologist for geologic hazard review and geotechnical investigation for high-density housing on liquefiable soils with shallow ground water.

**Harris Quarry - Excavation Slope Inspection Report, Willits, Mendocino County, CA.** Engineering Geologist for quarry inspection to fulfill County and Mine Safety and Health Administration requirements.

**Geotechnical Investigation, Proposed Black Oak Ranch Railcar Bridge, Laytonville, CA.** Engineering Geologist for an investigation to design a railcar bridge.

**Highway Landslide Geotechnical Investigations, Caltrans, Mendocino and Del Norte Counties, CA.** Engineering Geologist responsible for logging and describing earth materials and failure planes for Caltrans to be used for design and construction of highways.



*Appendix B*  
*Budget and Rate Sheet*

Proposed Budget

HELIX LABOR	Rate	Task 1		Task 2		Task 3		Task 4		Task 5.1		Task 5.2		Task 5.3		Task 5.4		Task 6		Task 7		Task 8		Task 9		Task 10		Task 11		Task 12		TOTAL												
		Project Management	Hours	Cost	Kick-Off Meeting	Hours	Cost	Notice of Preparation	Hours	Cost	Evaluate Existing Tech Studies	Hours	Cost	Geotechnical	Hours	Cost	CRAR	Hours	Cost	AQ/GHG/Energy/HRA	Hours	Cost	Noise	Hours	Cost	ADEIR	Hours	Cost	Pre-Print DEIR	Hours	Cost	DEIR	Hours	Cost	Admin RTC/FEIR	Hours	Cost	Final EIR	Hours	Cost	FOF/SOC	Hours	Cost	Public Hearing Support
Principal Planner	\$245	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	2	\$490	2	\$490	8	\$1,960	2	\$490	-	\$0	4	\$980	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	18	\$4,410	
Principal Planner	\$235	36	\$8,460	4	\$940	16	\$3,760	4	\$940	4	\$940	2	\$470	2	\$470	18	\$4,230	12	\$2,820	4	\$940	8	\$1,880	8	\$1,880	12	\$2,820	24	\$5,640	8	\$1,880	8	\$1,880	12	\$2,820	24	\$5,640	156	\$36,660					
Sr. Envir Project Manager	\$160	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	80	\$12,800	40	\$6,400	8	\$1,280	24	\$3,840	8	\$1,280	24	\$3,840	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	184	\$29,440			
Principal Acoustician	\$210	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	8	\$1,680	4	\$840	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	12	\$2,520			
Envir. Project Manager I	\$130	-	\$0	-	\$0	16	\$2,080	8	\$1,040	4	\$520	-	\$0	-	\$0	40	\$5,200	120	\$15,600	40	\$5,200	8	\$1,040	24	\$3,120	24	\$3,120	24	\$3,120	24	\$3,120	24	\$3,120	-	\$0	-	\$0	-	\$0	308	\$40,040			
Sr Noise/AQ Specialist III	\$200	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	8	\$1,600	-	\$0	4	\$800	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	12	\$2,400			
Air Quality/Noise Specialist	\$125	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	65	\$8,125	-	\$0	4	\$500	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	69	\$8,625			
Environmental Planner I	\$95	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	80	\$7,600	40	\$3,800	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	120	\$11,400			
Principal Cultural Resources Specialist	\$165	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	16	\$2,640	-	\$0	-	\$0	4	\$660	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	20	\$3,300			
Cultural Resources Project Manager	\$135	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	40	\$5,400	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	40	\$5,400					
Sr. GIS Specialist	\$165	-	\$0	-	\$0	4	\$660	-	\$0	-	\$0	4	\$660	-	\$0	-	\$0	24	\$3,960	8	\$1,320	4	\$660	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	44	\$7,260			
Word Processor	\$85	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	24	\$2,040	8	\$680	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	-	\$0	32	\$2,720			
Subtotal HELIX Labor		36	\$8,460	4	\$940	36	\$6,500	12	\$1,980	8	\$1,460	62	\$9,170	77	\$10,685	52	\$7,840	370	\$50,990	150	\$20,710	24	\$3,920	60	\$9,820	40	\$6,280	60	\$9,780	24	\$5,640									1,015	\$154,175			

SUBCONSULTANTS	Task 1	Task 2	Task 3	Task 4	Task 5.1	Task 5.2	Task 5.3	Task 5.4	Task 6	Task 7	Task 8	Task 9	Task 10	Task 11	Task 12	TOTAL		
<b>Subconsultant (SHN)</b>																		
Labor	-	\$0	-	\$0	-	\$0	-	\$21,500	-	\$0	-	\$0	-	\$0	-	\$0	-	\$21,500
Other direct costs		\$0		\$0		\$0		\$0		\$0		\$0		\$0		\$0		\$0
Subtotal Subconsultant	-	\$0	-	\$0	-	\$0	-	\$21,500	-	\$0	-	\$0	-	\$0	-	\$0	-	\$21,500
Subtotal Subconsultant Cost		\$0		\$0		\$0		\$21,500		\$0		\$0		\$0		\$0		\$21,500
HELIX mark-up	0%	\$0		\$0		\$0		\$0		\$0		\$0		\$0		\$0		\$0
Total Subconsultant Cost		\$0		\$0		\$0		\$21,500		\$0		\$0		\$0		\$0		\$21,500

EXPENSES	Task 1	Task 2	Task 3	Task 4	Task 5.1	Task 5.2	Task 5.3	Task 5.4	Task 6	Task 7	Task 8	Task 9	Task 10	Task 11	Task 12	TOTAL
Document Reproduction	\$36	\$55	\$0	\$18	\$36	\$0	\$59	\$269	\$783	\$264	\$73	\$164	\$200	\$200	\$0	\$2,157
Travel (car rentals, meals, hotels,etc)	\$0	\$0	\$455	\$0	\$0	\$255	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$327	\$1,036
Noise Meter	\$120	\$0	\$0	\$0	\$0	\$0	\$0	2 \$240	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$240
Records Search	\$0	\$0	\$0	\$0	\$0	\$500	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$500
Subtotal Expenses	\$36	\$55	\$455	\$18	\$36	\$755	\$59	\$509	\$783	\$264	\$73	\$164	\$200	\$200	\$327	\$3,933
HELIX Mark-Up on Expenses	10%	\$4	\$5	\$45	\$2	\$4	\$75	\$6	\$51	\$26	\$7	\$16	\$20	\$20	\$33	\$392
Total Expenses	\$40	\$60	\$500	\$20	\$40	\$830	\$65	\$560	\$861	\$290	\$80	\$180	\$220	\$220	\$360	\$4,325

TOTAL	Task 1	Task 2	Task 3	Task 4	Task 5.1	Task 5.2	Task 5.3	Task 5.4	Task 6	Task 7	Task 8	Task 9	Task 10	Task 11	Task 12	TOTAL
	\$8,500	\$1,000	\$7,000	\$2,000	\$23,000	\$10,000	\$10,750	\$8,400	\$51,851	\$21,000	\$4,000	\$10,000	\$6,500	\$10,000	\$6,000	\$180,000

**CONSULTING SERVICES**

Consulting services performed by HELIX typically include, but are not necessarily limited to, office, field, meetings, hearings and travel time. Consulting services for expert witness review, deposition, and/or testimony will be provided at one and one-half times our professional rates.

**DIRECT COSTS**

Certain identifiable direct costs will be charged to the project at cost plus ten percent. Examples of direct costs include subconsultants, vehicle or equipment rentals, airplane and train fares, parking, per diem and lodging, mileage, communications, reproduction, and supplies. A 4-wheel drive premium will be charged at \$25 per project day. There will be additional charges for plotting, color printing, aerial photographs and GPS services.

**PAYMENT**

Invoices will be submitted monthly. Payment on invoices is due within thirty days of receipt. If payment is not paid when due, then such sum shall bear interest at 1 ½ % per month on the unpaid balance, not to exceed the maximum legal rate of interest.

**PROFESSIONAL RATES**

Current hourly rates for consulting services:

Principal	\$230-265
Principal Acoustician	\$190-215
Principal Biologist	\$200-245
Principal Landscape Architect	\$160-190
Principal Planner	\$210-245
Principal Regulatory Specialist	\$190-245
Senior Regulatory Specialist	\$140-185
Regulatory Specialist	\$95-135
Principal Cultural Resources Specialist	\$165-185
Senior Project Manager I-III	\$160-210
Project Manager I-III	\$125-180
Assistant Project Manager	\$125-140
Environmental Planner I-III	\$95-120
Senior Fisheries Scientist	\$200-235
Senior Noise/Air Quality Specialist	\$185-205
Noise/Air Quality Specialist	\$115-145
Environmental Compliance Specialist	\$75-125
Archaeology Field Director	\$115
Senior Archaeologist	\$120-170
Staff Archaeologist	\$75-115
Senior Architectural Historian	\$150-160
Architectural Historian	\$100-145
Senior Landscape Architect	\$135-160
Landscape Architect	\$110-130
Landscape Planner I-III	\$95-115
Senior Scientist	\$135-175
Biologist I-V	\$90-135
Senior GIS Specialist	\$130-170
GIS Specialist I-III	\$75-120
Graphics	\$115
Technical Editor	\$110-120
Operations Manager	\$95-140
Word Processor I-III	\$85-90
Clerical	\$65-75

*Rates are subject to change on a yearly basis*

# M-GROUP PROPOSAL GROCERY OUTLET EIR

PRESENTED TO  
**CITY OF FORT BRAGG**



Image by Zahid Lilani from Pixabay

*m-group.us*  
*a new design on urban planning*

• **SUBMITTED BY M-GROUP**  
MARCH 29, 2022

499 HUMBOLDT STREET  
SANTA ROSA, CA 95404





**M-GROUP**

*a new design on urban planning*

policy planning • urban design • environmental review • historic preservation • community engagement • staffing solutions

March 29, 2022

Heather Gurewitz  
Associate Planner  
Community Development Department  
City of Fort Bragg  
416 North Franklin Street  
Fort Bragg, CA 95437  
Email: hgurewitz@fortbragg.com

**SUBJECT: Proposal to Provide Environmental Review Services for the City of Fort Bragg Proposed Grocery Outlet Project**

Dear Heather Gurewitz,

Metropolitan Planning Group (M-Group) is pleased to submit this proposal to provide environmental review services for the proposed Grocery Outlet Project (Project), in accordance with the California Environmental Quality Act (CEQA) and the City's Local Coastal Program. As requested by the City of Fort Bragg (City), M-Group proposes to prepare an Environmental Impact Report (EIR) to evaluate project level impacts in accordance with CEQA.

We have a successful track record of providing environmental review services to the City of Fort Bragg on past projects and are familiar with the local requirements and procedures. M-Group is well suited to provide the services that the City of Fort Bragg requests. Our staff is familiar with conducting environmental analyses in compliance with CEQA, and in line with federal, state, and local regulations and policies. This proposal outlines our team's understanding of the project and addresses our proposed scope of work, staffing, budget, and timeline.

Principal Olivia Ervin will provide oversight, quality control and serve as the Principal-in-Charge. Senior Planner Krystle Rizzi will be designated Project Manager and point of contact to the City, with support by Associate Planner Alaina Lipp. M-Group associate planners and analysts will also provide assistance.

The M-Group Team includes technical experts with whom we routinely partner to peer review the record, provide input on industry standards, and identify potential environmental effects of the project. Monk & Associates will peer review the Biological Resources Assessment, Evans & De Shazo will peer review the Cultural Resources Assessment, W-Trans will peer review the Transportation Analyses, and Illingworth & Rodkin will perform Air Quality Screening, provide a qualitative discussion of health risks, and will perform an Acoustical Analysis. The M-Group Team will conduct a thorough review of the existing record of documentation

and work with our professional experts to clarify existing conditions, disclose potential environmental effects of the project, identify feasible mitigation measures, and investigate project alternatives.

We trust that information contained herein is sufficient for the City's purposes in evaluating this proposal. Should any additional information, qualifications or references be necessary, they can be provided upon request. Our proposal is good for up to 90 days from the time of submittal.

We appreciate the opportunity to submit a proposal for this project and look forward to providing environmental services to the City of Fort Bragg. Please do not hesitate to contact us should you have any questions.

Sincerely,



**OLIVIA ERVIN**  
*Principal*  
[oervin@m-group.us](mailto:oervin@m-group.us)  
707.540.0723 x202

**Point of Contact | Project Manager**

**Krystle Rizzi, Senior Planner**  
499 Humboldt Street  
Santa Rosa, CA 95404  
[krizzi@m-group.us](mailto:krizzi@m-group.us) | 707.540.0723 x208



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M-Group exists to bring innovative and effective planning solutions to Bay Area cities and counties. Since the creation of the firm in 2006, we have brought the full range of planning services to over 65 Bay Area communities.

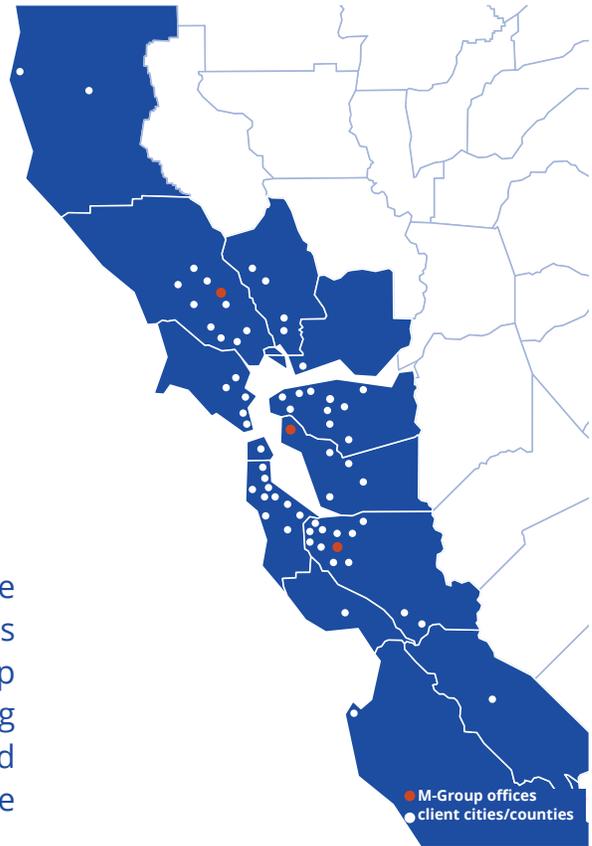
We are committed to *a new design on urban planning*. This approach to planning takes many forms both in our work and in our relationships with our clients. This new design includes:

- Very clear communication
- An enthusiastic and fun approach to planning
- A commitment to continuous improvement
- Creating a sustainable future by balancing the needs of the natural and built environments
- Creating a long-lasting, employee-centered, client focused firm

M-Group planners have extensive experience working on complex and high-profile projects throughout the region. Our planning group brings together a broad range of planning expertise and substantial real-world experience to help communities plan for the future.

Our team of 40+ planners is focused on delivering the following services:

- POLICY PLANNING
- URBAN DESIGN
- ENVIRONMENTAL REVIEW
- HISTORIC PRESERVATION
- COMMUNITY ENGAGEMENT
- STAFFING SOLUTIONS



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● **BERKELEY**  
 510.473.3090  
 2808 Adeline Street  
 Unit 1  
 Berkeley, CA 94703



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## RELEVANT EXPERIENCE

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### **CITY OF PETALUMA** | *FULL PLANNING DEPARTMENT SERVICES-VARIOUS PLANNING AND ENVIRONMENTAL SERVICES*

Since 2009 M-Group has provided the full range of services required of an in-house Planning Division to the City of Petaluma. In this capacity, M-Group environmental planners have prepared multiple environmental review documents for a variety of development and municipal projects. Tasks include review of site plans and schematics, characterization of existing conditions and development of a CEQA compliant project description. Records review of past studies and documentation are performed and project specific technical studies are identified. M-Group environmental planners collaborate with the project team to ensure that technical studies utilize appropriate methodology and clearly present findings and conclusions. M-Group completes initial studies, determines appropriate level of environmental review and carries out the environmental analysis. M-Group environmental planners present CEQA findings to the public and decision makers at hearings. M-Group prepares and files CEQA notices with the County Clerk and State Clearinghouse as appropriate. A few examples of specific projects completed for the City of Petaluma or remain in process are as follows:



- Managed and Co-Authored EIR for Riverfront Mixed-Use Project. Processed development application for the requested entitlements and environmental. The Project contained approximately 39-acres and involved a tentative subdivision map and rezoning including a mix of 237 residential units, 60,000 square feet of office, 30,000 square feet of retail, a 120-room hotel, a 3.5-acre recreational park, and a linear riverfront park.
- Managed EIR for the Rainier Cross Town Connector, prepared staff reports and findings and presented to decision makers. The project consisted of a 0.65 mile 4-lane arterial roadway extending over the Petaluma River and under Highway 101, connecting the east and west sides of Petaluma.
- Managing preparation of an EIR for the proposed Scott Ranch Project consisting of a 28

## B. RELEVANT EXPERIENCE

lot single family subdivision and extension of Helen Putnam Park on an approximately 58 acre property at the City margin within the Urban Growth Boundary.

- General Plan Consistency Exemption pursuant to CEQA Guidelines Section 15183 for the Wasatch Storage Facility. The project consisted of a mini-storage facility on an underutilized parcel within the City's Urban Growth Boundary.
- Silk Mill Addendum to IS/MND. The project consisted of the conversion of the historic Silk Mill, listed on the National Register of Historic Places, as a hotel.
- IS/MND City-wide Creeks Master Plan. The project included a comprehensive activities manual and associated environmental review for the City of Petaluma's Citywide Creeks Maintenance Plan.

**Key Staff:** Olivia Ervin, Krystle Rizzi

### CITY OF PACIFICA | *PACIFICA SCHOOL DISTRICT WORKFORCE HOUSING EIR*

M-Group was retained by the City of Pacifica to prepare an Environmental Impact Report (EIR) for redevelopment of a site containing a former elementary school, Oddstad School, which was closed in 2005 due to reduced enrollment numbers. The project proposes to demolish the existing, non-operational elementary school located on the 12.49-acre site at 930 Oddstad Boulevard, and construct 70 residential units, 11 of which will be below market rate (BMR), affordable to low- and moderate-income households. In addition to BMR units, the project will allocate a portion of the units as workforce housing for teachers and staff of the school district. The project will retain an existing recreational field and provide community amenities including a recreation building, restroom, and office. Other improvements include landscaping, lighting, and parking. The Draft EIR is currently in process and is anticipated to be released for public review and comment during summer of 2022.

**Key Staff:** Olivia Ervin, Krystle Rizzi, Alaina Lipp

### CITY OF HEALDSBURG | *544 TUCKER STREET EIR*

M-Group was retained by the City of Healdsburg, in April 2018 to prepare an Environmental Impact Report (EIR) to analyze impacts associated with the demolition of a historic resource. The subject resource — a c. 1872 Greek Revival residence— had previously been identified as a



contributor to the Tucker Street Historic District, which was determined eligible for listing on the California Register of Historic Resources. Eligibility of the 68-property district was based on its place in the history of Healdsburg's urban development and because it conveys an excellent representation of residential architectural styles from Healdsburg's founding through the present.

M-Group facilitated a scoping meeting and integrated input received into the EIR. M-Group prepared an EIR that disclosed and analyzed the project's potential to adversely affect the

## B. RELEVANT EXPERIENCE

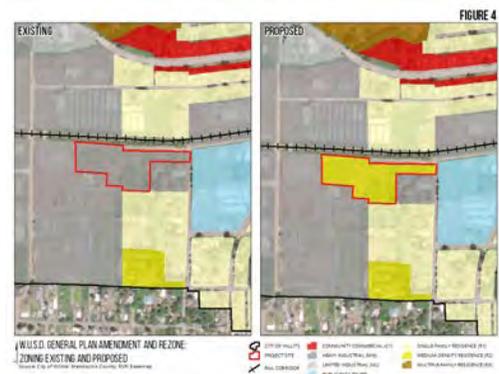
integrity of an identified historic resource (i.e. the eligible Tucker Street District). The EIR identified mitigation measures including photographic documentation prior to demolition, implementation of a salvage plan, erection of a plaque detailing the history of the home, and development of a self-guided walking tour booklet for the Tucker Street Historic District.

The Draft EIR was circulated for public review and was considered by the Planning Commission for adequacy. All comments received were assembled and responded to in the Final EIR. M-Group prepared the findings of fact, statement of overriding considerations, completed the staff report for the demolition application, and presented the project and CEQA findings at public hearings. The City of Healdsburg certified the EIR, adopted the MMRP and statement of overriding considerations and approved the project in August 2018.

**Key Staff:** Olivia Ervin, Krystle Rizzi

### WILLITS UNIFIED SCHOOL DISTRICT | WUSD GENERAL PLAN AMENDMENT AND REZONE

M-Group contracted with the Willits Unified School District (WUSD) to provide CEQA review services. The M-Group team worked collaboratively with City staff, the applicant team, and consultants through the CEQA review process, which culminated in an Initial Study/Mitigated Negative Declaration (IS/MND). The proposed project included a General Plan amendment and Rezone of approximately 3.15-acres of a 5.68-acre property from industrial General Plan and Zoning designations to residential.



The IS/MND analyzed a conceptual multi-family residential development at the highest permitted density, which could ultimately be facilitated by the project. M-Group's CEQA review services included peer review of a traffic impact study and archaeological survey report. To comply with changes to the CEQA Guidelines, which required that as of July 1, 2020 transportation impacts be evaluated using a vehicle miles traveled (VMT) metric, M-Group worked closely with the traffic consultant to ensure potential impacts were adequately addressed, which included preparation of a Transportation Demand Management (TDM) plan.

Through the review process, M-Group identified potential land use adjacency conflicts as a result of permitting a residential use adjacent to existing industrial uses. The CEQA document disclosed that although the project would not result in significant noise or air quality impacts, new residents introduced onsite could be exposed to elevated noise and air quality emissions associated with the adjacent, legally operating industrial use. In accordance with the City's guiding policies, a land use mitigation measure was imposed on the project to establish a buffer between the existing industrial use and a future residential development on the site.

M-Group also assisted the City in responding to comments on the IS/MND and the project merits, prepared the City Council staff report and resolution, and filing notices and posting to the SCH. On August 26, 2020, the Council approved the project entitlements, General Plan Amendment and Rezone, and the IS/MND.

**Key Staff:** Olivia Ervin, Krystle Rizzi

# C. KEY PERSONNEL

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## M-GROUP TEAM

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M-Group's Team has a robust background in the preparation of environmental review documents, technical studies, and analysis for multiple cities throughout the Bay Area. Below is a brief synopsis of our project team member's experience and background. Each team member's resume is included in **Appendix A**. In addition to our key team members noted below, M-Group is also able to pull from the expertise of over 40 in-house employees as demand and areas of expertise warrant.

### OLIVIA ERVIN | *PRINCIPAL-IN-CHARGE*



Olivia has a strong background in environmental planning with over 14 years of experience. Her hands on management style, ability to navigate political situations and to work with a wide range of groups, commissions, committees, and organizations provides leadership that ensures successful project implementation. She has assisted multiple jurisdictions with environmental review of mixed-use projects, residential development, business and office park, and industrial projects. In addition to land use and development projects, Olivia also has experience with program level review, General Plan Elements, restoration activities, and infrastructure projects including complete streets, flood control projects, and water supply planning. She is well versed in the CEQA process as well as all aspects of environmental review and compliance. Olivia is an effective team leader and assists jurisdictions in fulfilling lead agency obligations in accordance with CEQA.

### KRYSTLE RIZZI | *SENIOR PLANNER + PROJECT MANAGER*



Krystle is an accomplished senior planner with a broad range of experience in preparing environmental documents in compliance with CEQA. She is currently serving as project manager for the Pacifica School District Workforce Housing EIR, and is responsible for managing internal deadlines and providing high quality deliverables to City staff for review and acceptance. Krystle has developed expertise in reviewing and summarizing technical

## KEY PERSONNEL

studies to support environmental analyses, has served as the lead author for several environmental review projects, and is highly proficient in the development review process including preparing and presenting comprehensive staff reports. She is a talented writer and has a background in GIS, document production, and public outreach. Krystle's broad range of planning experience makes her highly qualified to manage environmental review from start to finish.

### ALAINA LIPP | ASSOCIATE PLANNER



Alaina Lipp is a highly educated and astute planner with a professional resume emphasizing environmental services, sustainability, and wastewater and stormwater management. She is well versed in land use planning, development review, policy planning, and grant writing. Her strong project management skills are founded on organization and open, fluid communication. She is an intentional planner who conducts thorough research and produces concise reports. She is a thoughtful speaker able to present effectively to commissions, councils, and the public.

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## SUBCONSULTANTS

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M-Group's team also includes sub-consultants who will peer review available past records and reports, prepare supplemental materials to bolster the record, and contribute their expertise throughout the environmental review process. M-Group has long-standing relationships with our sub-consultants and has successfully managed the preparation of technical studies and incorporated results of the analyses into environmental documents. For the proposed Grocery Outlet project, M-Group will leverage the expertise of the following subconsultants to prepare technical reports that inform the environmental review:

- Monk & Associates, Biological Resources Assessment
- Evans & De Shazo, Cultural Resources
- Illingworth & Rodkin, Acoustical Analysis
- W-Trans, Transportation Analysis



# D. REFERENCES

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## REFERENCES

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### **Peggy Flynn**

City Manager, City of Petaluma  
pflynn@cityofpetaluma.org  
707.778.4345

### **Christian Murdock**

Deputy Director of Planning, City of Pacifica  
cmurdock@pacifica.gov  
650.738.7341

### **Maya DeRosa**

Planning and Building Director, City of St. Helena  
(formerly City of Healdsburg)  
mderosa@cityofstheleena.org  
707.967.2783

### **Dusty Duley**

Community Development Director, City of Willits  
dduley@cityofwillits.org  
707.459.7124



# E. PROJECT UNDERSTANDING

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## PROJECT UNDERSTANDING

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It is our understanding that the City received and processed an application for a proposed Grocery Outlet on an approximately 1.63-acre site at 825, 845, and 851 South Franklin Street (APNs 018-120-47; -48; and -49). The project includes demolition of an existing 16,436-square-foot vacant office building, 47-space parking lot, and associated site improvements, and will construction a 16,157-square-foot, one-story, retail store with 55-space parking lot, associated site improvements, and infrastructure. The City published and circulated an Initial Study/ Mitigated Negative Declaration (IS/MND) for the proposed Grocery Outlet project for a 30-day review period from January 14, 2021 to February 16, 2021. At the Planning Commission public hearing on June 9, 2021 the MND was adopted and the project entitlements, including a Coastal Development Permit, Design Review Permit, and Lot Merger were approved. Following approval by the Planning Commission, the project was appealed to the City Council, who ultimately affirmed the adequacy of the IS/MND and upheld the Planning Commission's decision to adopt the IS/MND and approve the project entitlements. It is understood that the applicant has elected to vacate the approved entitlements and has requested that the City prepare an Environmental Impact Report (EIR) to further analyze and document potential environmental impacts that may occur as a result of the project.

The project, as a retail store is permitted by-right in the Highway Visitor Commercial Zone, however, given the site's location within the coastal zone, approval of a Coastal Development Permit, Design Review, and Parcel Merger are required. The discretionary nature of the approval is therefore subject to the provisions of the California Environmental Quality Act. As such, at the request of the City of Fort Bragg, M-Group has scoped the preparation of an Environmental Impact Report. It is understood that the following relevant project information and technical studies have been or will be made available to the M-Group Project Team:

- Project Narrative
- Site Plans/Civil/Landscaping
- Stormwater Control Plan
- Geotechnical Reports
- Biological Report

## APPROACH

- Cultural Report
- Project Objectives
- Phase 1 ESA
- Traffic Analyses
- Wetland Report
- Arborist Report
- IS/MND prepared by LACO Associates
- Public/Agency Comments received on IS/MND
- Correspondence with responsible/trustee agencies
- Staff reports and resolutions
- Other relevant project materials

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### APPROACH

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M-Group will provide an environmental compliance document that is concise, accurate, and able to withstand legal scrutiny. The M-Group team will work collaboratively with the City and may coordinate with the applicant as appropriate. Project Manager Krystle Rizzi will be available throughout the environmental review process to coordinate with City staff and will provide regular updates to the City to ensure that the schedule is proceeding in the agreed upon manner. Specific milestones and timelines will be determined at the kick-off meeting and periodically reviewed as part of the ongoing project management task.

We will take the initiative to keep the project on track by working proactively with our team, City staff, responsible agencies and the applicant team (as appropriate through the City). M-Group understands that completion of a successful environmental document is dependent upon clear and direct communication, sound technical analysis, and attentive project management. We stay actively engaged in our projects from inception to completion.

Based on our preliminary review of prior documents prepared for the project as well as public comments received, we anticipate that biological resources, transportation, construction impacts (air quality and noise), utilities and water usage, and stormwater, will be of particular interest. As such, our proposal includes peer review of previously prepared technical documentation as well as preparation of additional technical reports to further document the existing site conditions, evaluate potential impacts associated with project construction and operation, and develop recommendations to avoid, reduce or offset potentially significant impacts. A summary of the approach and scope for each of these proposed special studies is detailed in Task 4 below.

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## SCOPE OF WORK

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### TASK 1 | PROJECT INITIATION

Upon project initiation, M-Group will facilitate a kick-off meeting with City staff and, as appropriate, the applicant team. At the kick-off meeting project goals will be identified, communication protocols discussed, and the scope of work confirmed including the project schedule, data needs, and technical studies to be prepared by M-Group subconsultants and the applicant's team. As part of this task, M-Group will perform a full review of available information on the project site and vicinity including the Initial Study and Environmental Checklist, prepared by LACO Associates, photo documentation, relevant planning documents (General Plan, Zoning Ordinance and others), history and past uses of the site, and regulations applicable to the project and site.

**Task 1 Deliverables:** *Draft Kick-off Meeting Agenda, contact information, and preliminary schedule. Memorandum summarizing the kickoff meeting and data needs memo identifying additional information needed to fill in any identified gaps, as applicable.*

### TASK 2 | PREPARE PROJECT DESCRIPTION

M-Group will review the project description previously prepared for the Initial Study and Environmental Checklist and will augment this description as needed to ensure all project components are adequately described and documented as the project description will form the basis for analysis. The project description will identify proposed development activities, construction phasing, site conditions, and proposed project operation. The physical and regulatory context of the project site will be documented and project objectives identified. M-Group will prepare a draft project description and will coordinate with the City to confirm adequate scope and detail prior to conducting the environmental analysis.

**Task 2.1 Deliverables:** *One (1) electronic copy of draft project description for City review. Final Project Description for inclusion in the environmental impact report.*

### TASK 3 | NOTICE OF PREPARATION AND SCOPING MEETING.

M-Group will prepare the Notice of Preparation (NOP) in accordance with CEQA Guidelines Section 15082 for the City's review and acceptance. The NOP will describe the project and include an initial study checklist identifying environmental topics to be screened out and those to be analyzed in the EIR. M-Group will deliver a draft NOP and work with the City to circulate a public NOP. M-Group will facilitate the NOP scoping meeting, which will occur during the 30-day comment period for the NOP. M-Group will prepare graphics, slides, and presentation materials for the scoping meeting. M-Group will assist the City in preparing a project mailing list for all noticing and assumes that the City will maintain a list of interested parties, stakeholders and commenting agencies throughout the EIR process. The City will be responsible for circulating the NOP to all appropriate local, State, and Federal agencies

## SCOPE OF WORK

as well as filing the NOP with the County Clerk and posting to the City's website. M-Group can file the NOP with the SCH at the City's request. Following the scoping meeting, M-Group will prepare a scoping summary memo to document comments received. Each comment will be addressed in the EIR and a matrix will be included as an appendix indicating the page number or numbers where each comment is addressed. Please note that comments received on project merits will not be addressed in the EIR as those would be outside the scope of CEQA. Task 3 Deliverables: Electronic files (word and pdf) of the Draft and Final NOP, materials for the scoping meeting, and a scoping summary memo.

**Task 3 Deliverables:** *Electronic files (word and pdf) of the Draft and Final NOP, materials for the scoping meeting, and a scoping summary memo.*

### TASK 4 | TECHNICAL STUDIES

M-Group, in collaboration with our subconsultants, will conduct the following technical studies and tasks to support the analysis in the Draft Environmental Impact Report.

**Task 4 Deliverables:** *Technical Studies to be included as attachments to the Draft EIR.*

#### *Task 4.1 Biological Analysis Peer Review (Monk & Associates)*

Monk & Associates will perform a site visit to verify the presence of waters of the U.S. and State as well as any special-status plant and animal communities. Monk & Associates will peer review the Wetland Report, Biological Review Report, and Initial Study and Environmental Checklist prepared for the project. As part of the peer review process, Monk & Associates will review the California Department of Fish and Wildlife's CDFW's most recent version of the Natural Diversity Data Base (RareFind 5 application) for records of special-status plant and animal species known from the region. Similarly, the California Native Plant Society's (CNPS) electronic update of the Inventory of Rare and Endangered Vascular Plants of California (Fifth Edition) will be consulted for information concerning the potential occurrence of special-status plant species in the area. Based on the results of the peer review, Monk & Associates will prepare targeted recommendations and augments to the studies, as deemed appropriate which will be provided as a technical report to be included as an appendix to the EIR. In addition, Monk & Associates will respond to biological-related comments received during the public review period of the Draft EIR.

#### *Task 4.2 Cultural Resources Assessment Peer Review (Evans & De Shazo)*

A Secretary of Interior qualified Archaeologist will conduct a peer review of the cultural resources study report prepared for the project by Genesis Society on August 15, 2019 to ensure consistency and compliance with CEQA and Section 106 of the National Historic Preservation Act (NHPA), as warranted. The peer review will consist of an analysis of the methods, findings, and recommendations for the Project, and a review of report contents, format, and compliance with CEQA and Section 106 of the NHPA. A Peer Review Letter Report will be provided with a bulleted list of comments and recommendations pertaining

to the contents of the previous cultural resources report, referenced by the associated page number(s), and a summary paragraph that will outline any significant issues identified, and recommendations should the peer review identify deficiencies in the previous study. In addition, a Secretary of Interior qualified Archaeologist will provide written responses to cultural resources-related comments received during the public review period of the Draft EIR.

#### *Task 4.3 Transportation Impact Analysis Peer Review (W-Trans)*

W-Trans will conduct a peer review of the Transportation Impact Analysis and Addendum to ensure they have been prepared consistent with standard traffic engineering practices as well as City and Caltrans policies. The review will include an in-depth analysis of quantitative data to ensure accurate input and analysis. Comments received from Caltrans will be considered and compared to the analyses to ensure comments are addressed and any recommendations are incorporated. W-Trans will prepare a draft letter report describing details of the peer review as well as any additional findings and recommendations. In addition, W-Trans will also provide assistance in responding to transportation-related comments received during the public review period of the Draft EIR.

#### *Task 4.4 Air Quality and Noise Assessments (Illingworth & Rodkin)*

Illingworth & Rodkin will prepare Air Quality and Acoustical analyses to evaluate potential impacts resulting from temporary project construction activities as well as ongoing operation of the proposed project. The Acoustical Analysis will involve quantifying the existing ambient noise environment through a noise monitoring survey, calculation of construction noise and vibration levels, operational noise levels, assessment of potential impacts, and will conclude with recommended mitigation measures. The Air Quality and Greenhouse Gas Assessment will involve quantification of construction and operational emissions which will inform recommended mitigation measures to reduce potential impacts. Given the proximity of nearby sensitive receptors, a qualitative discussion of health risks associated with project construction will also be provided, and any necessary mitigation measures will be incorporated.

#### *Task 4.5 Review Technical Studies (M-Group)*

M-Group will review all tasks and technical studies prepared for the project to ensure that analyses are adequate for CEQA purposes, that conclusions rely on sound assumptions, and that appropriate methodology is utilized. As needed, M-Group will provide recommendations to ensure that all technical studies are clear and accurate, and that conclusions can be substantiated. M-Group will integrate results of the technical studies into the CEQA analysis.

### **TASK 5 | PREPARE ADMINISTRATIVE DRAFT ENVIRONMENTAL IMPACT REPORT**

Following release of the NOP, M-Group will initiate preparation of an Administrative Draft EIR (ADEIR). The Administrative Drafts of the DEIR will identify project objectives, evaluate the

## SCOPE OF WORK

potentially significant adverse effects of the project, and analyze feasible alternatives. The DEIR will include the following items:

- Introduction with graphics and detailed project description
- Executive Summary
- Environmental Setting/Context
- Introduction with graphics and detailed project description
- Environmental Setting/Context
- Environmental Evaluation
- Cumulative Impacts
- Project Alternatives
- Significant and Unavoidable Impacts (as applicable)
- Bibliography and References
- Appendices
- Mitigation Monitoring and Reporting Program (MMRP)

The Administrative Draft EIR will be provided electronically to the City for review and comment. Following receipt of City comments, M-Group will prepare a Screencheck Draft EIR for City review and concurrence prior to publication.

**Task 5 Deliverables:** *The Administrative DEIR and Screencheck DEIR for City review will be provided electronically.*

### **TASK 6 | PREPARE AND CIRCULATE DRAFT ENVIRONMENTAL IMPACT REPORT**

Upon confirmation that all comments on the Screencheck Draft EIR have been addressed to the City's satisfaction, the Public Review Draft DEIR will be assembled and released for at least a 30-day public comment period (45-days if a State Agency approval or permit is required for the project). M-Group will also prepare CEQA notices including a Notice of Availability and Notice of Completion. It is understood that the City of Fort Bragg will complete all local postings, mailings, and publications.

**Task 6 Deliverables:** *An electronic copy of the Public Review Draft DEIR, all references, resources and materials cited will be provided to the City. M-Group will prepare a draft CEQA Notice of Completion/Notice of Availability for the City's use. The City will be responsible for filing notices with the County Clerk. M-Group can assist with filing with the State Clearinghouse if needed.*

### **TASK 7 | PREPARE DRAFT RESPONSE TO COMMENTS, FINAL ENVIRONMENTAL IMPACT REPORT, AND MITIGATION MONITORING & REPORTING PROGRAM**

Following the close of the public review and comment period on the DEIR, M-Group will review the public comments and prepare a draft response to the comments for City review. The City will collect and forward a single set of all comments to M-Group. Our sub-consultant team members have built time into their scopes of work to help respond to comments.

M-Group will deliver the draft response to comments to the City for review. The City will provide M-Group with a comprehensive redline of staff's comments on the administrative draft. Depending on the type of comments received, a Master Response approach may be employed. This scope assumes five comment letters of normal detail (2-3 pages in length). Comments in excess of these assumptions will be considered outside of this scope of work and cost estimate and an augment will be required. This scope assumes that no new technical analyses or field work will be necessary to respond to comments.

M-Group will provide the City with an Administrative FEIR, including the Draft MMRP for review and comment. A Screencheck FEIR will be prepared addressing City comments. Once comments on the Administrative FEIR have been addressed to the City's satisfaction, M-Group will prepare the Public FEIR for circulation. The City will provide the Final EIR to all responding public agencies prior to public hearings.

**Task 7 Deliverables:** *Electronic versions (Word and PDF) of the response to comments document, electronic file containing all comment letters, and any additional references cited. The Administrative, Screencheck FEIR, and Public FEIR will be provided electronically.*

### TASK 8 | PREPARE FINDINGS OF FACT AND STATE OF OVERRIDING CONSIDERATIONS

M-Group will provide draft findings of fact for use by the City to support certification of the EIR. If the EIR concludes that project impacts cannot be mitigated to levels below significance, and would result in a significant and unavoidable impact, M-Group will also prepare a statement of overriding considerations.

**Task 8 Deliverables:** *Electronic versions (Word and PDF) of findings and statement of overriding considerations (as warranted).*

### TASK 9 | PUBLIC HEARINGS AND CEQA NOTICES

Our scope provides for the Principal-In-Charge and/or Project Manager to attend up to five public hearings (the scoping meeting, two Planning Commission meetings, and two City Council meetings), to assist staff with presentations, and respond to environmental questions. Additional public meetings will be charged on an hourly basis as needed. M-Group will provide support to planning staff for reports, presentations, and materials presented to the public and decision makers relating to the environmental review and findings. Following certification of the Final EIR, M-Group will assist the City in preparing a Notice of Determination (NOD). The City will be responsible for filing with the County Clerk, M-Group can file to the SCH if needed.

**Task 9 Deliverables:** *Environmental materials to support staff reports and public hearings; and Draft Notice of Determination for filing with the County Clerk and SCH.*

### TASK 10 | PROJECT MANAGEMENT AND COORDINATION

M-Group's Project Manager will coordinate the activities of M-Group and maintain open lines of communication with City staff throughout the process. M-Group will also coordinate the

## SCOPE OF WORK

activities of our subconsultants. This task includes bi-weekly meetings with City staff as well as monthly progress reports documenting complete and upcoming tasks.

**Task 10 Deliverables:** *Bi-weekly (as needed) conference calls to discuss data collection, methodological approaches, potential impacts, and proposed mitigation measures.*

### PROJECT ASSUMPTIONS

1. M-Group will coordinate with the City to ensure that any records and past reports relevant to the subject site including previous environmental review documents and any technical reports, etc., are obtained and referenced.
2. M-Group will coordinate directly with appropriate City departments and/or through the Community Development Department to ensure that any considerations associated with emergency vehicle access, police protection, and infrastructure capacity are incorporated into the environmental review.
3. M-Group's team will be granted access to the project site in timely manner to carry out necessary fieldwork and data collection.
4. The scope of work assumes that no technical analysis beyond those recommended herein will be warranted. If during the course of analyses, it is determined that further technical reports are needed, a budget adjustment will be required.
5. M-Group will coordinate directly with City staff to ensure effective and seamless review of deliverables throughout preparation, drafting and finalization.
6. City staff will provide comments on administrative draft deliverables in one consolidated document using the track changes function in word.
7. City staff will authorize M-Group to submit the documents to the State Clearinghouse.
8. City staff will assemble and provide M-Group with all comments received during the public comment period.
9. M-Group will provide draft and final materials electronically to the City.
10. Public hearings will be held remotely, if meetings are to be held in person, a budget augment will be required to account for travel time.
11. Staff will prepare and present staff report and findings pertaining to planning entitlements, M-group to provide support related to environmental component of the project.
12. Invoices will be provided monthly and be based upon the percentage of task completion.
13. Unexpected issues out of scope such as project delays or extended timelines out of the control of M-Group may necessitate a scope and budget augment.
14. Acquisition of any required regulatory agency permits, approvals, or certification is not provided for in this scope of work.



# F. BUDGET

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## PROJECT BUDGET

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Based on our knowledge of the project we anticipate a not-to-exceed budget of **\$178,020** to cover the tasks outlined above (including a project contingency that will require City approval to utilize). These project costs do not include the Fish and Wildlife CEQA Filing Fee. A detailed budget project is provided in **Appendix B**.

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## SCHEDULE OF CHARGES

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M-Group's hourly billing rates are inclusive of personnel time, administrative overhead, and incidental printing costs. M-Group proposes Fixed-Fee contracts with monthly invoicing on a percentage task completion basis.

M-Group's 2022 Hourly Rate Sheet is provided in **Appendix C**.

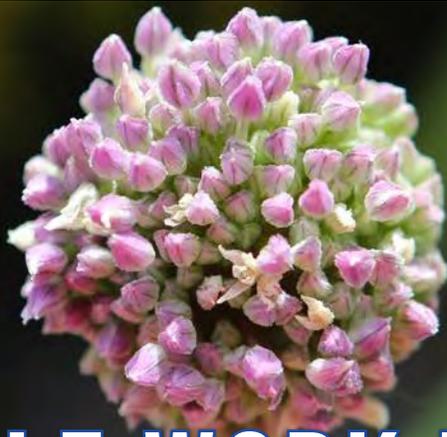


# G. WORK SCHEDULE

## PROPOSED TIMELINE

The following conceptual timeline indicates releasing the Public Review Draft of the EIR approximately five months after project initiation. M-Group has dedicated sufficient staff resources to complete the environmental review as expeditiously as possible. The following project schedule presumes that environmental review will commence once a sufficient level of project detail is developed to adequately evaluate potential environmental impacts and that comments on administrative draft documents will be provided in a timely manner to allow adequate time for M-Group to address substantive comments. At project kickoff, specific target dates will be established in close coordination with City staff.

TASKS	Months								
	1	2	3	4	5	6	7	8	9
1 Kick Off Meeting	█								
2. Prepare Project Description	█	█							
3 NOP/Scoping Meeting		█	█						
<b>Scoping Meeting</b>		█							
4. Prepare Technical Studies	█	█	█						
5 Prepare Admin Drafts DEIR		█	█	█					
City Review			█	█	█				
Prepare Public Review DEIR/ Notices				█	█				
<b>Public Review and Comment Period</b>					█	█	█		
6. Prepare Final EIR							█	█	
City Review								█	█
Finalize Final EIR								█	█
7. Prepare CEQA Findings & Notices									█
8. Public Hearings									█
9. Project Management	█	█	█	█	█	█	█	█	█



# H. SAMPLE WORK PRODUCT

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## SAMPLE WORK PRODUCT

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### **CITY OF HEALDSBURG** | *544 TUCKER STREET EIR*

M-Group was retained by the City of Healdsburg, in April 2018 to prepare an Environmental Impact Report (EIR) to analyze impacts associated with the demolition of a historic resource. The subject resource — a c. 1872 Greek Revival residence— had previously been identified as a contributor to the Tucker Street Historic District, which was determined eligible for listing on the California Register of Historic Resources. Eligibility of the 68-property district was based on its place in the history of Healdsburg’s urban development and because it conveys an excellent representation of residential architectural styles from Healdsburg’s founding through the present.

M-Group facilitated a scoping meeting and integrated input received into the EIR. M-Group prepared an EIR that disclosed and analyzed the project’s potential to adversely affect the integrity of an identified historic resource (i.e. the eligible Tucker Street District). The EIR identified mitigation measures including photographic documentation prior to demolition, implementation of a salvage plan, erection of a plaque detailing the history of the home, and development of a self-guided walking tour booklet for the Tucker Street Historic District.

The Draft EIR was circulated for public review and was considered by the Planning Commission for adequacy. All comments received were assembled and responded to in the Final EIR. M-Group prepared the findings of fact, statement of overriding considerations, completed the staff report for the demolition application, and presented the project and CEQA findings at public hearings. The City of Healdsburg certified the EIR, adopted the MMRP and statement of overriding considerations and approved the project in August 2018.

Links to documents are provided here as well as on a thumb drive included with the proposal.

[544 Tucker Street DEIR](#)

[544 Tucker Street FEIR](#)



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## ACCEPTANCE OF INSURANCE REQUIREMENTS

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M-Group fully accepts the City's insurance requirement in accordance with Attachment 2 of the RFP.



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## ACCEPTANCE OF TERMS OF CONSULTANT AGREEMENT

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M-Group fully accepts the content, terms and conditions detailed in the City's standard consultant services agreement as stated in RFP Attachment 2.

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# APPENDIX A

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## RESUMES

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**OLIVIA ERVIN**

**KRYSTLE RIZZI**

**ALAINA LIPP**

# OLIVIA ERVIN

## PRINCIPAL



### EXPERIENCE

#### M-Group

Principal  
2021 – Present

Principal Environmental  
Planner  
2017 – 2021

Environmental Planner  
2013 – 2016

#### Terra Nova Planning & Research

Senior Planner  
2009 – 2013  
Napa, CA

#### Terra Nova Planning & Research

Associate Planner  
2007 – 2009  
Palm Springs, CA

### EDUCATION

#### UC Davis Extensions

Planning and Environmental  
Law

#### Leadership Napa Valley

Napa, CA

#### Bachelor of Science Environmental Resource Science

UC Davis  
Davis, CA

### MEMBERSHIPS

American Planning  
Association (APA)  
Association of Environmental  
Planners (AEP)

*Olivia has over 14 years of experience with environmental compliance and land use planning. She provides lead agencies with environmental review services including project management, planning, and consulting. Her direct approach, organization system, and familiarity with the environmental review process make her an effective project manager. She is knowledgeable with technical aspects of CEQA, standards of review, and acceptable modeling tools. Her background includes a mix of development review, long range planning and municipal projects.*

### AREAS OF EXPERTISE

**CEQA & Environmental Review**  
**Project Management**  
**Technical Analysis**  
**Land Use Planning**  
**Written & Oral Communications**

### ENVIRONMENTAL + PLANNING

#### ON-CALL PLANNING SERVICES | CITY OF PETALUMA, CA

Serving as the City's contract environmental planner since 2013 conducting environmental review for a variety of development, legislative, and municipal projects. Manage administrative records, prepare staff reports, present CEQA findings to the public and decision makers at hearings for numerous projects involving all levels of environmental review. Planning staff liaison to other City departments to complete CEQA review for municipal projects including completion of a citywide creeks maintenance manual and corresponding CEQA document. Staff lead on advancing City's guidelines for SB 743 compliance.

#### ON-CALL PLANNING SERVICES | CITY OF SANTA ROSA, CA

Since 2016 M-Group has been on the City of Santa Rosa's on-call list for planning and environmental services. Serving as project manager for environmental review on a number of development review projects and completed CEQA analysis for the City's supplemental density bonus ordinance. Primary considerations include land use compatibility related to regional parks, open space and agricultural lands, hillside development, connectivity and expansion of bicycle and pedestrian facilities, infill development and infrastructure improvements.

#### ON-CALL PLANNING SERVICES | CITY OF COTATI, CA

Since 2016 M-Group has been providing on-call planning and environmental review services to the City of Cotati. Assists the City with CEQA compliance on a variety of development review projects, zoning code updates including the cannabis ordinance, and municipal projects. Project manager and primary point of contact for the City providing oversight of environmental services. Lead author on CEQA documentation for mixed-use development, subdivisions, and assisted living facility. Supports City staff presentation to decision makers and responding to comments.

#### ON-CALL PLANNING SERVICES | CITY OF CALISTOGA, CA

Since 2017 M-Group has been providing environmental review services to the City of Calistoga. Project manager and lead CEQA author for gas station and restaurant project, hotel and retail development, and expansion of industrial facility. Primary point of contact for the City providing oversight of environmental services. Supports City staff with staff reports, findings and resolutions and presentations at public hearings.

# OLIVIA ERVIN

## PRINCIPAL

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### **MAIN STREET STREETScape IMPROVEMENTS** | *CITY OF ST. HELENA, CA*

Part of project team providing design, landscaping, and environmental review services to the City of St. Helena for pedestrian improvements to Main Street/Highway 29. Overseeing preparation of the Area of Potential Effect and Historic and Archeological Studies pursuant to Caltrans standards and Section 106 consultation. Providing environmental services for CEQA determination and documentation to inform Caltrans' Preliminary Environmental Study and NEPA determination.

### **AIRPORT AREA SPECIFIC PLAN + EIR** | *PERMIT SONOMA, CA*

Part of project team preparing Specific Plan and EIR for an update to the industrial planning area east of the Airport including a SMART station. The Plan intensifies uses and introduces a variety of residential land uses to the planning area. Prepared a water demand estimate to inform the water supply study, which was approved by the Town of Windsor, as the water provider for the area. Coordinating with County staff, project team, Town, and stakeholders through plan development and environmental review. Managing air quality and transportation analyses. The Specific Plan and EIR are currently in process.

### **JAGUAR WAY EXTENSION** | *TOWN OF WINDSOR, CA*

For the Town's Public Works Department, lead environmental review services for Jaguar Way Extension (0.5 mile roadway including bridge over Starr Creek providing connectivity between Starr Road and Windsor Drive). Assessed various design options for multi-modal access, evaluated environmental constraints, and prepared IS/MND. Assisted Town in fulfilling lead agency obligations, prepared notices and staff report. Prepared and circulated response to comments addressing California Department of Fish and Wildlife concerns and public comment letters. Presented staff report and findings at Council hearing.

### **RAINIER CROSS TOWN CONNECTOR EIR** | *CITY OF PETALUMA, CA*

Managed the environmental review process for the Rainier Cross Town Connector Project; a 0.65 mile 4 lane arterial, including a bridge over the Petaluma River and SMART corridor and an undercrossing of Highway 101. Oversaw a team of consultants and carried out interagency coordination. Prepared public notices, staff reports, and managed the administrative record. Authored findings, statement of overriding considerations, and resolutions. Presented to decision makers and provided response to comments during public review.

### **SAN RAFAEL GENERAL PLAN UPDATE AND ADDENDUM** | *CITY OF SAN RAFAEL, CA*

Conducted targeted review and updated General Plan goals, policies and programs while making adjustments that respond to changes that had occurred in the decade following adoption of the General Plan. Prepared an Addendum to the San Rafael General Plan EIR for focused General Plan amendments and provided support through the public review and approval process.

### **ADDENDUM TO GENERAL PLAN EIR HOUSING ELEMENT** | *CITY OF SAN RAFAEL, CA*

Completed an Addendum to the San Rafael General Plan EIR to incorporate the 2015-2023 Housing Element in the City's General Plan. Coordinated with housing specialists, reviewed housing inventory records, and researched demographic data. Prepared project description, characterized the regulatory setting and conducted an environmental analysis examining impacts of the Housing Element relative to what was analyzed in the General Plan EIR. Presented findings at public hearing and responded to comments.

# KRYSTLE RIZZI

## SENIOR PLANNER



### EXPERIENCE

#### M-Group

Senior Planner  
2021 – Present

Associate Planner  
2019 - 2021

Assistant Planner  
2017-2019

#### Apple Inc. via Apex Systems

GIS Technician  
2016-2017  
Sunnyvale, Calif.

#### San Jose State University

Research Assistant  
2016  
San Jose, Calif.

#### City of San Jose

Recreation Leader, Walk n' Roll  
2015-2016  
San Jose, Calif.

### EDUCATION

#### Master of Urban Planning

San Jose State University  
San Jose, Calif.

#### Bachelor of Arts in Environmental Studies and Planning

Sonoma State University  
Rohnert Park, Calif.

*Krystle is an experienced planner with expertise in environmental and development review and project management. Additionally, Krystle has a background in GIS, transportation planning, community outreach, and research. Krystle is a highly skilled writer and has developed expertise in reviewing and summarizing technical studies to support environmental analyses consistent with the California Environmental Quality Act (CEQA). Krystle demonstrates strong organizational, communication, and analytical skills.*

### AREAS OF EXPERTISE

**Environmental Review**  
**Community Engagement**  
**Staffing Solutions**  
**Policy Planning**  
**Geographic Information Systems (GIS)**

### ENVIRONMENTAL REVIEW

#### HEARN VETERANS VILLAGE | SANTA ROSA, CALIF.

Prepared an Addendum to the 2016 Roseland Specific Plan and Annexation EIR, documenting that the project would not result in any new or more severe impacts beyond those previously identified. The Addendum characterized the regulatory context, summarized the impact determinations of the Specific Plan EIR, and evaluated the project and conditions relative to the CEQA Guidelines Section 15162. Findings were presented in accordance with CEQA Guidelines Section 15164(e). Entitlements requested for the project included a Tentative Parcel Map, though additional building detail information was also provided to allow for a more thorough analysis of potential environmental impacts associated with construction of four single-family structures and four accessory dwelling units to be occupied by up to 32 veteran residents. In December 2021, the Planning Commission unanimously adopted the Addendum to the 2016 Roseland Specific Plan and Annexation EIR and approved the Tentative Parcel Map.

#### 38 DEGREES NORTH ADDENDUM | SANTA ROSA, CALIF.

Prepared an Addendum to the previously adopted Initial Study/Mitigated Negative Declaration (IS/MND) for the 38 Degrees North Phase 2 project which authorized construction of eight three-story buildings containing 172 residential units and resident amenities, preservation of a portion of the site for open space, and reservation of a portion of the site for future construction of an approximately 21,000 square foot community shopping center. The revised project, referred to as 38 Degrees North Phase 3, removed the previously conceptualized community shopping center, and instead proposed 30 additional residential units in two buildings. As part of the project, entitlements for Design Review, General Plan map and text amendments, and a Zoning Map amendment were requested by the applicant. Given that the Phase 2 IS/MND previously analyzed physical impacts of future construction of the community shopping center, the Addendum focused on resource areas that required an update to address the revised project and any changes to the environmental setting, impacts, and mitigation measures that may have occurred. Specifically, the Addendum included a discussion of Aesthetics, Air Quality, Greenhouse Gases, Land Use and Planning, Noise, Population and Housing, and Transportation. The Addendum concluded that the project would not result in any new or more severe impacts beyond those that were previously identified in the Phase 2 IS/MND. The Planning Commission recommended adoption of the

# KRYSTLE RIZZI

## SENIOR PLANNER

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Addendum and approval of the General Plan amendments, and in December 2021 the City Council adopted the Addendum and approved the General Plan amendments for the project.

### **CASA GRANDE RESIDENTIAL PROJECT** | *PETALUMA, CALIF.*

Prepared an Initial Study/Mitigated Negative Declaration analyzing demolition of existing single-family homes and other structures onsite to develop a 36 residential lot subdivision with two common lots containing bioretention basins and a public right-of-way dedication. The project required rezoning, a vesting tentative map, and Site Plan and Architectural Review. Reviewed applicant-prepared technical studies for adequacy under CEQA including analysis of proposed mitigation measures. The project was reviewed by the City Council and the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program was approved in December 2020.

### **WUSD GENERAL PLAN AMENDMENT AND REZONE** | *WILLITS, CALIF.*

Prepared an Initial Study/Mitigated Negative Declaration (IS/MND) analyzing a conceptual multi-family residential development at the highest permitted density, which could ultimately be facilitated by the General Plan Amendment and Rezone Project. CEQA review services included peer review of a traffic impact study and archaeological survey report. Worked closely with the traffic consultant to ensure potential impacts with regard to VMT were adequately addressed, which included preparation of a Transportation Demand Management (TDM) plan. Through the review process, identified potential land use adjacency conflicts as a result of permitting a residential use adjacent to existing industrial uses and in accordance with the City's guiding policies, established a land use mitigation measure which established a buffer between the existing industrial use and a future residential development on the site. Also assisted the City in responding to comments on the IS/MND and the project merits, prepared the City Council staff report and resolution. On August 26, 2020, the Council approved the project entitlements, General Plan Amendment and Rezone, and the IS/MND.

### **CALISTOGA LOOP GAS STATION** | *CALISTOGA, CALIF.*

Assisted in the preparation of an Initial Study/Mitigated Negative Declaration for a proposed gas station, car wash, convenience store, and sit-down restaurant at the corner of Foothill Boulevard and Petrified Forest Road. Assisted in Air Quality and GHG analysis and conducted review using the California Emissions Estimator Model (CalEEMod).

### **ADOBE ROAD WINERY** | *PETALUMA, CALIF.*

Assisted in the preparation of a Class 32 infill exemption justification for a winery and tasting room along the Petaluma River in an existing urban area in downtown Petaluma. Justification included the analysis and incorporation of findings from technical studies for traffic, noise, and water quality.

### **131 LIBERTY MIXED-USE PROJECT** | *PETALUMA, CALIF.*

Assisted in the preparation of a Class 32 infill exemption justification for a mixed-use project with commercial, office and residential components. In addition to preparing the exemption, assisted with research for historic review.



# ALAINA LIPP

## ASSOCIATE PLANNER

### EXPERIENCE

#### M-Group

Associate Planner  
Feb 2022 – Present

#### City of Sausalito

Assistant Planner  
Apr 2020 – Feb 2022  
Sausalito, Calif.

#### San Francisco Public Utilities Commission

Watershed Planner  
Jun 2018 – Apr 2020  
San Francisco, Calif.

#### Bureau of Land Management

GS-05 Range & Fuels Technician  
May 2014 – Sep 2014  
Pocatello, Idaho

### EDUCATION

#### Master of Landscape Architecture and Environmental Planning

University of California at Berkeley  
Berkeley, Calif.

#### B.S. - Biology

California State University at Channel Islands  
Camarillo, Calif.

B.A. – English

University California at Santa Barbara  
Santa Barbara, Calif.

*Alaina Lipp is a motivated and evolving planner. She has professional, educational, and volunteer background in environmental planning complemented by a strong foundation of policy planning, development review, and grant writing experience. Alaina is an enthusiastic planning professional with strong leadership and organizational skills. She is self-directed yet thrives in collaborative environments and is quick to adapt to dynamic situations.*

### AREAS OF EXPERTISE

**Environmental Review**

**Ecological Restoration**

**Wastewater + Stormwater Management**

**Land Use Planning**

**Development Review**

**Grant Writing**

### PROFESSIONAL EXPERIENCE

#### ASSOCIATE PLANNER | M-GROUP

Process planning applications, review and analyze land use impacts and compliance with local and state regulations and city policies. Prepare Initial Studies and ordinances. Communicate with applicants, consultants, city staff and the public. Conduct research and analyze collected data. Prepare technical and administrative reports. Make presentations to Planning Commission, City Council and at public hearings and community meetings. Prepare and maintain maps pertaining to zoning, land use and other planning related activities.

#### ASSISTANT PLANNER | CITY OF SAUSALITO, CALIF.

Plan review and permitting coordination for residential and commercial construction, accessory dwelling units, tree removal permits, lot line adjustments, and sign permits. Developed applications and informational materials, managed website content. Project managed the city's SB 1383 compliance activities and ordinance. Presented to Planning Commission, City Council, and Historic Preservation Commission as needed. Staff liaison to the Sustainability Commission and interfaced with city departments to achieve the objectives of the commission. Wrote ordinances and resolutions, presented them to the appropriate approving body, and guided public outreach and engagement. Project managed and co-wrote grants for the Zero Waste JPA grant and HCD LEAP grant.

#### WATERSHED PLANNER | SAN FRANCISCO PUBLIC UTILITIES COMMISSION

Reviewed plan submittals for content and completeness and coordinated plan review with civil engineers and landscape architects. Managed submittal tracking system, generated reports on program activities and created interpretive graphics to report to both specialist and public audiences. Collaborated with PUC staff to execute strategies to improve efficiency and outcomes of the Stormwater Management Ordinance program. Presented to diverse community groups at outreach events. Collaborated with other city agencies working on synergistic programs such as the Better Roofs Ordinance, the Non-Potable Ordinance, and Better Streets Ordinance.

# ALAINA LIPP

## ASSOCIATE PLANNER

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### PROFESSIONAL AFFILIATIONS

American Planning Association  
California

American Society of Landscape  
Architects

Golden Gate National Recreation  
Area (GGNRA) Volunteer

Watershed Nursery Volunteer

**GS-05 RANGE & FUELS TECHNICIAN** | *BUREAU OF LAND MANAGEMENT,  
POCATELLO, IDAHO*

Monitored risk indicators in Wildland Urban Interface (WUI) zones. Performed monitoring, analysis, and reporting for restoration and conservation objectives of Environmental Assessment (EA). Surveyed academic research on threatened species for renewal of EA for NEPA compliance. Mapped invasive species throughout the project areas and applied treatments where appropriate.

# APPENDIX B

## BUDGET

### City of Fort Bragg Grocery Outlet

Task Number / Description	M-Group Hours	Task Subtotals
<b>TASK 1: PROJECT INITIATION</b>		
Kick-Off Meeting + Background Research	6	\$1,070
<b>Task 1 Subtotal:</b>	<b>6</b>	<b>\$1,070</b>
<b>TASK 2: PREPARE PROJECT DESCRIPTION</b>		
Prepare Project Description	30	\$4,490
<b>Task 2 Subtotal:</b>	<b>30</b>	<b>\$4,490</b>
<b>TASK 3: NOTICE OF PREPARATION &amp; SCOPING MEETING</b>		
Prepare Notice of Preparation	50	\$7,280
Prepare for and Attend Public Scoping Meeting	24	\$3,660
<b>Task 3 Subtotal:</b>	<b>74</b>	<b>\$10,940</b>
<b>TASK 4: TECHNICAL STUDIES</b>		
Review Draft Technical Studies (Subtask 4.5)	30	\$4,370
<b>Task 4 Subtotal:</b>	<b>30</b>	<b>\$4,370</b>
<b>TASK 5: PREPARE ADMINISTRATIVE DRAFT EIR</b>		
<b>Task 5 Subtotal:</b>	<b>333</b>	<b>\$48,995</b>
<b>TASK 6: PREPARE AND CIRCULATE DRAFT ENVIRONMENTAL IMPACT REPORT</b>		
Prepare Public Review Draft EIR	42	\$6,030
Prepare Notices and Circulate to the State Clearinghouse	9	\$1,175
Attend PC Public Review and Comment Meeting on Draft EIR	0	\$0
<b>Task 6 Subtotal:</b>	<b>51</b>	<b>\$7,205</b>
<b>TASK 7: PREPARE RESPONSES TO COMMENTS, FINAL EIR, AND MMRP</b>		
Catalog Comment Letters	28	\$3,870
Prepare Administrative Draft Final EIR	42	\$6,030
Prepare MMRP	16	\$2,200
<b>Task 7 Subtotal:</b>	<b>86</b>	<b>\$12,100</b>
<b>TASK 8: PREPARE FINDINGS OF FACT AND STATEMENT OF OVERRIDING CONSIDERATIONS</b>		
Prepare Final EIR	26	\$3,860
Prepare Findings of Fact	26	\$3,920
Prepare Statement of Overriding Considerations	19	\$2,865
<b>Task 8 Subtotal:</b>	<b>71</b>	<b>\$10,645</b>
<b>TASK 9: PUBLIC HEARINGS AND CEQA NOTICES</b>		
Prepare for and Attend Planning Commission Meeting (2)	22	\$3,830
Prepare for and Attend City Council Meeting (2)	18	\$3,210
File the Notice of Determination	4	\$510
<b>Task 9 Subtotal:</b>	<b>44</b>	<b>\$7,550</b>
<b>TASK 10: PROJECT MANAGEMENT AND COORDINATION</b>		
Management and Coordination	60	\$10,000
<b>Task 10 Subtotal:</b>	<b>60</b>	<b>\$10,000</b>
<i>Project Subtotal (hours + budget)</i>	785	\$117,365
<i>Direct Costs (Supplies, Printing, Mailing and Mileage)</i>		\$2,905
<i>10% Contingency Costs</i>		\$11,740
<b>M-Group Total</b>		<b>\$132,010</b>
<b>SUBCONSULTANT TECHNICAL STUDIES (Includes M-Group Administration Fee)</b>		<b>Total Cost</b>
Task 4.1	Monk & Associates	\$15,435
Task 4.2	Evans & DeShazo	\$3,775
Task 4.3	W-Trans	\$12,500
Task 4.4	Illingworth & Rodkin	\$14,300
<b>Subconsultant Subtotal:</b>		<b>\$46,010</b>
<b>Project Total</b>		<b>\$178,020</b>

#### NOTES

- 1 M-Group reserves the right to re-allocate hours within M-Group to complete the tasks, as needed.
- 2 Cost Proposal is for a Not-to-exceed Contract with monthly invoicing based on task completion.
- 3 Travel time and expenses have been factored into the budget.
- 4 Use of the Contingency will require City approval.

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# APPENDIX C

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## 2022 RATE SHEET

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M-GROUP STAFF	HOURLY RATE
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Admin Analyst Planning Tech	\$85
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Assistant Planner Assistant Urban Designer Social Media Coordinator	\$100
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Associate Planner Associate Urban Designer GIS Mapping Services	\$130
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Environmental Planner Historic Preservation Specialist Public Art Specialist	\$145
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Senior Planner Senior Urban Designer Senior Environmental Planner Project Manager	\$155
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Principal Planner Principal Environmental Planner Principal Policy Planner Director of Urban Design	\$175
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Principal	\$220 - \$270
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NOTES:

- \* Hourly rates are subject to annual adjustment.
- \* Sub-consultants include a 10% administration fee

**From:** [Mike Schwerin](#)  
**To:** [Lemos, June](#)  
**Cc:** [Patrick Britton](#); [Robert Edgerton](#); [Joanne Dramko](#)  
**Subject:** April 25 City Council Meeting - Grocery Outlet CEQA  
**Date:** Thursday, April 21, 2022 11:18:24 AM  
**Attachments:** [25April2022\\_CouncilMeeting\\_GroceryOutletCEQAConsultant.pdf](#)

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Ms. Lemos,

I am respectfully submitting the attached letter for the City Council's consideration at its April 25, 2022 meeting.

Please let me know if you have any questions regarding the attached letter.

Respectfully,

**Michael Schwerin**  
*Chief Executive Officer*

**HELIX Environmental Planning, Inc.**  
7578 El Cajon Boulevard  
La Mesa, CA 91942  
619.462.1515 tel  
619.462.0552 fax  
[MikeS@helixepi.com](mailto:MikeS@helixepi.com)  
[helixepi.com](http://helixepi.com) | [LinkedIn](#) | [Facebook](#) | [Twitter](#)

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April 21, 2022

June Lemos, MMC, City Clerk  
City of Fort Bragg  
416 North Franklin Street  
Fort Bragg, CA 95437

**Subject: Proposal for an Environmental Impact Report Pursuant to the California Environmental Quality Act for the Proposed Grocery Outlet**

Dear Ms. Lemos:

We understand that the City of Fort Bragg (City) has preliminary selected De Novo Planning Group (De Novo) to prepare the California Environmental Quality Act documentation for the proposed Grocery Outlet project, and that an approval of an agreement with De Novo for this work is on the April 25 City Council meeting agenda.

We have reviewed the proposal prepared by De Novo, as included in the City Council meeting packet posted on the City's website. Although the proposal appears to be largely sound in its approach and scope (despite its multiple references to the wrong Lead Agency), the labor and cost estimates provided in the proposal are blatantly unrealistic.

Specifically we refer to Tasks B, C, and E, in which De Novo has indicated that it will prepare a Project Description, Initial Study, and full and complete administrative draft Environmental Impact Report, and also conduct a public scoping meeting, using only 21 hours of staff time (at a cost of \$3,037.50). These are tasks that typically require dozens, if not hundreds, of labor hours to complete.

De Novo's representation that it can complete these tasks within 21 hours is impossible to accept at face value. One possible explanation would be if De Novo has already completed a substantial portion of this work under contract to a different party (i.e., not the City) and is merely planning to "touch up" the work that it has already completed. I want to make clear that HELIX has no evidence of any such third-party contracts; we are merely trying to understand how a firm could commit to completing so much work within 21 hours.

To that end, we respectfully request that at the City Council meeting, there be a full and open disclosure of whether De Novo has completed, or is under separate contract to prepare, any CEQA compliance work for the proposed Grocery Outlet Project. If any such arrangement exists, we strongly feel that should be publicly disclosed before the City Council makes its final decision in this matter. If no such third-party agreement(s) exists, we think that it would be in the public interest for De Novo to disclose how it will be able to complete an Initial Study, NOP, and Administrative Draft EIR using only 21 hours of staff labor.

I would like to close by thanking the City for the opportunity to prepare a thoughtful and appropriately costed proposal for this important project.

Sincerely,



Michael Schwerin  
Chief Executive Officer

**From: Leslie Kashiwada**  
**To: Fort Bragg City Council**  
**Re: Agenda Item 8B on April 25, 2022**

On Feb 28, 2022, I submitted a comment (see below) about the decision by Best Development Group (BDG) to have the City Council vacate their approval of the MND and building permit for a Grocery Outlet Bargain Market (GOBM) on the southwest corner of the intersection of S. Franklin St and South St. They made this request to make way for an EIR, which is the CEQA document that should have been prepared from the start.

I expressed my concern that the EIR would merely be a repackaging of the MND. My concerns are even greater now viewing the proposed contract with De Novo Planning Group (DNPG) to prepare the EIR. The scope of work indicates that they plan to use some of the previously prepared studies (e.g., traffic and biological study, which are incomplete and dated) instead of running new studies. Given all the special conditions the Planning Commission put on the MND, it is clear that these, and other studies, need to be redone. In particular, the traffic control around the intersection of S. Franklin St. and South St. must be fully analyzed, including pedestrian safety from housing units within walking distance of the project along all potential routes to the entrance of the store. In addition, redesign of the building footprint (assuming the existing building cannot be reused) and layout of the parking lot is essential. The biological studies were fatally flawed in design and execution and must be redone. These are just a few examples of the deficiencies of the existing studies and analysis.

I am especially concerned that the bid for this contract is ridiculously low for the quantity and quality of the work that must be done to assure that significant impacts are analyzed and multiple alternatives are proposed for mitigation. In addition, It is also imperative that a consulting company based in El Dorado Hills, CA shows that it can be sensitive to and responsive about the location-specific concerns of building a GOBM in Fort Bragg, a small rural community in a remote location with a very special community environment.

Please see my previous comments below for a more detailed description of essential requirements for a full, complete, and up to date EIR for this project.

**From: Leslie Kashiwada**  
**Re: Agenda Item 7A on Feb 28, 2022**

I was intrigued to find out that Best Development Group (BDG) requested the City vacate their prior approvals for the Grocery Outlet Project on South Franklin Street. I was particularly amused to read the letter from BDG, where they expressed the conviction that they could mount a “spirited legal defense of the MND” given the small size of the project and its minimal environmental effects.

It’s important to remember that this project is not small in relation to our community, and that significant impacts were found (and that other impacts were ignored). An EIR is not just a matter of adding a few paragraphs to a pre-existing MND. All the prior concerns of the public and all the mitigations imposed by the Planning Commission must be addressed. Multiple alternatives must be provided with more than just a perfunctory statement that a given alternative is not viable. It is perfectly appropriate to state that the cost of a given alternative might be too high to make the project financially viable, but that does not invalidate that alternative.

The letter implies that the City will complete the EIR, and I assume BDG will pay the costs. Regardless of who prepares the EIR, it should include, but not be limited to, the following list of items:

1. Traffic Study: A thorough, complete, and up-to-date traffic study, with analysis of the various options for traffic control on Main Street, South Franklin Street, North Harbor Drive, and South Street. Pedestrian safety as well as flow of vehicles must be fully addressed.
2. New build versus Reuse of Existing Building: The General Code for the City states that reuse of existing buildings is preferred and encouraged. The similarity in square footage of the Old Social Services Building and the proposed new build requires that a full analysis be done for building reuse. If the exiting building is deemed unusable due to mold, then an air quality study should be performed. If the layout of the existing building is not workable, present diagrams showing that the space cannot be reconfigured to meet the needs of the client (Grocery Outlet). Indicate how the existing parking lot might be repurposed for loading and unloading, and for parking.
3. New building placement and parking lots: Alternatives for the placement of the new building should be presented and include analysis of parking lot function and pedestrian safety. The Planning Commission specifically asked BDG about building placement and they said it couldn’t be changed. However, in the same hearing, BDG went on to say that they did change the placement of a new building in

another location because (drum roll please) the Planning Commission in that town demanded it. As currently proposed, there is far too much interaction between vehicles and customers walking to the entrance of the building. One of the mitigations imposed by the Planning Commission to provide separation between customers walking to the entrance and vehicles coming, going, and driving in the parking lot is not actually possible given the proposed configuration of the parking lot. Therefore, it is essential to provide alternatives for building placement, parking lot placement, and placement of the entrance. In addition, accommodation for employee parking needs to be addressed

4. Noise Study: The MND for this project used data from a previous study done nearby (not as part of this project). Their analysis indicated no significant impact. The analysis done by an expert for FBLBM used the same data to show that there would be significant impact. During the appeal hearing, a statement was made that those data could not be relied on as valid. That means a new study should be done in the actual area of the project.
5. Biological Review: The initial and follow up studies were an embarrassment. The surveys were superficial and included some incorrect species identifications. The timing of the survey for wetland plants was inappropriate. The bat studies, as requested by California Department of Fish and Wildlife were never completed. A time series should be undertaken with in depth surveys of flora and fauna monthly for 1 year (or quarterly at a minimum). The study should include wet as well as dry seasons.
6. Survey for wetland soils: While the methodology appeared appropriate, the area with the greatest likelihood of having wetland soils was studiously avoided. This must be corrected.
7. Retention of Mature Trees: While many of the mature trees on the site are Monterey Cypress (a tree that is not native to the area), these mature trees provide important habitat and should be maintained. The EIR should include analysis of the measures to be taken that will protect not only these trees, but also their root structure.

These are a few of the areas that must be fully addresses in an EIR. Simply repackaging the MND will not be sufficient.



# City of Fort Bragg

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## Text File

File Number: 22-183

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Business

**In Control:** City Council

**File Type:** Resolution

**Agenda Number:** 8C.

Receive Report and Consider Adoption of City Council Resolution Approving the Revised 2022 Citywide Design Guidelines Update for Commercial, Industrial, and Multi-Family Buildings



AGENCY: City Council  
MEETING DATE: April 25, 2022  
DEPARTMENT: CDD  
PRESENTED BY: K. Locke  
EMAIL ADDRESS: [klocke@fortbragg.com](mailto:klocke@fortbragg.com)

## AGENDA ITEM SUMMARY

### **TITLE:**

**Receive Report and Consider Adoption of City Council Resolution Approving the Revised 2022 Citywide Design Guidelines Update for Commercial, Industrial, and Multi-Family Buildings**

### **ISSUE:**

The City of Fort Bragg's Design Guidelines were initially adopted by the City Council in 2004. The Guidelines have been an integral part of city planning in Fort Bragg and have helped the city retain its character and maintain property values. While effective on many levels, the existing Guidelines have become outdated. They do not address current trends and are not entirely relevant to many proposed projects. In 2019, the Planning Commission considered revisions to the Citywide Design Guidelines at three public meetings related to reuse of the former Mill Site. However, this document was never formally adopted by City Council. As a result of these meetings, an Ad-Hoc Committee was appointed – composed of two Councilmembers (Albin-Smith and Morsell-Haye) and two Planning Commissioners (Andreis and Rogers), to refine and update the City's Design Guidelines. This Ad-Hoc Committee met multiple times from February 2021 to February 2022. The intent was not to start "from scratch," but rather to reorganize and finalize the work done in 2019. A draft of the updated document may be found on **Attachment 1**.

### **ANALYSIS:**

#### *What are Design Guidelines?*

Design Guidelines are a qualitative document independent of the General Plan, Inland Land Use and Development Code (ILUDC), and Coastal Land Use and Development Code (CLUDC). The goal of the Design Guidelines is to provide clear and useful recommendations and requirements for the design, construction, review, and approval of commercial, industrial, and multi-family residential development in Fort Bragg. The guidelines are intended as a reference point for a common understanding of the minimum qualitative design expectations and offer as a way of achieving attractive and functional projects.

As opposed to the ILUDC or CLUDC, the Design Guidelines are also intended to be interpreted with some flexibility as not all design criteria may be workable/appropriate for each project. In some circumstances, one guideline may be relaxed in order to accomplish another, more important guideline. The overall objective is to ensure that the intent and spirit of the Guidelines are followed and to attain the best possible design within reason.

#### *Relation to Other Documents*

Fort Bragg's Design Guidelines provide an important benchmark for evaluating new construction, additions, and remodels, and are the basis for the "findings" necessary for many project approvals. Section 18.71.050(F)(7) & 17.71.050(F)(7) of the Inland & Coastal

AGENDA ITEM NO. 8C

Development Code requires a Design Review permit for most construction activities and further establishes that such permit may not be issued unless the design complies and is consistent to the Design Guidelines. The Guidelines also implement General Plan policies related to Community Design.

*Summary of Action*

Subsequent to the completion of the Guidelines, staff brought the document to Planning Commission twice. On March 16, 2022, a workshop/public hearing was held to gather feedback. Responses from the Commission and the public were minimal. Staff updated the document accordingly to this feedback.

On March 30, 2022 a second public hearing was held to present the revised document and finalize any additional feedback. The Planning Commission made their recommendation to City Council without any additional changes to the document.

**RECOMMENDED ACTION:**

Staff recommends that Council adopt a resolution approving the revised 2022 Citywide Design Guidelines Update for Commercial, Industrial, and Multi-Family Buildings.

**ALTERNATIVE ACTION(S):**

Provide direction to staff for further revision of the Citywide Design Guidelines Update.

**FISCAL IMPACT:**

There is no immediate financial impact or budget action necessary due to the recommended action.

**GREENHOUSE GAS EMISSIONS IMPACT:**

The Citywide Design Guidelines is a qualitative document intended to provide guidance to staff, applicants, and the public on development projects. There is no quantifiable scope to calculate emission impacts. The Design Guidelines will only apply to specific project sites under new construction or remodeling of existing structures. At time of review of these projects, greenhouse gas impact will be reviewed on a case by case basis.

**CONSISTENCY:**

*Conformance with the City of Fort Bragg General Plan*

The revisions to the Guidelines were reviewed to be in compliance with the goals, policies, and programs of the City of Fort Bragg General Plan. The following section includes said goals, policies, and programs from the Inland General Plan and comparison to Design Guideline policies or staff analysis:

<b>General Plan Goal, Policy, or Program</b>	<b>Related Design Guidelines Standard or Analysis</b>
Program CD-1.1.1: Periodically update the Citywide Design Guidelines.	Throughout the years there have been multiple attempts to update, consolidate, and replace the Design Guidelines. The most recent adopted version of the document occurred in

	2004 but other attempts to update have occurred in 2011, 2013, and 2019. The 2021 update would be culmination of these attempts and would allow for compliance with Program CD-1.1.1.
<b>Commercial &amp; Multi-family Residential Development</b>	
Program CD-1.1.2 Commercial and Multifamily Development: Continue to complete Design Review of commercial and multi-family development to ensure that they fit harmoniously with the scale and design of existing buildings and streetscape of the City.	<ol style="list-style-type: none"> <li>1. Commercial development shall compliment and/or Incorporate design elements and features from the historic architectural styles of the Central Business District, such as bay windows, porches, projecting eaves, awnings, and similar elements that add visual interest to the development.</li> <li>2. Architectural style should be compatible with the surrounding character, including building style, form, size, materials, and roofline.</li> </ol>
Policy S-3.4 Drought Tolerant Landscaping: New development shall include drought tolerant landscaping for landscaped areas in commercial and multi-family residential uses.	<ol style="list-style-type: none"> <li>1. Landscaping designs that do not require irrigation systems are preferred. Projects that include irrigation shall emphasize water-efficient plants. Rainwater and greywater are encouraged to meet all irrigation needs.</li> <li>2. Trees and plants native to the Northern California coast and those that flourish in the region shall be selected. Plant materials should also be selected for their low maintenance qualities.</li> </ol>
<b>Central Business District</b>	
Goal CD-2 Preserve the central business district as the commercial, civic, historic, and cultural center of the community.	<ol style="list-style-type: none"> <li>1. Buildings should be composed of elements and details representative of Fort Bragg’s architectural heritage. This may be expressed through the use of window and door treatments, storefront details, cornices, etc. Designers should familiarize themselves with the design elements and details used on older buildings in the downtown area and should incorporate contemporary versions of these older designs.</li> </ol>
Goal LU-3 Ensure that the Central Business District remains the historic, civic, cultural, and commercial core of the community.	<ol style="list-style-type: none"> <li>1. Buildings should be composed of elements and details representative of Fort Bragg’s architectural heritage. This may be expressed through the use of window and door treatments, storefront details, cornices, etc. Designers should familiarize themselves with the design elements and details used</li> </ol>

	on older buildings in the downtown area and should incorporate contemporary versions of these older designs.
Policy CD-2.2 Pedestrian Activity: Encourage increased pedestrian movement and activity in the Central Business District.	1. The provision of usable pedestrian-oriented spaces is strongly encouraged. The following types of open space should be considered: plazas; courtyards; outdoor dining; corner cut-off areas with enhanced amenities; and mid-block pathways and/or alleys.
Policy CD-2.5 Strengthen the Distinctive Identity of the Central Business District: Strengthen the distinctive identity and unique sense of place of the Central Business District.	1. Buildings should be composed of elements and details representative of Fort Bragg’s architectural heritage. This may be expressed through the use of window and door treatments, storefront details, cornices, etc. Designers should familiarize themselves with the design elements and details used on older buildings in the downtown area and should incorporate contemporary versions of these older designs.
Policy LU-3.1 Central Business District: Retain and enhance the small-scale, pedestrian-friendly, and historic character of the Central Business District (CBD).	1. Whenever an infill building is proposed, identify the common horizontal elements (e.g. cornice line, window height/width and spacing) found among neighboring structures, and develop the infill using a similar rhythm or alignment.
Policy LU-3.2 Mixed Uses: Support mixed use development (i.e., a combination of residential and commercial uses) in the Central Business District that does not conflict with the primary retail function of this area.	No guideline is related to this policy but no guideline conflicts with this policy.
Policy LU-3.3 <u>Historic Buildings and Mixed Uses</u> : In the Central Business District and in other commercial areas with historic residential structures, encourage residential uses, mixed residential, and commercial uses, and the preservation of historic structures.	No guideline is related to this policy but no guideline conflicts with this policy.
Policy LU-3.4 <u>Encourage Infill Development</u> : Encourage infill development of vacant and underdeveloped land in the Central Business District and adjacent commercial areas before amending	1. Whenever an infill building is proposed, identify the common horizontal elements (e.g. cornice line, window height/width and spacing) found among neighboring structures, and develop the infill using a similar rhythm or alignment.

the Inland General Plan and rezoning to obtain additional commercial land elsewhere.	
<b>Chapter 3: Special Use Commercial Design Guidelines</b>	
No specific General Plan goals, policies, or programs were found related to any of the special use types.	
<b>Chapter 4: Industrial</b>	
Goal LU-5 Support industrial development which is consistent with the protection, enhancement, and restoration of natural and scenic resources.	<ol style="list-style-type: none"> <li>1. Light industrial buildings in the Mill Site Light Industrial zoning district should have an industrial or contemporary architectural character that is consistent with the historic fabric of the Mill Site or the development patterns of the nearby skunk train industrial buildings.</li> <li>2. When industrial/warehouse uses are located adjacent to less intense uses (e.g., residential or retail commercial), additional landscaping in conjunction with appropriate decorative walls and setbacks should be provided to mitigate potential adverse impacts.</li> </ol>
Policy LU-5.1 <u>Siting New Industrial Development</u> : Site new industrial development so that it is contiguous with, or in close proximity to, existing developed areas able to accommodate it, or where such areas are not able to accommodate it, in other areas with adequate public services and where it will not have significant adverse effects on natural and scenic resources, either individually or cumulatively.	<ol style="list-style-type: none"> <li>1. When industrial/warehouse uses are located adjacent to less intense uses (e.g., residential or retail commercial), additional landscaping in conjunction with appropriate decorative walls and setbacks should be provided to mitigate potential adverse impacts.</li> <li>2. Street side facades of large industrial buildings, that are visible from a public street, should include architectural features such as reveals, windows, openings, and changes in color, texture, and material to add interest to the building elevation and reduce visual mass.</li> </ol>
Policy LU-5.2 <u>Industrial Land Use Standards</u> : Require that industrial development avoid or minimize creating substantial pollution, noise, glare, dust, odor, or other significant adverse impacts.	<ol style="list-style-type: none"> <li>1. Outdoor lighting (e.g., location, height, and number) should be designed to foster security. Site and building entries should have enhanced illumination to increase visibility and safety.</li> <li>2. Large expanses of highly reflective surface and mirror glass exterior walls should be avoided to prevent glare impacts on adjacent public streets and properties.</li> </ol>
Program S-3.4.1: Require landscaping for all new commercial and industrial	No guideline is related to this program but no guideline conflicts with this program.

development to use drought tolerant plants and no vegetative turf unless recreation needs or other area functions specifically requires turf.	
<b>Chapter 5: Sign Guidelines</b>	
Policy CD-3.1 Entryways: Clearly define the points of entry to the City through the use of distinctive signs, lighting, and landscaping.	See Chapter 2 – Gateways & Corridors.
Program CD-3.1.1: Maintain distinctive signs placed in a landscaped area at the south entryway at Highway 20/Highway One and at the north entryway on Highway One at the City Limits.	See Chapter 2 – Gateways & Corridors.

**ENVIRONMENTAL DETERMINATION:**

The Design Guidelines are categorically exempt from the requirements of the California Environmental Quality Act (CEQA) Class 1 Section 15301 Existing Facilities because the Design Guidelines will only apply to specific project sites under new construction or remodeling of existing structures. At the time approval for a new building or remodeling of an existing building is considered, an environmental determination will be made on a case-by-case basis.

**IMPLEMENTATION/TIMEFRAMES:**

If approved, the Design Guidelines update would be adopted on April 25, 2022 and made effective thirty (30) days after.

**ATTACHMENTS:**

1. Draft 2022 Guidelines Update
2. Planning Commission Recommendation
3. City Council Resolution of Approval



# Fort Bragg

## Citywide Design Guidelines

*April 2022*



# Acknowledgements

## City Council

Bernie Norvell, Mayor  
Jessica Morsell-Haye, Vice Mayor  
Tess Albin-Smith,  
Marcia Rafanan  
Lindy Peters

## Planning Commission

Jeremy Logan, Chair  
Jay Andreis, Vice Chair  
Stan Miklose  
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Nancy Rodgers

## Ad-Hoc Team

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Nancy Rodgers, Chair Planning Commission

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Special thanks to former Community Development Director,  
Marie Jones for work on 2019 draft, which served as template.

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## Chapter 1: Introduction

The City of Fort Bragg is a special place with a unique identity that its residents cherish. It is framed by natural scenery, including forests, the ocean, and bordered by two rivers. In town, the “built environment” which incorporates the residential and commercial architecture, is both unique in history and character. Many buildings and sites speak to the city’s logging roots and its role as an early commercial center in Mendocino County. The Fort Bragg built environment is described as low-key with Victorian roots. These are aspects that the community values and seeks to retain.

Over time, new construction is bound to alter the character of Fort Bragg, or be respectful of it. The challenge is to accommodate compatible development that builds from the design traditions of the community while promoting design excellence and creative new solutions. This publication sets forth design criteria with a goal to guide new development and property improvements to best meet the needs of the individual property owner while also enhancing the existing character that defines Fort Bragg.

### Purpose & Guiding Principles

The City of Fort Bragg’s Citywide Design Guidelines set expectations for site and building design in order to maintain and enhance the small-town, coastal, historic, and rural character of Fort Bragg. Property owners and professionals will use these Guidelines when planning improvements. City staff, committees, and commissions will use them in development review. The Design Review permit process mandates consistency with the Citywide Design Guidelines. These Guidelines will work in conjunction with other City regulations

to ensure development throughout the City is functional and attractive.

Situations may arise that are not covered by the Guidelines, in which case; project designers should consider the following Guiding Principles which reflect ideas from the community, the Design Review Ad-Hoc Committee, Planning Commission, and City Council.

#### *Guiding Principle 1: Community Character*

Project design should reflect and strengthen the distinct identity of Fort Bragg – a rural, historic small town on the Mendocino coast.

#### *Guiding Principle 2: Support Connectivity*

Project design should incorporate safe, functional and multimodal connections that are easy to navigate by walking, bicycling and public transit. When feasible, new streets should follow existing development pattern.

#### *Guiding Principle 3: Public Enhancements*

Project proposals should positively enhance the adjacent public realm by contributing to the collective good of community. This means building places, and not individual sites; making design consideration in the context of streets, sidewalks, public spaces, parks, and trails and looking at how the community interacts with these public spaces.

#### *Guiding Principle 4: Water & Power Sustainability*

Do more with less. Development should incorporate water and power efficient design strategies.

## How to Use Design Guidelines

The Citywide Design Guidelines consist of five chapters. The document is organized through a hierarchical approach, meaning the guidelines build off each other. The “General Guidelines” in Chapter 2 is the basis for all development. All commercial projects are required to comply with this section. Beyond the General Guidelines are the “Gateways & Corridors” which set specific standards for certain locations in the City. Finally, the “Guiding Principles” (listed above) are the overarching goals all projects should try to achieve.

Once an application for a project is submitted, the City of Fort Bragg will review the project for consistency with the Design Guidelines. Some guidelines may not apply to all projects due to a variety of reasons, such as land use, architectural design, or site-specific issues. Therefore, projects can generally meet the guidelines and still be consistent. This determination is at the discretion of the Community Development Department or the reviewing body (Planning Commission or City Council).

## Difference between Development Code & Guidelines

The Inland & Coastal Land Use & Development Code promotes the public health, safety, and general welfare of the community. The Development Code provides a variety of policies that implement the General Plan as well as base standards for physical development throughout the city. Similar to the Development Code, the Citywide Design Guidelines apply to all projects. However, the Development Code regulations dictate exactly what a property owner can and cannot do with a property, the Design Guidelines focus on how a project is constructed. For example, the City Development Code regulates how many parking spaces are required, while the Guidelines facilitate design objectives such as pedestrian mobility and landscaping.

## Relation to Mill Site Reuse

The former Georgia Pacific Mill Site, sited west of Highway One is currently zoned Timber Resources Industrial and provides a unique development opportunity for the City of Fort Bragg and the region. Following closure of the mill, extensive community planning has taken place to determine reuse of the Mill Site, including future development design considerations. These design considerations are imbedded into the citywide approach, and several guidelines specifically reference development west of Main Street.

## Geographic Approach

This document considers the location of development within the City, rather than zoning designations to inform design. The City of Fort Bragg is home to a variety of development contexts, each of which contribute to Fort Bragg’s unique character and small town charm. General Commercial Zoning along S. Franklin Street has an intimate, residential quality appropriate for multiuse development and mid-size retail – whereas, General Commercial along S. Main Street is characterized by large parking lots catering to vehicles, ideal for grocery stores and gas stations.

In order to address the intricacies of each context, five geographic areas are identified, each meriting special design consideration, as shown on **Page 10**. The design features of each location serve as cues for new development. Each geographic area is distinguished in numerous ways, whether it be the site design, opportunities for new development, or dominant architectural features.

## Central Business District

The Central Business District (CBD) represents the core of Fort Bragg’s historic downtown – the civic, cultural, and commercial center of the community. It is a pedestrian-oriented area accommodating everything from government and professional offices, to retail, restaurants, tourism, and similar types of businesses at a dense scale; residential uses are allowed and encouraged on upper floors or located to the rear of commercial uses. The predominant architectural style reflects Victorian era of the 1860-1930’s. Development is not required to recreate the actual historic architecture, but development should fit within the context of the historic look and contribute to the unique character of downtown. Key objectives for future development and remodels in this area include:

- Maximize transparent storefronts, especially along the ground floor.
- Orient buildings to face the street. Architecture should complement and respect the historic Central Business District vernacular.
- Keep the downtown a lively mixed use, 24hr core.



*Example of the downtown streetscape*



*Example of the downtown streetscape*

## Corridors

As mentioned previously, most commercial development in the City resides along Franklin Street and CA Route 1, referred to as Main Street. The Central Business District splits these streets to create four unique character areas identified as Main Street North, Main Street South, Franklin Street North, and Franklin Street South. Each character area has specific guidelines only applicable their respective locations.

### *Main Street North*

From the intersection of N Main Street and Pine Street to Pudding Creek lies the Main Street North corridor. Existing development varies in land uses and building types. The predominant type of building looks residential in nature, many serving as Bed & Breakfast Inns. Development generally sits adjacent to streetscape, with driveways and rear parking lots. Future development in the area should consider:

- Main Street North is lined with traditional Victorian and craftsman housing. The existing character of the area should be maintained and vernacular architecture is encouraged.
- Future development should maintain a pedestrian level scale.
- Emphasis on site design by incorporating street trees and other landscaping features that soften the structures and hardscaping/



*Example of a North Main Bed & Breakfast*

### *Main Street South*

The Main Street South corridor extends from the intersection of S Main Street & Oak Street to the Noyo River. Development in this corridor currently combines a variety of building types and uses including large retail stores, gas stations, motels, and other miscellaneous services. The majority of the existing development is set back from the street, placing surface parking lots between the street and buildings. Existing buildings in this area generally lack the significant architectural character, style and detail found in the Central Business District, and while sidewalks are present throughout the corridor, numerous driveways, large parking lots and five lanes for vehicular travel make this area less pedestrian-friendly. Key objectives for future development and remodels in this area include:

- Locate buildings close to Main Street.
- Emphasis on front yard trees and landscaping.
- Locate parking toward the rear or side of buildings.
- Encourage a cohesive use of land rather than fragmented commercial sites.

### *Franklin Street North*

The Franklin North corridor extends from Pine Street to Manzanita Street along North Franklin Street. Development is primarily industrial, with a few exceptions. Buildings are typically large in area and one-story in height. The majority of sites utilize chain-link fencing and graveled parking areas. Form follows function, maintaining access for large equipment and large warehouses as a priority over pedestrian-oriented and designed development. Key objectives for future development and remodels in this area include:

- Provide a generous landscape buffer between the building and public realm.
- When a fence is used, ensure it allows visual permeability, except when screening outdoor storage.
- Articulate large industrial buildings to break down the overall scale.
- Site or building entries should be visible from the public right-of-way.

### *Franklin Street South*

From the intersection of Oak and Franklin Street to North Harbor Drive lies the Franklin South Corridor. This corridor on the eastern side of the street is mainly an eclectic mix of single-family residences in a variety of building forms, setbacks, and landscape character. While the western portion is mainly made up of hotels and commercial development. Due to this mix of development, there is no significant architectural style and detail present throughout the corridor. Sidewalks and class II bikeways are present on both sides and speed limits are a maximum of 30MPH making it one of the more pedestrian friendly streets in town.

With some relatively large opportunity sites in this area, new development is likely to have a transformative impact. As new development occurs, new sites and buildings should be designed with the objectives listed below in mind.

- Ensure a comfortable pedestrian environment through design approaches for a front setback area.
- Limit parking to the rear or alley of primary structures.
- Create a visual and physical connection between a buildings entry and the public realm.
- Emphasis on front yard trees and landscaping.
- Mixed-use development is heavily encouraged.

## Gateways

Considered the “edge of town” entry points from the surrounding countryside. Gateways are conceptually simple, ensuring the transition from the outside of town and the urban character of the streetscapes within the town be appropriate in their “small town character.”

The City of Fort Bragg has two defined gateways, north of the Pudding Creek Bridge and South of the Noyo Harbor Bridge. Development in these locations should follow the following guidelines in addition to the General Guidelines.

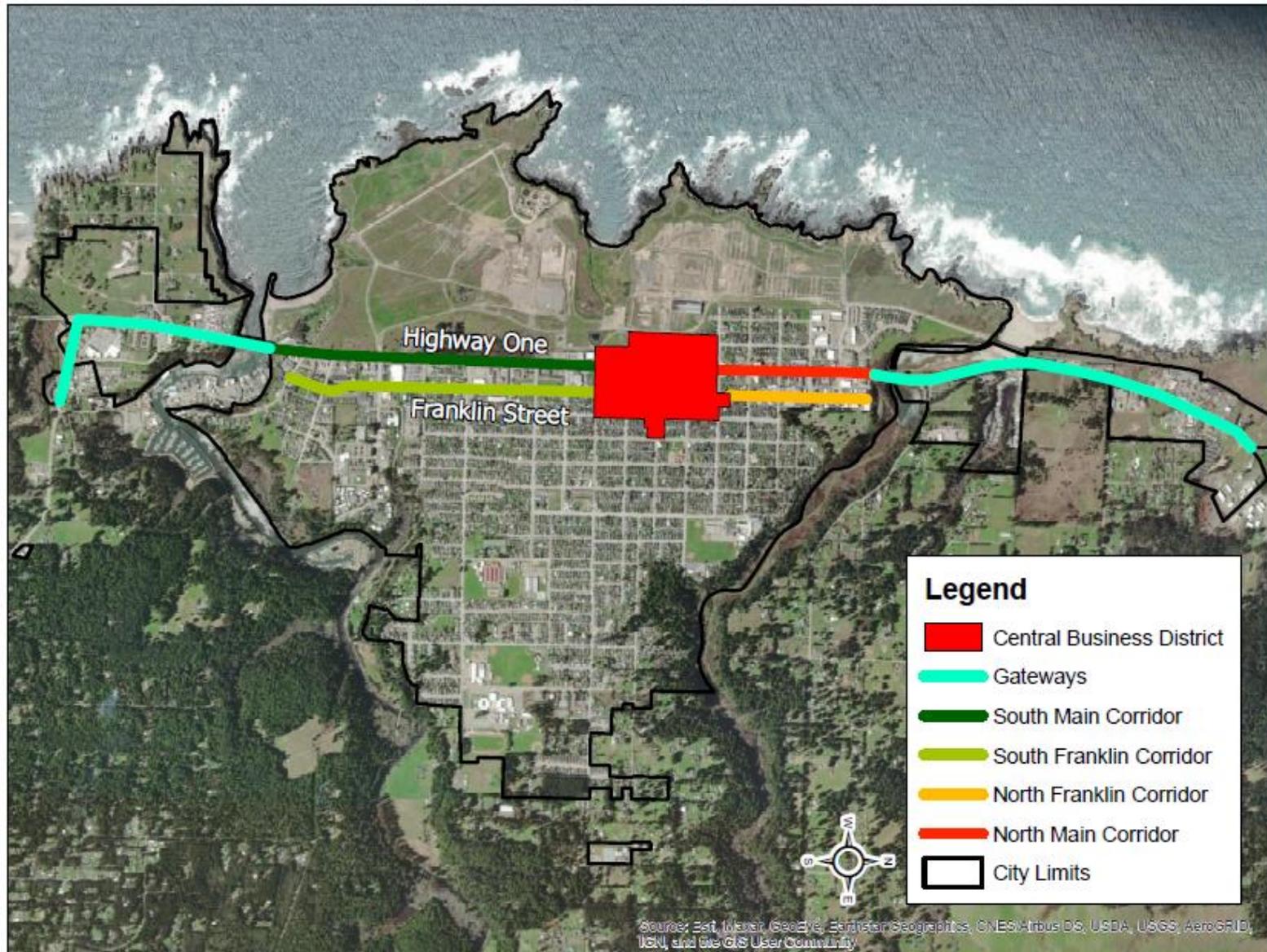
- Development should not detract from views to the ocean.
- Signage should be modest in scale and should not block the view shed.
- Significant landscaping shall be installed to reinforce the transition from a wilderness environment to an urban environment in gateway development. Landscaping should include trees and drought tolerant plants. Lawns are discouraged.
- Site design should include open space around the periphery of the gateway development to reinforce the transitional quality of the area.
- Site design should include installation of bike lanes and sidewalks.
- Gateway development should model Green Building techniques and materials.

### General Plan Reference

Goal CD-3, Policy CD-3.1, 3.2



# City of Fort Bragg Gateways & Corridors

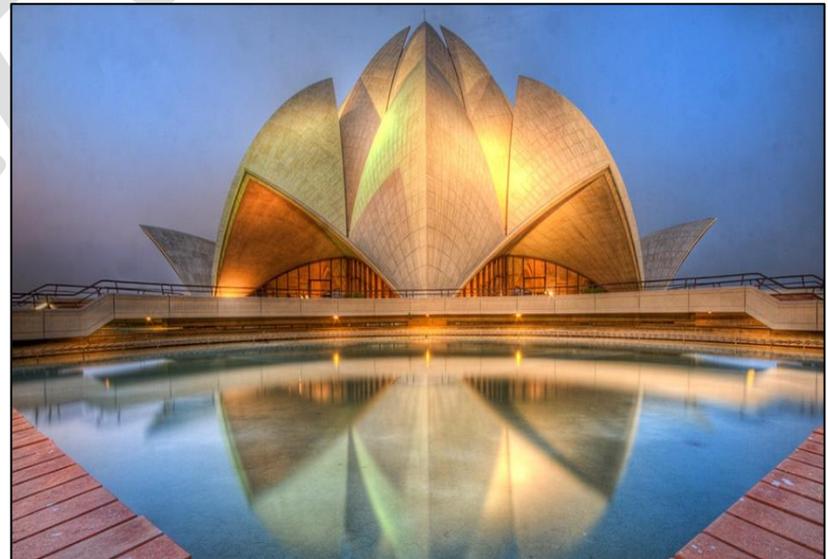


## Exceptional Design Exemption

The City recognizes the importance of innovative, exceptional and truly place making design, which may fall outside of the design criteria herein. Therefore, in addition to waivers and concessions made under applicable State laws, a project applicant may request exceptions, in writing, as part of the planning permit application for the proposed project. The request for an exemption must contain detailed information as to how the building design is going to create a unique space that is innovative or exceptional and a list of the design criteria which the applicant is unable to comply with in order to achieve this.

The decision-maker (Planning Commission or City Council) will consider the request and information provided and make findings to approve or deny the request. All of the following findings shall be made prior to approving an exemption.

- Approving the exemption will not create a safety hazard or impair the integrity and character of the neighborhood in which the property is located.
- Buildings and projects shall facilitate fundamentally new materials, methods, and tectonic articulations and/or forms of construction.
- The proposed project meets the purpose and guiding principles of the Design Guidelines.



## Chapter 2: General Citywide Design Guidelines

### Massing, Elevations & Articulations

#### *Mandatory Standards*

1. Structures shall be well-articulated on all sides visible from public streets and views. The highest level of articulation occurs on the front façade, and on all elevations visible from the public right of way. This includes variation in massing, roof forms, and wall planes, as well as surface articulation. Avoid boxy and monotonous facades that lack human scale dimensions and have large expanses of flat blank wall planes visible to the public.
2. All development adjacent to the Coastal Trail (Noyo Headlands Park) shall step back the structures on the upper floor from the side of the building that is adjacent to the Coastal Trail. Roof decks and balconies that overlook the coastal trail should be unobtrusive.
3. The scale of buildings shall relate to the surrounding development patterns. Buildings with greater height than surrounding buildings should step back the structure on the upper floors from street and public spaces to lessen the appearance of mass and bulk.
4. Architectural details and materials shall be incorporated on the lower part of the building facade to relate to human scale. These pedestrian scale elements can include awnings, trellises, windows, building base articulation, and changes in materials, textures, and colors.

[General Plan Reference](#)  
Goal CD-1, Goal S-1

#### *Preferred Standards*

1. Architectural elements that add visual interest, scale, and character such as projecting balconies, trellises, recessed windows, window and door detailing, or green garden walls should be incorporated to help articulate facades and blank walls.
2. Break up large building forms by vertical and horizontal variations in wall and roof planes, building projections, projecting ribs, reveals, door and window bays and similar design elements. To divide the building mass into smaller scale components, building faces over 50 feet long should reduce the perceived mass and bulk by one or more of the following: change of roof or wall plane; projecting or recessed elements, such as trellises, balconies, openings, etc.; varying cornice or rooflines; or other similar means.
3. All building and site design should use passive solar design strategies for space heating and lighting to reduce energy demand to the extent feasible.



*Structures shall be well-articulated on all sides visible from public streets.*

## Architectural Form & Detail

### *Mandatory Standards*

1. Franchise architecture is strongly discouraged. Buildings shall be readily reusable by other tenants and should not be identified with a design that is specific to a franchise.
2. Commercial development shall compliment and/or Incorporate design elements and features from the historic architectural styles of the Central Business District, such as bay windows, porches, projecting eaves, awnings, and similar elements that add visual interest to the development

### *Preferred Standards*

1. Commercial development should include a higher level of architectural detailing and higher quality materials at the pedestrian level of the building.
2. Architectural style should be compatible with the surrounding character, including building style, form, size, materials, and roofline.
3. The use of awnings, canopies, recesses, and arcades is strongly encouraged to provide protection for pedestrians and to add interest and color to buildings. Awning placement should fit within the scale, proportion, and rhythm created by the distinct architectural elements and should not cover piers, pilasters and other architectural details. Awnings should be compatible in color and design with the buildings. Awning frames and supports should be painted or coated metal or other non-corroding material and designed to withstand wind loads.

General Plan Reference  
Policy LU-4.1



*Commercial development shall incorporate design elements from the historic Central Business District.*



*Use of recesses are strongly encouraged to provide protection for pedestrians. (Union Lumber Company Store Early 1900s)*

## Roof Form

### Mandatory Standards

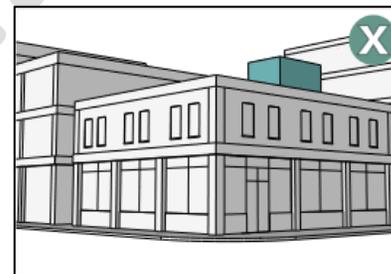
1. Roof forms shall be used to distinguish various building forms, create an interesting roof line, and help break up the building massing.
2. Buildings shall incorporate passive solar design and include at least one roof plane that is large enough to accommodate photovoltaic (PV) panels to meet the majority (>50%) of the building's energy needs, when feasible.
3. All roof-mounted equipment shall be effectively and attractively screened through the use of various architectural detailing including, but not limited to, roof form, decorative parapets or cornices that match the architectural character and materials of the building.
4. Highly reflective materials shall be avoided.
5. Balconies, roof gardens and roof decks shall be designed to minimize impacts on privacy in neighboring buildings and lots.



*Rooftop garden natural screening*

### Preferred Standards

1. Roof forms such as gable, hip or shed roof combinations are strongly encouraged. If parapet roofs are used they should include detailing typical of Fort Bragg's character and design. Special care should be exercised in designing how the roof frames or meets the sky, which may include but not be limited to: use of false fronts, architectural detailing, and roof overhangs.
2. Roof overhangs are encouraged to create shadow and add depth to facades. Where applicable to the architectural style, roof eaves should extend at least 12" from primary wall surface to enhance shadow lines and articulation of surfaces and protect from driving rain. Smaller roof overhangs are permissible with rain screen or other technologies. Roof overhangs should be designed to facilitate passive solar heating.
3. Exposed structural elements (beams, rafter tails, etc.) are encouraged as roof overhang details.
4. Natural and non-reflective roof materials are encouraged, including cool roof and green roof techniques (planted with native plantings) are encouraged.



*Roof mounted equipment shall be screened.*



## Windows, Doors, and Entries

### Mandatory Standards

1. The size and location of doors and windows shall relate to the scale and proportions of the overall structure.
2. The main building entrance shall be distinguished from the rest of the building and easily recognizable and oriented toward the street, internal walkway, or courtyard whenever possible.
3. Buildings located on corners in pedestrian areas shall provide for visibility around the corner, by either including windows on both walls that intersect at the corner or an angled corner entryway.
4. Exterior stairways shall compliment the architectural style of building, where required they should be open to allow views for natural surveillance.
5. Windows shall be incorporated at storefront locations.
6. The use of clear glass (at least 80% light transmission) is recommended. Dark tinted glass and reflective mirror-like glass are not allowed.



*Corner lot windows for visibility purposes*

### Preferred Standards

1. Front entry design should incorporate two or more of the following: front porch or stoop; recessed doors, archways, or cased openings; canopies; decorative detailing or placement of art; a projecting element above the entrance; integration of architectural elements such as flanked columns or decorative fixtures; changes in the roofline or a tower feature.
2. Window and door type, material, shape, and proportion should complement the architectural style of the building.
3. In general, upper stories should have a window to wall area proportion that is smaller than that of ground floor storefronts (typically 30 to 50 percent).
4. Windows should be articulated with accent trim, sills, kickers, shutters, window flower boxes, balconies, awnings, or trellises authentic to the architectural style of the building.



*Front entries should incorporate multiple design features*

## Materials

### *Mandatory Standards*

1. The following table identifies materials that are encouraged, acceptable and discouraged for use on a building's façade:

#### Encouraged

- Horizontal and vertical redwood or solid wood siding
- Stone veneer, authentic rock and similar natural materials
- Shingle siding
- Natural stone and brick
- Smooth stucco, hand troweled stucco
- Fiber cement siding with an authentic appearance, profile and dimension, over 3/8" thickness
- Other like materials

#### Acceptable

- Manufactured stone with an authentic appearance
- Board and batten
- Formed concrete
- Steel
- Glass block
- Corrugated metal/Architectural Metal Siding
- Other like materials
- Fiber cement siding with an authentic appearance, under 3/8' thickness

#### Discouraged

- Low quality wood engineered siding
- Textured/rough stucco
- Concrete block
- Ceramic tile, except as an accent material
- Slump rock
- Highly tinted, reflective, or opaque glass
- Vinyl siding
- Other like materials

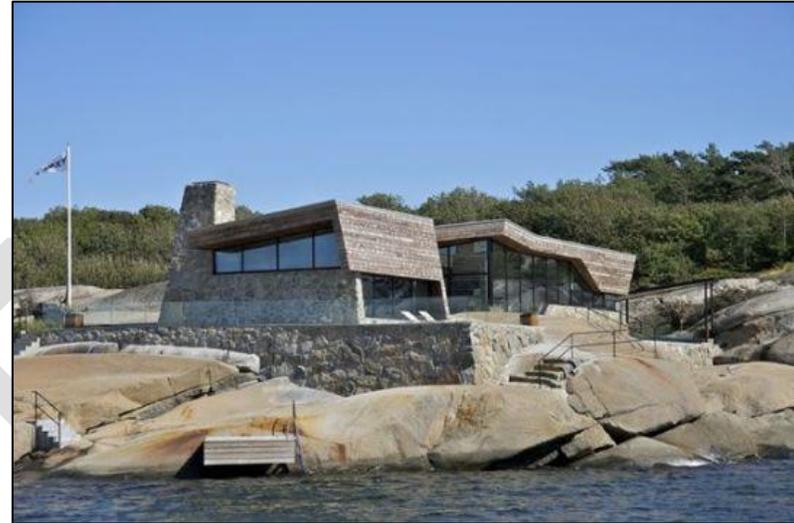
### *Preferred Standards*

1. Materials should be varied to provide architectural interest, however, the number of materials and colors should be limited and not exceed what is required for contrast and accent of architectural features. Exterior materials and architectural details should relate to each other in ways that are traditional and logical.
2. The use of green building and sustainable materials is encouraged to exceed the minimum required by the California Building Code.
3. Materials should enhance different parts of a building's façade and be consistent with the desired architectural style.
  - a. Where appropriate to the architectural style, materials and textures should vary between the base and body of a building to break up large wall planes and add visual base to the building.
  - b. Heavier materials and darker colors should be used lower on the building elevation to form the building base.
  - c. Exterior columns or supports for site elements, such as trellises and porches, should utilize materials and colors that are compatible with the rest of the development.
4. Material changes should occur at intersecting planes, preferably at inside corners of changing wall planes or where architectural elements intersect such as a chimney, pilaster, projection, or fence line.

## Colors

### *Preferred Standards*

1. Colors should enhance different parts of a building's façade and be consistent with the desired architectural style.
2. Colors west of Highway 1 should recede in the view shed or be subordinate to the view shed. Dark earth tone colors are preferred where the view shed is dark and lighter earth tone colors may be preferred where the background is lighter.
3. Colors should visually relate building elements to each other, and also individual façades to each other. The colors chosen for a building façade should complement neighboring façades (but should not replicate).
4. Color should be used as an important design element in the development's appearance. Historically, certain color palettes were associated with particular architectural styles. Whenever possible, exterior building colors should reflect the basic colors of the architectural style or period of the building or its environment. For example, bright and rich color combinations associated with the Victorian Era are appropriate downtown. However, in the coastal zone color pallet should focus on earth tone colors. Bright and sharply contrasting colors should be avoided.
5. Colors used on exterior façades should be harmonious and contrasting compatible colors are encouraged to accentuate details
6. No fewer than two colors should be used on any given façade, and three or more colors are preferred. This includes any "natural" colors such as unpainted brick or stone. The three preferred colors should constitute the primary base color, secondary color and minor trim color.



*Dark earth tones are preferred to blend with the environment*

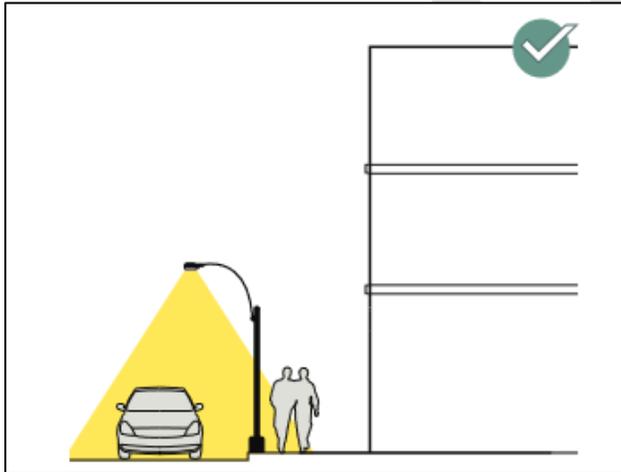


*Historic color palettes should be used in the downtown*

## Lighting

### Mandatory Standards

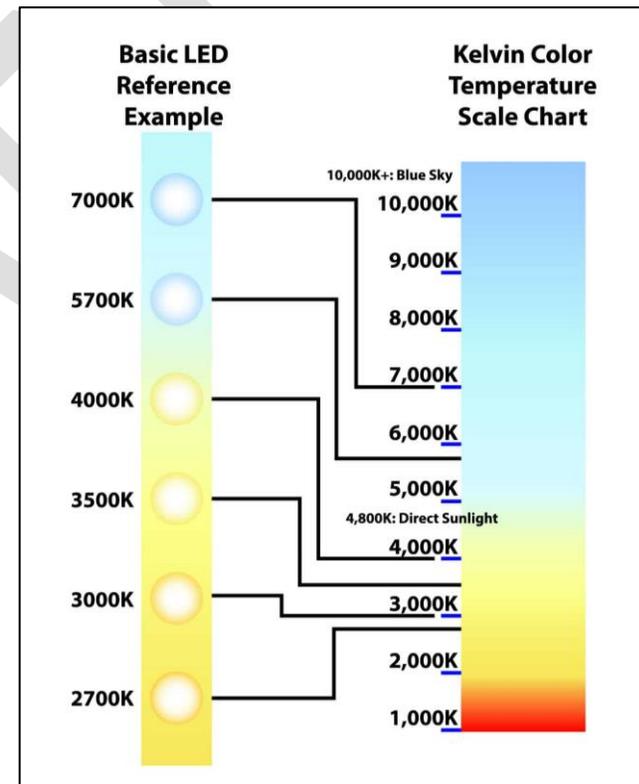
1. Exterior lighting shall be designed as part of the overall architectural style of the building and should illuminate entries, driveways, walkways, and activity areas.
2. Entrances shall be well illuminated for safety and identification purposes.
3. Lighting sources shall be hidden unless the sources are an integral part of the design. Lighting fixtures should not project above the fascia or roofline of the building.
4. Partial or full cutoff lighting is required. Exterior lighting shall be located and designed to avoid shining directly onto nearby residential properties, and shall minimize off-site glare. The latest technical and operational energy conservation concepts should be considered in lighting designs.
5. Parking lot lighting fixtures shall be no taller than 16 feet in height and shall cast light downward without allowing glare or light to encroach upon neighboring properties.



*Design lighting to minimize spill onto adjacent properties*

### Preferred Standards

1. Subtle and minimalist lighting may be used to accent architectural features and landscaping. Accent lighting should not contribute to glare or distract from the overall ambient night lighting in the neighborhood.
2. Exterior lighting should not have a color temperature above 4500 Kelvin.
3. Site lighting should minimize impact between the various uses (i.e. shielding commercial lighting from residential uses).



*Light temperature scale*

## Site Planning

### *Mandatory Standards*

1. Buildings should be sited in order to minimize impacts to surrounding development and open space. Care should be taken to place structures well to minimize impacts to natural areas and natural contours.
2. Buildings should generally be oriented toward the street. Buildings on corner parcels should establish a strong tie to both streets.

### *Preferred Standards*

1. Climate factors such as prevailing winds, window and door orientation, and the positioning of buildings on the site should be coordinated to maximize energy conservation and Photovoltaic (PV) access.
2. All building and site design should use passive solar design strategies for space heating and lighting to reduce energy demand to the extent feasible.



*Orient buildings toward the street*

## Landscape

### *Mandatory Standards*

1. Plants and trees with root systems that uplift hardscape materials shall be appropriately located away from paved and concrete areas.
2. Landscaping on parcels that are adjacent to the Coastal Trail shall use plants native to the northern California Coast. Invasive plants are prohibited.
3. Trees and plants native to the Northern California coast and those that flourish in the region, shall be selected. Plant materials should also be selected for their low maintenance qualities.



*Plants native to the Northern California Coast shall be selected*

### *Preferred Standards*

1. Landscaping should generally incorporate plantings utilizing a three-tiered system: ground covers (including flowering plants – annuals and perennials), shrubs/vines, and trees.
2. Landscaping should enhance the character and sense of place for each project. Landscaping should help complete the design of a site and should not be added as an afterthought. The choice, placement, and scale of plants should relate to the architectural and site design of the project. Landscaping should enhance the quality of the development by framing and softening the appearance of buildings, screening undesirable views and equipment, providing buffers from incompatible uses, and providing protection from wind and rain. Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity.
3. Landscaping designs that do not require irrigation systems are preferred. Projects that include irrigation shall emphasize water-efficient plants. Rainwater and greywater are encouraged to meet all irrigation needs.
4. Bio-swales and rain gardens should be utilized within landscaped areas to infiltrate storm water on site.
5. Landscaping features that define and accent specific areas such as building entry, parking lot entrances and the main walkways to community facilities are encouraged.

[General Plan Reference](#)  
Goal S-1, Goal S-3, Policy S-3.4

## Fencing and Screening

### *Mandatory Standards*

1. Fences or walls of more than 100 ft should provide variation in the design – via changes in height, materials, embellishments, step backs, gates, etc. - to break up the length and provide visual interest.
2. Screening should not result in hiding places or entrapment areas.

### *Preferred Standards*

1. Fences should be kept as low as possible while still performing their intended security, screening, or separation functions.
2. Fencing materials and colors of fences and walls should be consistent and compatible with the architectural themes on the site. Open, wooden or natural fencing are preferred.
3. Use of berms, vines and plantings should be used to screen less desirable areas from public view; i.e., any solid, windowless elevations, trash enclosures, propane tanks, parking areas, storage areas, loading areas, public utilities, and mechanical equipment.



*Well articulated fencing*

## Open Space & Pedestrian Circulation

### *Preferred Standards*

1. Courtyards, public space, plazas, and landscaped areas are encouraged.
2. Trees and public art should also be incorporated into courtyard, plaza, and mid-block passage design.
3. Open space areas should be sheltered from the noise and traffic of adjacent streets or other incompatible uses. Open space siting should give consideration to prevailing breezes and sun orientation in order to provide a comfortable environment.
4. Ideally, at least 50 percent of the open space area should have access to direct sunlight.
5. Shelters are encouraged to provide protection from inclement weather.
6. In commercial areas, open spaces and passages should be inviting, well lit, and accessed from multiple locations.

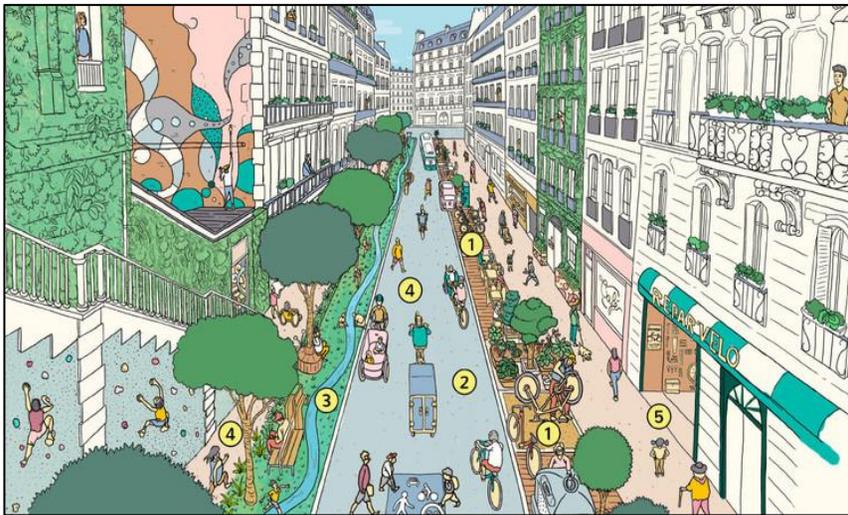


*Mid-block passage public art*

## Site Amenities

### Mandatory Standards

1. Building numbers and individual unit numbers shall be readily visible, in a consistent location, well lighted at night, and compatible with the overall design of the development.
2. Internal circulation signs and visitor parking areas shall also be clearly indicated. A directory that shows the location of buildings and individual dwelling units within the development is encouraged.



*Pedestrian amenities are strongly encouraged*

### Preferred Standards

1. Where bus routes are located near the development, the site design should consider convenience and comfort factors for users. These include direct access, widened sidewalks, seating areas, and weather protection provided near public transit stops.
2. Pedestrian amenities (benches, shelters, drinking fountains, lighting, trash receptacles, electric vehicle charging stations, and bicycle racks) are strongly encouraged.
3. Pedestrian activity areas should provide a sufficient level of wind and rain protection for pedestrians. Canopies, trees, or other methods of protection should be provided.
4. The relative size and design of private street furniture should be compatible with the architectural style of the building to which it relates, while also complementing street furniture in the public realm. Street furniture should be constructed of durable, easily maintained materials that will not fade, rust, or otherwise quickly deteriorate.
5. Decorative paving is encouraged for entryways, courtyards, plazas, pedestrian walkways, and crosswalks. Paving materials should complement the architectural design of the building and landscape design: stamped concrete, stone, brick, pavers, exposed aggregate, and colored concrete are recommended

General Plan Reference  
Goal C-1, Goal C-10, Policy C-3.4

## Pedestrian Circulation

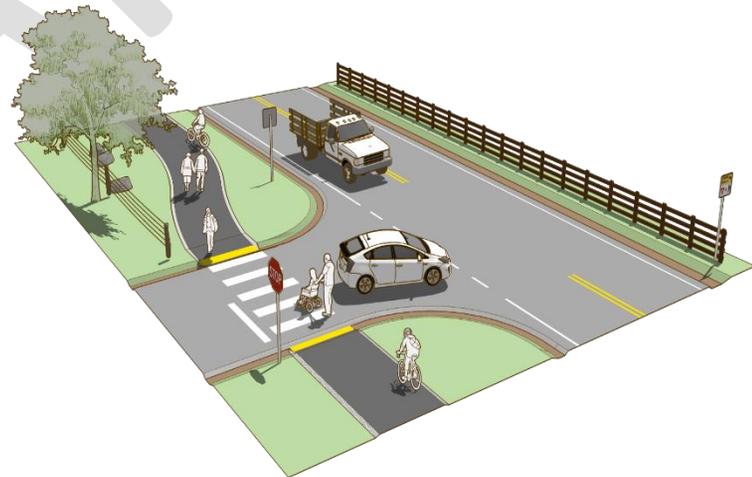
Pedestrian access connects buildings to their surroundings and encourages street activity.

### *Mandatory Standards*

1. On-site drop-off areas shall be adjacent and parallel to streets and/or drive aisles to allow vehicles to get out of the main flow of traffic and stop. These include bus stops and pedestrian pick-up/drop-off areas
2. Development adjacent to Coastal Trail (Noyo Headlands Park) shall provide pedestrian access to the coastal trail.

### *Preferred Standards*

1. Continuous, clearly marked pathways should be provided from the parking areas to main entrances of buildings. Design walkways and parking lots to minimize pedestrians crossing parking stalls and landscape islands to reach building entries.
2. Convenient pedestrian connections should be provided to adjoining developments, commercial projects, and other compatible land uses.
3. Pedestrian access to adjacent existing or planned open space areas and corridors should be provided for the development's users.
4. Raised walkways, decorative paving, landscaping, and/or bollards that separate pedestrians from vehicular circulation are encouraged.
5. Pedestrian walkways should connect common areas (parking, open space, playground, etc.) to site buildings, sidewalks and adjacent parks.



*Examples of preferred pedestrian pathways*

## Circulation and Parking

Safe and efficient parking and circulation arrangements take into consideration the needs of pedestrians, children at play, parking lot appearance, and prevention of car theft or damage.

### *Mandatory Standards*

1. Parking lots shall be well designed, with consideration given to landscaping, lighting, building massing, and pedestrian/vehicular circulation.
2. Parking shall be designed for safe ingress and egress. Site plans should balance the need to provide adequate vehicular access with the need to eliminate unnecessary driveway entrances.
3. Pedestrian connections and linkages within parking lots shall have a well-defined separation from vehicular circulation.
4. Shared parking is encouraged. Parking areas serving multiple uses shall consolidate parking into larger lots.

### *Preferred Standards*

1. Locate parking lots to the rear of buildings, along alleys, or on side streets to avoid conflicts on major streets. When this is not possible, design the primary entry to the lot with patterned concrete or pavers to differentiate it from the sidewalk.
2. Parking areas should be linked to adjacent public sidewalks, pedestrian walkways, alleys, and open space areas.
3. Parking lots shaded with solar panels are encouraged.
4. Pedestrian access from parking lots to building entries should be defined in the design of the parking lots, creating clear and visible walkways. In addition, walkways should be landscaped with shade trees or shrubs and other pedestrian amenities. Pedestrian connections should

connect parking area to sidewalk through buffer areas at key locations.

5. The use of brick, interlocking pavers, and cobblestones and or permeable paving for drive isles and parking lots are encouraged.
6. Special accents that define the main parking lot entry are strongly encouraged.
7. Dead-end aisles are strongly discouraged.



*Solar and shaded parking lots are encouraged*

## Additions, Remodels, & Renovations

### *Mandatory Standards*

1. The design of a proposed addition shall follow the general scale, proportion, massing, and detailing of the original structure. New additions shall be interpretations of, or improve upon, the design of the existing structure wherein the main characteristics of the existing building are incorporated or improved upon using modern construction methods. This may include:
  - a. Using similar proportions
  - b. Extending the architectural lines from the existing building to the addition
  - c. Sensitivity to the patterns of window and entrance spacing and openings
  - d. Harmonizing with existing colors and materials
  - e. Inclusion of similar architectural details (i.e. window/door trim, lighting fixtures, decoration)
2. Building materials used for the addition shall be of comparable or better quality than the existing building.

### *Preferred Standards*

1. Introducing or changing the location, size, or style of windows or other openings that alter the architectural rhythm or character of the original building is discouraged.
2. When original decorative details and architectural elements were covered up in previous remodeling, these forgotten details should be restored and incorporated in the design of the remodeled building.

## Garages & Ancillary Structures

### *Mandatory Standards*

1. Accessory structures shall be complementary in form, material, and color to the primary buildings.
2. The number of accessory structures shall be minimized; uses shall be combined where possible into one accessory structure.

### *Preferred Standards*

3. Refuse and recycling storage areas are encouraged to be covered

## Loading and Delivery

### *Mandatory Standards*

1. Loading and delivery service areas shall be located and designed to minimize their visibility, circulation conflicts, and adverse noise impacts to the extent feasible.
2. Loading and delivery areas shall be screened with portions of the building, architectural wing walls, freestanding walls and/or landscaping planting.

## Chapter 3: Specific Land Uses

### Central Business District

#### Site Planning

##### *Mandatory Standards*

1. Buildings in CBD should be built to the front and side property lines to form a continuous line of active building fronts along the street and avoiding gaps. Exceptions to this standard are:
  - a. Portions of a building's façade may be set back to provide areas for plazas, pedestrian areas, pedestrian paseos, outdoor eating spaces, and small landscaped areas. Such areas should be provided with outdoor furniture and amenities appropriate for the space.
  - b. The provision of corner setbacks and cutoffs is strongly encouraged to facilitate pedestrian movement, provide better visibility for drivers, and accentuate corner buildings.
2. No parking shall be provided between building front doors and the street they face in the Central Business District.

##### *Preferred Standards*

1. In walkable shopping areas, building entrances should be spaced no more than 50 feet apart from each other (to provide a seamless retail experience and to increase social interaction and sidewalk activity).
2. The provision of usable pedestrian-oriented spaces is strongly encouraged. The following types of open space should be considered: plazas; courtyards; outdoor dining; corner cut-off areas with enhanced amenities; and mid-block pathways and/or alleys.



*Buildings should be built to the front and side property lines*



*No parking shall be provided between the front doors and street*

## Architecture

### Mandatory Standards

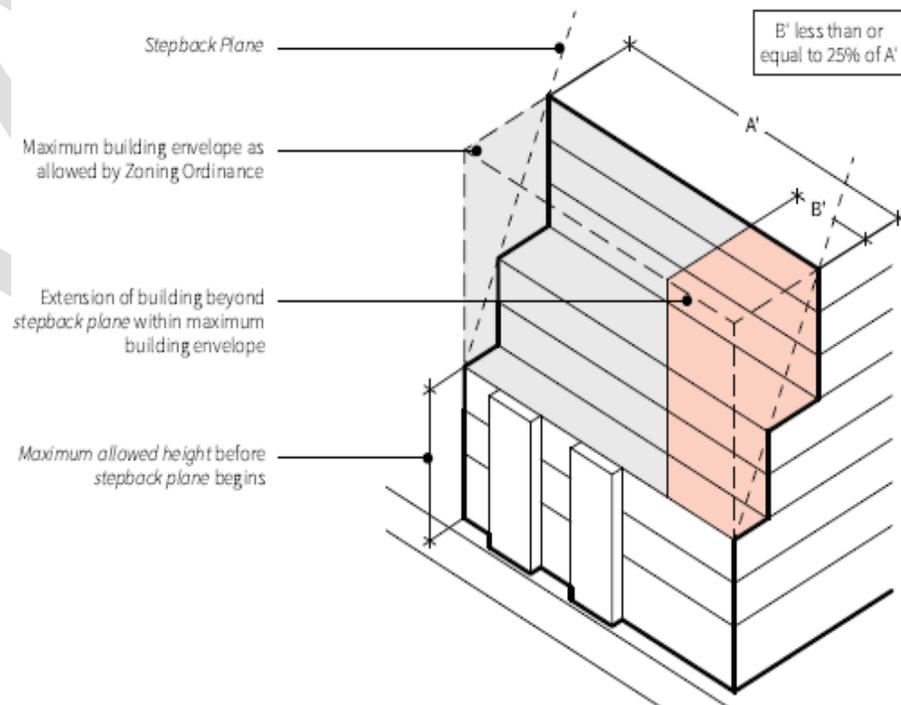
1. Human scale shall be created through the use of building forms such as arches, walls, trellises, awnings, arbors and pergolas.
2. Buildings shall be designed with a well-defined base, mid-section or body, and a top story or roofline.
  - a. Building base – The design of the building base should differentiate it from the upper floors of the building. This may be a projection of the lower wall surface and/or a different material or color. It may be created by a heavier or thicker design treatment of the entire ground floor for a building of two or more floors, or by a setback of the upper floors.
  - b. Mid-Section – The preferred architectural character of the mid-section is to treat it as a solid wall with recessed windows or groupings of windows. Long or large wall surfaces with flush-mounted windows or without windows should be avoided.
  - c. Roofs and Rooflines – The design of roofs and rooflines should provide visual interest from the streets below and should complement the overall façade composition. Roofs of historic commercial buildings should be used as an inspiration for new designs. Flat roofs and false facades are acceptable if a strong, attractively detailed cornice and/or parapet wall is provided.
3. Blank walls on elevations visible from public streets and gathering spaces are prohibited.
4. To divide the building mass of larger buildings into smaller scale components, buildings over 50 feet of frontage, visible from a public right of way, shall reduce the perceived mass and bulk by using one or more of the following:

- a. change in roof heights or wall plane;
- b. projecting or recessed elements;
- c. varying cornice or rooflines; or
- d. other similar means.

### Preferred Standards

1. Architectural features in good proportion with the overall structure are encouraged. Gables, turrets, towers, or similar elements are encouraged to accent buildings at street corners, at the terminus of a street corridor, alley, or pedestrian way. Corner buildings should have prominent corner entrances.

*Divide buildings into smaller scale components*



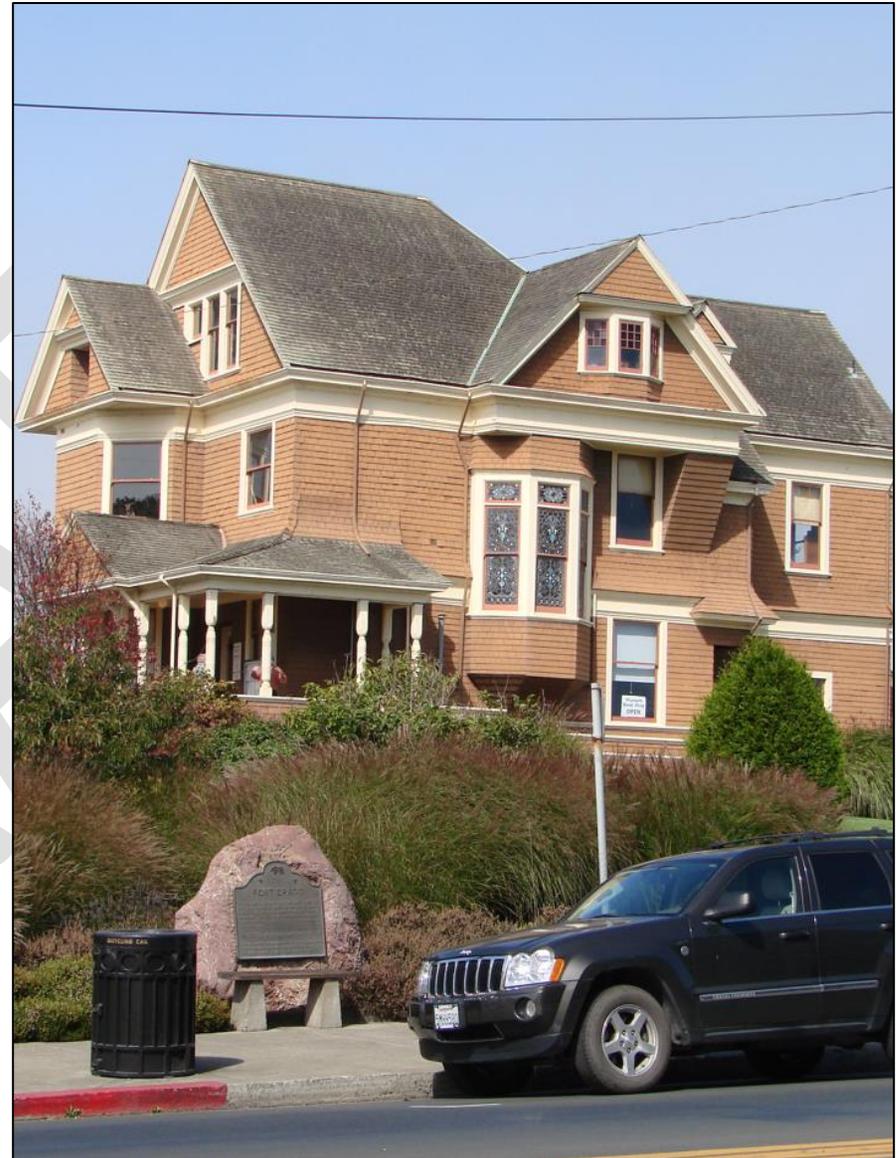
## Architectural Compatibility

### *Mandatory Standards*

1. Buildings shall be composed of elements and details representative of Fort Bragg's architectural heritage. This may be expressed through the use of window and door treatments, storefront details, cornices, etc. Designers should familiarize themselves with the design elements and details used on older buildings in the downtown area and should incorporate contemporary versions of these older designs.
2. Whenever an infill building is proposed, identify the common horizontal elements (e.g. cornice line, window height/width and spacing) found among neighboring structures, and develop the infill using a similar rhythm or alignment.

### *Preferred Standards*

1. The overall pattern of windows, wall panels, pilasters, building bays, and storefronts should be based on a module derived from Fort Bragg's prevailing module of ground level building features. Generally, storefronts and building bays should be based on modules of approximately 25 to 50 feet in width.



*Buildings in the Central Business District should represent Fort Bragg's architectural heritage*

## Storefronts

### *Preferred Standards*

1. Each storefront should be treated like a small building with its own base, roofline, and door and window pattern.
2. The base panel (bulkhead) below the display window should be a minimum of 24 inches and a maximum of 40 inches. Materials in this area should be visually heavier than adjacent walls.
3. Recess entries that provide for weather protection and a transition zone from sidewalk activity into the store are strongly encouraged. Recommended treatments include: special paving materials such as ceramic tile or brick; ornamental ceilings such as coffering; and decorative light fixtures.
4. Cornices should be provided at the second floor to differentiate the storefront from upper levels of the building and to add visual interest.
5. The following details are encouraged to add visual interest and functionality: hanging or mounted light fixtures with decorative metal brackets; decorative scuppers, catches and downspouts; balconies, rails, finials, corbels, plaques; and metal grille work at vent openings.
6. Doors should be substantial and well detailed. They should match the materials, design, and character of display window framing.



*Each storefront should be treated like a small building*

## Mixed Use Development

### Site Planning

#### *Mandatory Standards*

1. Loading areas and refuse storage facilities for the commercial use should be located as far as possible from residential units and should be completely screened from view from adjacent residential portions of the project or another adjacent residential uses. The location and design of refuse storage facilities should mitigate nuisances from odors when residential uses might be impacted.



*Buildings should be arranged to create opportunities for common open space*

#### *Preferred Standards*

1. Residential buildings should be arranged to create opportunities for common open space for the residential use. Common open space areas should be completely separated from other uses on the site and should provide a semi-private gathering place for residents.
2. Buildings should be constructed near or along the front property line(s). The minimal allowable setback should be used from the primary property line(s) is encouraged.
3. On larger parcels, projects should provide pass-through or plazas to facilitate pedestrian access to parking areas and surrounding uses and to create pedestrian gathering spaces.



*Buildings should be constructed along the front property line(s) with minimal setbacks*

## Architecture

### Mandatory Standards

1. When residential & commercial uses are in the same structure, separate pedestrian entrances should be provided for each use.
2. All roof-mounted equipment should be completely screened from views above. Special consideration should be given to the location and screening of noise generating equipment such as refrigeration units, air conditioning, and exhaust fans so that they don't disturb residences.

### Preferred Standards

1. The architectural style and use of materials should be consistent throughout the entire mixed use project. However, differences in materials and/or architectural details may differentiate the residential portion of the project from the commercial portion of the project.



*Architectural style and materials should be consistent*

## Site Amenities

### Mandatory Standards

1. Outdoor Space. include publicly accessible, designed outdoor space for resident and public use, that is proportionate to the size of the proposed buildings. Public spaces can include plazas, parks, courtyards, corridors, sidewalk cafes, trails, outdoor seating areas and/or similar active and passive areas. Public spaces should be located in visually prominent, accessible and safe locations that promote year-round activity.
2. Usable open space or public gathering places accessible to the community (e.g., a roof garden, expanded waiting area adjacent to a bus stop, etc.) shall be provided when feasible.
3. Landscaping, shade trees, and benches shall be incorporated into the site design as well as outdoor dining areas to encourage pedestrian activity on the ground floor level of a building.



*Outdoor space shall be proportional to the size of the buildings*

## Street Design

### *Preferred Standards*

1. Street Design interior to each development, the pedestrian-oriented street design should include on-street, diagonal or parallel parking; wide pedestrian walkways along building frontages; street trees in tree grates or planting beds; and/or bulb-outs. Pedestrian crossings shall be included at regular intervals along the internal roadway system for maximum connectivity. Center median landscaping is encouraged.
2. Design landscaping islands and walkways to connect building entries where possible.
3. Minimize conflicts between pedestrians and vehicles by limiting curb cuts. Provide shared curb cuts between adjacent properties in multi-site developments wherever possible.



*Landscaping islands and walkways should connect building entries*



*Pedestrian oriented walkways with street trees*



*Provide shared curb cuts*

## Compatibility with Adjacent Uses

### Mandatory Standards

1. To ensure and protect the privacy of residents in adjacent single-family homes, windows in mixed-use projects facing single-family residences within 15 feet of the property line, shall be carefully arranged. Examples of privacy options include translucent or louvered windows, offset window patterns, or clerestory windows.
2. Site planning and building design shall provide for convenient pedestrian access from the public street into the nonresidential portions of the project, through such means as courtyards, plazas, walkways, and street furniture.
3. Upper floors of mixed-use buildings shall be stepped back when adjacent to single-family residences.
4. Mixed-use projects shall be designed to minimize vehicular circulation on streets through local single-family neighborhoods.
5. New development over a single story shall be designed in a way to minimize impact to natural ventilation and solar access on adjacent properties.



*Upper level step backs reduce looming impacts*

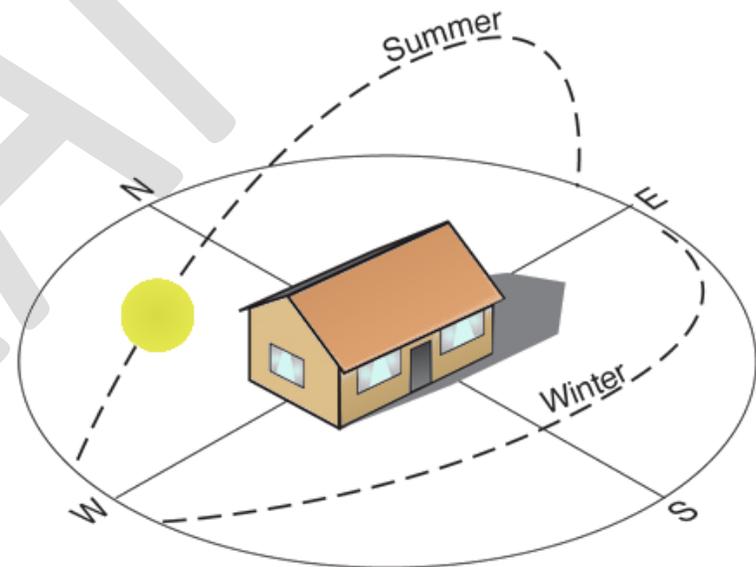
## Construction between Uses

### Mandatory Standards

1. Common walls between residential and non-residential uses shall be constructed to minimize the transmission of noise and vibration.
2. Orient buildings to minimize impact on natural ventilation and natural daylight for residences.

### Preferred Standards

1. Where practical, mechanical equipment and other sources of noise should be located away from building areas and exterior spaces designed for use by residents.



*Orient buildings to minimize impact on natural daylight*

## Site Planning – New Single Family Subdivisions

### *Mandatory Standards*

1. Variation of development patterns within new neighborhoods is necessary to achieve visual diversity and avoid a monotonous appearance. One or more of the following techniques shall be incorporated into the project's design to help achieve diversity.
  - a. Varied front yard setbacks – Placement of homes and garages close to or back from the street creates different patterns of visible open space. The structures themselves, when close to the street, also add diversity to the view.
  - b. Varied lot widths & sizes – Making some lots wider, and some narrower, than the average lot provides different amounts of open area between structures. It also allows placement of different sizes and shapes of homes that give a neighborhood more character and individuality.

### *Preferred Standards*

1. Residential structures should be oriented and placed for:
  - 1) energy access and efficiency; and
  - 2) to improve the streetscape with front doors and porches facing the street and garages and parking pulled behind the buildings to reduce their dominance

## Site Planning – New Multi Family Developments

### *Mandatory Standards*

1. New multi-family residential structures shall be compatible with other development in the immediate area. New structures should complement existing development through scale, proportion, height, form, style, siting, and relationship to surrounding structures.
2. Buildings shall be oriented toward the street. Buildings should be oriented to provide some privacy yet still relate to the street and the existing community. Doors should be visible from the street and windows should allow residents to have "eyes on the street" for natural surveillance.
3. In addition to a street orientation, the clustering of multi-family units shall be a consistent site planning element. Whenever possible, buildings should be configured around courtyards, gathering areas, and open spaces.

### *Preferred Standards*

1. Mailboxes should be located in highly visible, heavy use areas for convenience, to allow for casual social interaction, and to promote safety. A bench or seating area in close proximity to the mailbox location is strongly encouraged. A recycling receptacle should be located adjacent to the mailboxes.
2. Clusters of smaller buildings (with one to 6 units) are preferred over large buildings with more than 6 units per building.

## Architectural Design

### *Mandatory Standards*

1. Carports and detached garages shall be designed as an integral part of the development's architecture. They should be similar in material, color, and detail to the main buildings of the development. Flat roofs should be avoided. Prefabricated metal carports should not be used.
2. The design of houses shall be varied within new neighborhoods to create diversity and interest. Housing on a street should include variability in massing, composition, architectural style, finish materials and colors. Repeating designs are permissible, only if the quality of the design is excellent and repetition is part of the architectural style. Repetitive units should not comprise more than eight units in a row or 50% of the units on any single block, whichever is more.

### *Preferred Standards*

1. Use of single-family residential design elements (e.g., pitched roofs, porches, individual entries) are recommended to reduce perceived density, give identity to the development and its individual dwelling units, add visual interest, and be compatible with the neighborhood context.
2. Buildings with height greater than two stories that step back the structure on the upper floor from the street and public spaces are encouraged.

## Open Space & Outdoor Play Areas

### *Mandatory Standards*

1. Children's play areas shall be visible from as many units as possible and from private open space areas. Direct convenient access from private open space to the communal play area is encouraged.
2. Projects in new developments shall include bike paths as part of the street section, where feasible. Additionally, landscaping should be provided between the sidewalk and the street.

### *Preferred Standards*

1. A series of connected open space areas of varying shape, appearance and usage are encouraged.
2. Boundaries between private and common open spaces should be clearly defined by low walls or plant materials.
3. Outdoor play areas should be located adjacent to laundry rooms, community centers, or similar common facilities. Play areas should not be located near public streets, parking, or entry areas unless physically separated by appropriate walls, fencing, or dense landscaping.
4. Hard surface areas for outdoor activities (e.g., bicycle riding, skating, rope jumping, and hopscotch) should be provided. These active play areas should be safely separated from vehicular use areas.
5. In larger developments, separate, but not necessarily segregated, play areas or informal outdoor spaces should be provided for different age groups for safety reasons. Small developments may combine play areas (e.g., a tot lot incorporated into a larger activity area for older children).
6. Seating areas should be provided where adults can supervise children's play and also where school-age children can sit. Seating location should consider comfort factors, including sun orientation, shade, and wind.

## Auto Repair Services

### *Mandatory Standards*

1. Parking spaces for vehicles stored for repair shall be located in the least visible areas of the site. Surface parking lots shall be screened with active building space, fencing, art and/or landscaping to provide a visual buffer between the public right of way and stored vehicles.
2. Special design considerations should be made for the storage of oil, lubricants and other potentially hazardous materials.
3. Compressors and pneumatic equipment shall be used in enclosed structures.
4. Adequate storage and trash areas shall be designed to accommodate disposal of junk parts, packing, and used oil and lubricants.

### *Preferred Standards*

1. Service/work bays should be oriented so that the interiors are not visible or audible from adjacent public streets, residential structures, or active open space. If such an arrangement is not possible, dense landscaping and/or screen walls should be used.
2. Sufficient space for vehicle drop-off should be provided. Site design should provide space for vehicle stacking during peak hours.

## Service Stations

### *Mandatory Standards*

1. Driveway cuts shall be limited to two per site.
2. Each pump island shall include a vehicle stacking area for at least three vehicles on at least one end of the pump island.

### *Preferred Standards*

1. Buildings containing service or car wash bays should not face toward a public street nor adjacent residential property.
2. The site design for projects located at street corners should provide a strong design element at the corner to help frame the public right-of-way and anchor the corner.
3. The site design should accommodate circulation and traffic volumes, minimizing paving and soften paved areas with landscaping.



*Soften paved areas with landscaping*

## Drive-Through Businesses

### *Mandatory Standards*

1. The building shall be the dominant visual feature from the street frontage, not the parking lot or the drive-through aisle.
2. Drive-through aisles shall be located at the rear of the building away from the street frontage whenever possible. If the drive-through aisle is located between the building and the street, dense landscaping and landscaped berms should be provided to screen the drive-through aisle from view from the street.
3. Drive-through lanes shall accommodate vehicle stacking at the menu board and at the pick-up windows to ensure adequate circulation.
4. Drive through lanes and loading/unloading areas shall be located as far as possible from residential properties.
5. Franchise formula architecture is strongly discouraged. Franchise identifying features shall be limited to the company's logo and signs.

### *Preferred Standards*

1. Outdoor eating areas are encouraged and should include details such as trellises, low walls, fountains, etc.
2. Drive through should have a waiting area.



*Franchise architecture is strongly discouraged*



*Drive through lanes shall be screened with dense landscaping*

## Hotels and Motels

### *Mandatory Standards*

1. Walkway, stairway, balcony railings, and other similar architectural details shall be consistent with basic building design.
2. Hotels adjacent to, or across from, the Coastal Trail and parkland are subject to the following specific design guidelines, in addition to all the remaining relevant guidelines.
  - Properties shall provide pedestrian connections to the Coastal Trail
  - Hotels that provide a pedestrian friendly building frontage that faces the Coastal Trail;
  - All buildings shall be architecturally complete when viewed from the Coastal Trail;
  - All fences and walls should frame and protect views to coast;
  - All buildings shall be sided with natural or natural appearing materials that have been proven to perform in harsh coastal environments; and
  - All structures shall be of muted colors of natural tones.
3. Surface parking lots shall be screened with active building space, art, landscaping, etc. to provide a buffer between the public right of way and vehicles while still allowing for visibility. Delivery and loading areas should be located toward the rear of the property and screened to minimize impact on incompatible uses.

### *Preferred Standards*

1. Design of hotels and motels should draw upon the architecture of historic hotels in California for inspiration and design features, materials, and color.
2. For structures over two stories, access to guestrooms should be provided from the hallway interiors.
3. Roof forms typical of residential buildings such as gable, hip or shed roof combinations are strongly encouraged.
4. Parking, delivery and loading area, and mechanical equipment should be screened from parks and pedestrian oriented streets with buildings and landscaping.



*Hotels adjacent to the coastal trail are subject to additional design guidelines*

# Chapter 4: Industrial

## Architectural Form & Detail

### **Mandatory Standards**

1. Large unadorned and un-fenestrated wall expanses are permissible for industrial buildings. However, large expanses shall be broken up with expansion joints, reveals, and/or changes in texture, color or materials.
2. Large expanses of highly reflective surface and mirror glass exterior walls shall be avoided to prevent glare impacts on adjacent public streets and properties.
3. The mass and scale of large, box-like industrial buildings may be reduced through the incorporation of varying building heights and setbacks along the front and street side building façades.
4. Street side facades of large industrial buildings, that are visible from a public street, shall include architectural features such as reveals, windows, openings, and changes in color, texture, and material to add interest to the building elevation and reduce visual mass.
5. The following table identifies materials that are encouraged, acceptable, and discouraged for use on building façade:

#### Encouraged

- Horizontal and vertical redwood or solid wood siding
- Shingle siding
- Stucco
- Fiber cement siding
- Other like materials

#### Acceptable

- Real brick and rock
- Board and batten
- Formed concrete

- Steel or other metals
- Glass block
- Concrete block
- Ceramic tile
- Other like materials

#### Discouraged

- T1-11 or other low quality wood siding
- Corrugated fiberglass
- Slump rock
- Highly tinted, reflective, or opaque glass
- Other like materials

### **Preferred Standards**

1. The mass and scale of large, box-like industrial buildings may be reduced through the incorporation of varying building heights and setbacks along the front and street side building façades.
2. Light industrial buildings in the Mill Site Light Industrial zoning district should have an industrial or contemporary architectural character that is consistent with the historic fabric of the Mill Site or the development patterns of the nearby skunk train industrial buildings.
3. Street side facades of large industrial buildings, that are visible from a public street, should include architectural features such as reveals, windows, openings, and changes in color, texture, and material to add interest to the building elevation and reduce visual mass.
4. Primary building entries should be readily identifiable and well defined through the use of projections, recesses, columns, roof structures, or other design elements.

## Accessory Buildings

### ***Mandatory Standards***

1. Temporary buildings (e.g., portable modular units and shipping containers) shall not be visible from public streets or parks.
2. Modular buildings shall be skirted with material and color that is compatible with the modular unit and the main buildings on the site.

### ***Preferred Standards***

1. The design of accessory buildings (e.g., security kiosks, maintenance buildings, and outdoor equipment enclosures) should be incorporated into and be compatible with the overall design of the project and the main buildings on the site.



*Temporary buildings shall not be visible from public streets*

## Landscape Design

### ***Mandatory Standards***

1. When industrial/warehouse uses are located adjacent to less intense uses (e.g., residential or retail commercial), additional landscaping in conjunction with appropriate decorative walls and setbacks should be provided to mitigate potential adverse impacts.

### ***Preferred Standards***

1. Landscape design should follow an overall concept and should link various site components together.
2. The use of trees and shrubs, near, and vines, on, walls to soften the appearance of buildings and walls and to deter graffiti is strongly encouraged.



*Use of landscaping is strongly encouraged*

## Walls & Fences

### ***Mandatory Standards***

1. The colors, materials, and appearance of walls and fences, including walls for screening purposes shall be compatible with the overall design character/style of the development.
2. When security fencing is required adjacent to streets, it shall consist of wrought iron, tubular steel, wood fencing or similar materials.

### ***Preferred Standards***

1. Walls and fences can be used to visually soften blank surfaces and to deter graffiti.



*Screening fencing shall consist of wrought iron, tubular steel, etc.*

## Outdoor Lighting

### ***Mandatory Standards***

1. Outdoor lighting (e.g., location, height, and number) shall be designed to foster security. Site and building entries should have enhanced illumination to increase visibility and safety.

## Storage & Utility Equipment

### ***Mandatory Standards***

1. Outdoor storage areas (for raw and finished goods) should be screened from views from the public right of way, where feasible.
2. If refuse storage areas, fuel tanks, generators, and fire check safety valves cannot be located out of public view, the design of these areas should incorporate architectural screening elements and landscaping compatible with the design of buildings and landscaping on the site.



*Outdoor lighting shall be designed to foster security*

## Chapter 5: Signs

### Overall Sign Guidelines

#### *Mandatory Standards*

1. Signs shall be designed to relate to the architectural features of the building on which they are located and create visual continuity with other storefronts on the same or adjacent buildings.
2. Signs shall coordinate with the building design, materials, color, size, and placement.

#### *Preferred Standards*

1. Signs that reflect the type of business through design, shape, or graphic form are encouraged.
2. Signs should coordinate with the building design, materials, color, size, and placement.



*Signage shall coordinate with the building design*

### Sign Legibility

#### *Preferred Standards*

1. Use a brief message. The fewer the words, the more effective the sign's message. A sign with a brief, succinct message is simpler and faster to read, looks cleaner, and is generally more attractive.
2. Encourage unique signs, but avoid typefaces that are too faddish or bizarre.
3. Use significant contrast. Generally, light colored letters and a darker, contrasting background presents the most visible and best-looking image.
4. Use symbols and logos. Pictographic images will usually register more quickly in the viewer's mind than a written message.
5. Signs, which advertise the occupant business through the use of graphic or crafted symbols, such as shoes, keys, glasses, or books, are encouraged. Figurative signs may be incorporated into any of the allowable sign types identified above.
6. Use easy to read lettering styles. Typefaces that are difficult to read reduce the sign's ability to communicate. Avoid spacing letters and words too close together.
7. Lettering shall not occupy more than 75 percent of the sign face.

## Sign Placement

### *Mandatory Standards*

1. Signs shall not project above the edge of the rooflines and should not obstruct windows and/or doorways.
2. The location and extent of signs and advertising shall not obstruct scenic views.

### *Preferred Standards*

1. Hanging signs attached to buildings that project perpendicular to the building are encouraged in pedestrian areas.
2. Signs should be placed at or near the public entrance to a building or main parking area to indicate the most direct access to the business.
3. Signs should be sized and placed consistent with the proportions of the building's façade. For example, a particular sign may fit well on an upper, more basic wall, but would overpower and obstruct the finer detail of a lower storefront area. A sign appropriate near the building's entry may look tiny and out of place above the ground level.
4. Signs should not cover or interrupt the architectural details or ornamentation of a building's façade.

## Sign Color

### *Mandatory Standards*

1. Bright Day-Glo (fluorescent) colors are prohibited as they are distracting.

### *Preferred Standards*

1. Three or fewer colors are encouraged on a single sign.
2. Contrast is an important influence on the legibility of signs. The most aesthetic and effective graphics are produced when light colored letters and images are placed on a dark contrasting colored background.



*Most aesthetic graphics are produced with light lettering and contrasting backgrounds*

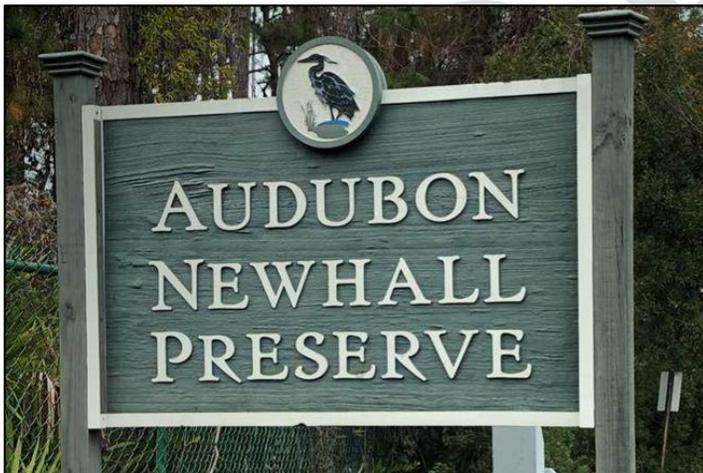
## Sign Materials

### *Mandatory Standards*

1. Sign materials shall be very durable. The following sign materials are encouraged for downtown Fort Bragg:
  - a. Wood (carved, sandblasted, etched, properly sealed and painted, or stained); Wood should be properly sealed to minimize moisture damage.
  - b. Metal (formed, etched, cast, engraved, and properly primed and painted or factory coated to protect against erosion)
  - c. Subtle custom neon tubing incorporated into sign or reminiscent of historic signs
2. Should a sign cabinet be made out of a plastic material, it shall emulate preferred sign materials (i.e. wood or metal).

### *Preferred Standards*

1. Sign materials should be selected with consideration for the architectural design of the building's façade. Sign materials should complement the materials on the façade and should contribute to the legibility of the sign.



*Example of a plastic sign emulating wood*

## Wall Signs

### *Mandatory Standards*

1. Wall signs shall not project from the surface upon which they are attached more than that required for construction purposes and in no case more than 6 inches.

### *Preferred Standards*

1. A wall sign should be located where the architectural features or details of the building suggest a location, size, or shape for the sign. The best location for a wall sign is generally a blank area between the first and second floors of a building.
2. Wall signs and "ghost" signs painted directly on a structure may be appropriate in some cases. On historic structures ghost signs often lend an air of age and authenticity.



*Example of an acceptable ghost sign*

## Sign Illumination

### *Mandatory Standards*

1. Lighting of all exterior signs shall be directional to illuminate the sign without producing glare on pedestrians, autos, or adjacent residential units.
2. Internally-illuminated cabinet-type signs are discouraged. If internally illuminated cabinet signs are used, their sign panels should be opaque so that when illuminated only the lettering, not the background, is illuminated. The background or field should have a non-gloss, non-reflective finish. White and light backgrounds are prohibited for internally illuminated cabinet signs.
3. Blinking, rotating, flashing, changing, or reflecting lights are prohibited.
4. Electrical transformer boxes and raceways shall be concealed from public view. If a raceway cannot be mounted internally behind the finished exterior wall, the exposed metal surfaces of the raceway shall be finished to match the background wall, or integrated into the overall design of the sign. If raceways are necessary, they shall be as thin and narrow as possible and shall never extend in width or height beyond the area of the sign's lettering or graphics. All exposed conduit and junction boxes shall be appropriately concealed from public view.

### *Preferred Standards*

1. Indirect sign illumination is preferred.
2. Whenever indirect lighting fixtures are used, care shall be taken to properly shield the light source.
3. The use of backlit, individually cut letter signs is strongly encouraged for all types of business and signs, including monument-type signs.
4. Neon lighting is discouraged for the lettering of the sign except neon lettering that has a historic quality; innovative use of neon for images or logos may be appropriate.
5. Exterior sign light should be dimmed or turned off after 10PM when adjacent to residential sites.
6. Sign lighting should maintain a Kelvin color temperature below 4,500 Kelvin.



*Internally illuminated cabinet signs are discouraged*

## Projecting Signs

### *Mandatory Standards*

1. Projecting signs shall be used for ground floor uses only. On a multi-storied building, the sign should be suspended between the bottom of the second story windowsills and the top of the doors or windows of the first story.
2. Sign supports and brackets shall be compatible with the design and scale of the sign and the architectural design of the building. Decorative iron and wood brackets are encouraged.
3. Internal illumination of projecting signs is prohibited.

### *Preferred Standards*

1. Small, pedestrian-oriented signs are strongly encouraged. Projecting signs are especially appropriate in downtown Fort Bragg and Neighborhood Commercial developments.
2. Projecting signs should be hung at a 90-degree angle from the building face. The distance between projecting signs should be at least 25 feet.
3. Where overhangs or covered walkways exist, pedestrian-oriented hanging signs are encouraged.



*Projecting signs hung at 90 degrees and separated by 25'*

## Awning Signs

### *Mandatory Standards*

1. Backlit, internally illuminated awnings are prohibited.
2. Only permanent signs that are an integral part of the canopy or awning shall be used. Awning signs shall be painted directly on the awning.

### *Preferred Standards*

1. Awning signs are appropriate for ground and second floor uses.
2. Sign text should only be located on the valance portion of the awning. Letter color should be compatible with awning and building colors.
3. Materials intended for a coastal climate are encouraged.



*Prohibited awning sign*

## Window Signs

### *Mandatory Standards*

1. Window signs (permanent or temporary) shall not cover more than 20-percent of the area of each window.
2. The text or sign copy of a window sign shall be limited to the business name, and brief messages identifying the product or service (e.g. "maternity wear" or "attorney"), or pertinent information (e.g. "reservations required").

### *Preferred Standards*

1. Window signs should be individual letters placed on the interior surface of the window and intended to be viewed from outside. Glass-mounted graphic logos are also appropriate.



*Window signs shall be limited to the business name and brief messages identifying the product or service*

## Monument Signs

### *Mandatory Standards*

1. Monument signs may be internally illuminated, however, the sign copy shall be the only portion of the sign face that is illuminated. The sign background or field shall be opaque with a non-gloss, non-reflective finish. Signs with individual back-lit letters, or stenciled panels with three-dimensional push-through graphics are encouraged.
2. Monument signs shall be placed perpendicular to the street.
3. Monument signs shall be placed so that sight lines at entry driveways, pedestrian throughways, and circulation aisles are not blocked.
4. Monument signs shall be designed to create visual interest and compliment their surroundings. Monument signs shall incorporate architectural elements, details, and articulation as follows:
  - a. Provide architectural elements on the sides and top to frame the sign pane(s). Use columns, pilaster, cornices, and similar details to provide design interest.
  - b. Incorporate materials and colors into the sign support structures to match or be compatible with materials and colors of the development the sign serves so it does not appear out of scale with its adjacent building(s).
5. Monument signs shall incorporate landscaping at their base. Landscaping around monument signs should be designed to ensure the long-term readability of the sign.

### *Preferred Standards*

1. Freestanding monument signs (on ground) are strongly encouraged over pole signs.

## Pole Signs

### *Mandatory Standards*

1. Single Pole-mounted signs are discouraged.
2. Pole signs shall incorporate architectural elements into the sign portion of the sign as well as the supporting structure.
3. Pole signs may be internally illuminated, however, the sign copy shall be the only portion of the sign face that is illuminated.
4. Pole signs shall incorporate a landscaped area at the base of the sign equal to one to two times the size of the sign face.
5. Pole signs are prohibited in the Central Business District.



*Pole signs shall incorporate a landscaped area at the base*



*Pole signs shall incorporate architectural elements into the sign portion of the sign as well as the supporting structure.*

## RESOLUTION NO. PC 07-2022

### RESOLUTION OF THE FORT BRAGG PLANNING COMMISSION RECOMMENDING APPROVAL OF THE UPDATED CITYWIDE DESIGN GUIDELINES FOR COMMERCIAL, INDUSTRIAL, AND MULTI-FAMILY BUILDINGS

**WHEREAS**, the City of Fort Bragg adopted the City of Fort Bragg Design Guidelines in July of 2004;

**WHEREAS**, Program CD-1.1.1 of the Inland General Plan requires the Citywide Design Guidelines to be periodically updated; and

**WHEREAS**, there have been numerous attempts to update the Design Guidelines in 2011, 2013, and 2019; and

**WHEREAS**, the proposed 2022 Design Guidelines update is a culmination of the 2011, 2013, and 2019 work; and

**WHEREAS**, The Design Guidelines are categorically exempt from the requirements of the California Environmental Quality Act (CEQA) Class 1 Section 15301 Existing Facilities because the Design Guidelines will only apply to specific project sites under new construction or remodeling of existing structures. At the time approval for a new building or remodeling of an existing building is considered, an environmental determination will be made on a case-by-case basis; and

**WHEREAS**, the Planning Commission of the City of Fort Bragg at its meeting on March 16, 2022 held a workshop at which time interested persons had an opportunity to review the proposed changes; and

**WHEREAS**, at its meeting of March 30, 2022 the Planning Commission made a recommendation to the City Council that it approve the updated Design Guidelines; and

**NOW, THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Fort Bragg does hereby make the following findings with respect to the 2022 Citywide Design Guidelines Update:**

1. **Section 1.** That the above recitations are true and correct
2. **Section 2.** Findings regarding the 2021 Citywide Design Guidelines Update. The Planning Commission hereby finds the Design Guidelines update complies with all of the following related goals, policies and programs in the General Plan:
  1. *Program CD-1.1.1: Periodically update the Citywide Design Guidelines.*
  2. *Program CD-1.1.2 Commercial and Multifamily Development: Continue to complete Design Review of commercial and multi-family development to ensure that they fit harmoniously with the scale and design of existing buildings and streetscape of the City.*
  3. *Policy S-3.4 Drought Tolerant Landscaping: New development shall include drought tolerant landscaping for landscaped areas in commercial and multi-family residential uses.*

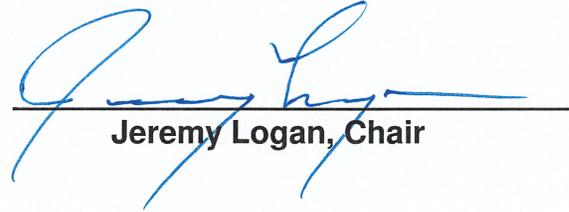
4. *Goal CD-2 Preserve the central business district as the commercial, civic, historic, and cultural center of the community.*
5. *Goal LU-3 Ensure that the Central Business District remains the historic, civic, cultural, and commercial core of the community.*
6. *Policy CD-2.2 Pedestrian Activity: Encourage increased pedestrian movement and activity in the Central Business District.*
7. *Policy CD-2.5 Strengthen the Distinctive Identity of the Central Business District: Strengthen the distinctive identity and unique sense of place of the Central Business District.*
8. *Policy LU-3.1 Central Business District: Retain and enhance the small-scale, pedestrian-friendly, and historic character of the Central Business District (CBD).*
9. *Policy LU-3.2 Mixed Uses: Support mixed use development (i.e., a combination of residential and commercial uses) in the Central Business District that does not conflict with the primary retail function of this area.*
10. *Policy LU-3.3 Historic Buildings and Mixed Uses: In the Central Business District and in other commercial areas with historic residential structures, encourage residential uses, mixed residential, and commercial uses, and the preservation of historic structures.*
11. *Policy LU-3.4 Encourage Infill Development: Encourage infill development of vacant and underdeveloped land in the Central Business District and adjacent commercial areas before amending the Inland General Plan and rezoning to obtain additional commercial land elsewhere.*
12. *Goal LU-5 Support industrial development which is consistent with the protection, enhancement, and restoration of natural and scenic resources.*
13. *Policy LU-5.1 Siting New Industrial Development: Site new industrial development so that it is contiguous with, or in close proximity to, existing developed areas able to accommodate it, or where such areas are not able to accommodate it, in other areas with adequate public services and where it will not have significant adverse effects on natural and scenic resources, either individually or cumulatively.*
14. *Policy LU-5.2 Industrial Land Use Standards: Require that industrial development avoid or minimize creating substantial pollution, noise, glare, dust, odor, or other significant adverse impacts.*
15. *Program S-3.4.1: Require landscaping for all new commercial and industrial development to use drought tolerant plants and no vegetative turf unless recreation needs or other area functions specifically requires turf.*
16. *Policy CD-3.1 Entryways: Clearly define the points of entry to the City through the use of distinctive signs, lighting, and landscaping.*
17. *Program CD-3.1.1: Maintain distinctive signs placed in a landscaped area at the south entryway at Highway 20/Highway One and at the north entryway on Highway One at the City Limits.*

**NOW, THEREFORE, BE IT FURTHER RESOLVED** that the Fort Bragg Planning Commission recommends approval of the Citywide Design Guidelines to the City Council of the City of Fort Bragg.

**The above and foregoing Resolution was introduced by Commissioner Andreis, seconded by Commissioner Rogers, and passed and adopted at a regular meeting of the**

Planning Commission of the City of Fort Bragg held on the March 30, 2022 by the following vote:

**AYES:** Planning Commissioners Andreis, Miklose, Roberts, Rogers and Chair Logan.  
**NOES:** None.  
**ABSENT:** None.  
**ABSTAIN:** None.  
**RECUSED:** None.



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Jeremy Logan, Chair

**ATTEST:**



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Sarah Peters, Administrative Assistant

**RESOLUTION NO. \_\_\_\_-2022**

**RESOLUTION OF THE FORT BRAGG CITY COUNCIL TO ADOPT THE  
UPDATED CITYWIDE DESIGN GUIDELINES FOR COMMERCIAL,  
INDUSTRIAL, AND MULTI-FAMILY BUILDINGS**

**WHEREAS**, the City of Fort Bragg adopted the City of Fort Bragg Design Guidelines in July of 2004;

**WHEREAS**, Program CD-1.1.1 of the Inland General Plan requires the Citywide Design Guidelines to be periodically updated; and

**WHEREAS**, there have been numerous attempts to update the Design Guidelines in 2011, 2013, and 2019; and

**WHEREAS**, the proposed 2021 Design Guidelines update is a culmination of the 2011, 2013, and 2019 work; and

**WHEREAS**, the Planning Commission of the City of Fort Bragg at its meeting of March 16, 2022 held a workshop at which time interested persons had an opportunity to review the proposed changes; and

**WHEREAS**, at its meeting of March 30, 2022 the Planning Commission made a recommendation to the City Council that it approve the updated Design Guidelines; and

**WHEREAS**, the City Council has reviewed and considered the draft Citywide Design Guidelines Update; and

**WHEREAS**, the Design Guidelines are categorically exempt from the requirements of the California Environmental Quality Act (CEQA) Class 1 Section 15301 Existing Facilities because the Design Guidelines will only apply to specific project sites under new construction or remodeling of existing structures. At the time approval for a new building or remodeling of an existing building is considered, an environmental determination will be made on a case-by-case basis.

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Fort Bragg does hereby make the following findings with respect to the 2021 Citywide Design Guidelines Update:

1. **Section 1.** That the above recitations are true and correct
  
2. **Section 2.** Findings regarding the 2021 Citywide Design Guidelines Update. The City Council hereby finds the Design Guidelines update complies with all of the following related goals, policies and programs in the General Plan:
  1. *Program CD-1.1.1: Periodically update the Citywide Design Guidelines.*
  2. *Program CD-1.1.2 Commercial and Multifamily Development: Continue to complete Design Review of commercial and multi-family development to ensure*

- that they fit harmoniously with the scale and design of existing buildings and streetscape of the City.*
3. *Policy S-3.4 Drought Tolerant Landscaping: New development shall include drought tolerant landscaping for landscaped areas in commercial and multi-family residential uses.*
  4. *Goal CD-2 Preserve the central business district as the commercial, civic, historic, and cultural center of the community.*
  5. *Goal LU-3 Ensure that the Central Business District remains the historic, civic, cultural, and commercial core of the community.*
  6. *Policy CD-2.2 Pedestrian Activity: Encourage increased pedestrian movement and activity in the Central Business District.*
  7. *Policy CD-2.5 Strengthen the Distinctive Identity of the Central Business District: Strengthen the distinctive identity and unique sense of place of the Central Business District.*
  8. *Policy LU-3.1 Central Business District: Retain and enhance the small-scale, pedestrian-friendly, and historic character of the Central Business District (CBD).*
  9. *Policy LU-3.2 Mixed Uses: Support mixed use development (i.e., a combination of residential and commercial uses) in the Central Business District that does not conflict with the primary retail function of this area.*
  10. *Policy LU-3.3 Historic Buildings and Mixed Uses: In the Central Business District and in other commercial areas with historic residential structures, encourage residential uses, mixed residential, and commercial uses, and the preservation of historic structures.*
  11. *Policy LU-3.4 Encourage Infill Development: Encourage infill development of vacant and underdeveloped land in the Central Business District and adjacent commercial areas before amending the Inland General Plan and rezoning to obtain additional commercial land elsewhere.*
  12. *Goal LU-5 Support industrial development which is consistent with the protection, enhancement, and restoration of natural and scenic resources.*
  13. *Policy LU-5.1 Siting New Industrial Development: Site new industrial development so that it is contiguous with, or in close proximity to, existing developed areas able to accommodate it, or where such areas are not able to accommodate it, in other areas with adequate public services and where it will not have significant adverse effects on natural and scenic resources, either individually or cumulatively.*
  14. *Policy LU-5.2 Industrial Land Use Standards: Require that industrial development avoid or minimize creating substantial pollution, noise, glare, dust, odor, or other significant adverse impacts.*
  15. *Program S-3.4.1: Require landscaping for all new commercial and industrial development to use drought tolerant plants and no vegetative turf unless recreation needs or other area functions specifically requires turf.*
  16. *Policy CD-3.1 Entryways: Clearly define the points of entry to the City through the use of distinctive signs, lighting, and landscaping.*
  17. *Program CD-3.1.1: Maintain distinctive signs placed in a landscaped area at the south entryway at Highway 20/Highway One and at the north entryway on Highway One at the City Limits.*

**NOW, THEREFORE, BE IT FURTHER RESOLVED**, that the City Council of the City of Fort Bragg does hereby adopt the Findings stated hereinabove and approve of the 2022 Citywide Design Guidelines update found on Attachment 1 in its entirety.

The above and foregoing Resolution was introduced by Councilmember \_\_\_\_\_, seconded by Councilmember \_\_\_\_\_, and passed and adopted at a regular meeting of the City Council of the City of Fort Bragg held on the 25<sup>th</sup> day of April, 2022, by the following vote:

- AYES:**
- NOES:**
- ABSENT:**
- ABSTAIN:**
- RECUSED:**

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**BERNIE NORVELL**  
Mayor

**ATTEST:**

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**June Lemos, MMC**  
City Clerk



# City of Fort Bragg

416 N Franklin Street  
Fort Bragg, CA 95437  
Phone: (707) 961-2823  
Fax: (707) 961-2802

## Text File

File Number: 22-180

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**Agenda Date:** 4/25/2022

**Version:** 1

**Status:** Business

**In Control:** City Council

**File Type:** Ordinance

**Agenda Number:** 8D.

Receive Report and Consider Adoption of Urgency Ordinance No. 981-2022 Placing a 45-Day Moratorium on the Approval of Applications and Permits for Cannabis Dispensaries in the Inland Zoning Area



AGENCY:	City Council
MEETING DATE:	April 25, 2022
DEPARTMENT:	City Manager
PRESENTED BY:	David Spaur
EMAIL ADDRESS:	<a href="mailto:dspaur@fortbragg.com">dspaur@fortbragg.com</a>

## AGENDA ITEM SUMMARY

**TITLE:**

**Receive Report and Consider Adoption of Urgency Ordinance No. 981-2022 Placing a 45-Day Moratorium on the Approval of Applications and Permits for Cannabis Dispensaries in the Inland Zoning Area**

**ISSUE:**

On [September 27, 2021](#), City Council adopted Urgency Ordinance 972-2021 imposing a 45-day moratorium on the approval of applications and permits for cannabis dispensaries in the Inland Zoning Area.

On [November 8, 2021](#), that moratorium was extended for four months through March 8, 2022 by adoption of Urgency Ordinance 975-2021.

On [February 28, 2022](#), the moratorium was again extended for a period of 45 days through April 14, 2022 by adoption of Urgency Ordinance 976-2022.

No more than two extensions of an interim urgency ordinance may be adopted according to [California Government Code Section 65858](#). A new urgency ordinance is required to continue the moratorium on cannabis permits until regulations currently under consideration concerning cannabis dispensaries are finalized, adopted by the City Council, and become effective, at which time the Council may rescind the urgency ordinance prior to its stated termination.

**ANALYSIS:**

Previous Meetings on Cannabis Regulations

The moratorium was initially put in place to allow the Planning Commission and the City Council time to seek public input, discuss various issues surrounding cannabis dispensaries, and provide direction regarding new regulations for cannabis cultivation and retail cannabis sales. The last several meeting agendas and minutes can be reviewed through the following links:

- [Planning Commission Agenda 2/23/2022](#)
- [Planning Commission Agenda 3/9/2022](#)
- [Planning Commission Agenda 3/23/2022](#)
- [City Council Agenda 3/28/2022](#)

- [Planning Commission Minutes 2/23/2022](#)
- [Planning Commission Minutes 3/9/2022](#)
- [Planning Commission Minutes 3/23/2022](#)
- [City Council Minutes 3/28/2022](#)

On [April 11, 2022](#), the public hearing to consider introduction of Ordinance 979-2022 relating to cannabis regulations was opened and immediately continued to a date certain, May 23, 2022, because more time was required to process the associated environmental documents.

### Urgency Ordinances

Under [California Government Code Section 65858](#), a city or county may adopt an interim ordinance to temporarily prohibit certain land uses that may be in conflict with a contemplated general plan, specific plan or zoning proposal that the City Council and/or Planning Commission is considering or studying. The temporary prohibition or moratorium provides municipalities time to study the impact of certain activities and develop appropriate regulations, if deemed necessary.

An urgency ordinance can be passed without advance notice to the public and is effective immediately for up to 45 days but must be approved by a four-fifths vote of the legislative body. The urgency ordinance may be extended for a total period of two years as set forth in the IMPLEMENTATION/TIMEFRAMES section of this report. Any extension of the moratorium must also be approved by a four-fifths vote of City Council.

### Proposed Ordinance

Similar to the previous cannabis moratorium ordinance, the proposed ordinance would place a hold on the approval of any cannabis dispensary in the Central Business District. This would allow staff and the City Council to finalize updates to the existing Cannabis Business Regulations as directed by the City Council at its meeting of March 28, 2022.

### **POSSIBLE ACTION(S):**

1. City Council upon review may waive the reading of the Ordinance and adopt by title only an Urgency Ordinance of the City of Fort Bragg placing a temporary moratorium on the approval of applications and permits for cannabis dispensaries.
2. Council can decide not to adopt the Urgency Ordinance.
3. Council may provide alternative direction to staff.

### **FISCAL IMPACT:**

The impact of the 45-day temporary moratorium on cannabis dispensaries to the City's revenue is likely minimal.

### **GREENHOUSE GAS EMISSIONS IMPACT:**

N/A

### **CONSISTENCY:**

The City's current Inland General Plan provides support for regulating formula businesses. The current purpose statement from Element 6 of the Inland General Plan:

#### A. Purpose

The Community Design Element establishes goals, policies, and programs to preserve and enhance Fort Bragg's authentic, small town character. The community is defined in part by its isolated location on the magnificent coastline of Mendocino County. Its sense of place derives from its heritage as a regional center for the timber and fishing industries. As the economy evolves to a more tourism and service-based economy, the community has acknowledged the importance of maintaining the historic identity of

downtown and the integrity of the residential neighborhoods, while enhancing views and access to the coastline and planning for continued growth and development.

The Community Design Element is concerned primarily with the visual quality of the City, or what residents and visitors see. The City's appearance is essential to the quality of life in Fort Bragg. Visual quality and amenities go hand-in-hand with long-term economic development strategies, and strengthen the stability and desirability of the community. To be attractive to residents, visitors, and businesses, the City must be concerned about its appearance, physical character, and livability. Existing residential real estate values and the desirability of businesses that depend on tourism are closely tied to the visual character of the community.

Policies and Goals from the City's General Plan include:

***Policy LU-3.1 Central Business District: Retain and enhance the small-scale,***

***Goal C-5 Regard the quality of life in Fort Bragg and maintaining community identity as more important than accommodating through-traffic.***

***Policy C-5.1 Community Priorities for Transportation Improvements: Place a higher priority on maintaining a sense of place and enhancing the attractiveness of the Central Business District than on efficient traffic flow and movement.***

***Policy CD-2.5 Strengthen the Distinctive Identity of the Central Business District: Strengthen the distinctive identity and unique sense of place of the Central Business District.***

***Policy CD-6.1 Protect and Preserve Buildings and Sites with Historic and Cultural Significance to the Community.***

#### **IMPLEMENTATION/TIMEFRAMES:**

An urgency ordinance may remain in effect for only 45 days, unless it is extended by another four-fifths vote. Ten days prior to the expiration of the urgency ordinance or any extension, City Council will need to issue a written report describing the measure taken to alleviate the condition that led to the adoption. After notice and a hearing, a local government can extend the ordinance for either ten months and 15 days, with the option to extend it once more for an additional one-year, or once for 22 months and 15 days. Either option equates to an extension of up to two years and requires the four-fifths vote. The legislative body may rescind the urgency ordinance prior to its stated termination.

Public Hearing on Urgency Ordinance 981-2022: April 25, 2022

Effective Date of Urgency Ordinance 981-2022 if adopted: April 25, 2022

Expiration Date of Urgency Ordinance 981-2022 unless extended: June 9, 2022

Continued Public Hearing on Introduction of Ordinance 979-2022: May 23, 2022

Adoption Date of Ordinance 979-2022 if introduced on May 23, 2022: June 13, 2022

Effective Date of Ordinance 979-2022 if adopted: July 13, 2022

As shown by the above schedule, this proposed Urgency Ordinance 981-2022 will need to be extended after 45 days to allow Ordinance 979-2022 to be adopted and become effective. Anticipated date to extend the moratorium before its June 9<sup>th</sup> expiration date is May 23, 2022. At that time, a 60-day extension will allow sufficient time for the new cannabis ordinance, 979-2022, to become effective.

**ATTACHMENTS:**

1. Proposed Urgency Ordinance 981-2022

**NOTIFICATION:**

1. Cannabis Notify Me subscriber list

**BEFORE THE CITY COUNCIL OF THE CITY OF FORT BRAGG**

**AN UNCODIFIED URGENCY  
ORDINANCE ESTABLISHING A 45-DAY  
MORATORIUM ON APPROVAL OF  
APPLICATIONS AND PERMITS FOR  
CANNABIS DISPENSARIES IN THE  
INLAND ZONING AREA**

**URGENCY ORDINANCE  
ORDINANCE NO. 981-2022**

**WHEREAS**, Government Code section 65858 allows a city to adopt, as an urgency measure, an interim ordinance for the immediate preservation of the public safety, health and welfare without following the procedures otherwise required prior to adoption of a zoning ordinance; and

**WHEREAS**, the City of Fort Bragg (“City”) previously adopted ordinances governing cannabis businesses, including dispensaries and manufacturing; and

**WHEREAS**, on November 8, 2016, the electorate of the State of California approved Proposition 64 (“Prop 64”) which enacted the Adult Use of Marijuana Act (“AUMA”), to be codified in California Health and Safety Code at various sections and in California Business and Professions Code at various sections. The AUMA allows adults 21 and over to use, possess, and cultivate limited amounts of marijuana, establishes a state licensing and regulatory scheme for marijuana businesses serving the recreational market; and expressly allows local jurisdictions to prohibit outdoor cultivation of marijuana for personal use, to regulate indoor cultivation of marijuana for personal use, and to prohibit all non-medical and recreational marijuana businesses from locating and operating within their jurisdictions; and

**WHEREAS**, on June 27, 2017, the Governor approved Senate Bill 94 which combined the regulatory schemes for Medical Marijuana Regulation and Safety Act (“MMRSA”) and AUMA into a single, comprehensive regulatory scheme known as the Medicinal and Adult-Use Cannabis Regulation and Safety Act (“MAUCRSA”); and

**WHEREAS**, on February 25, 2019, the City Council of the City of Fort Bragg (“City Council”) sought to establish rules and regulations by which cannabis businesses may be permitted by considering an ordinance regulating retail, laboratory testing, manufacturing, and distribution cannabis businesses and accessory uses; and

**WHEREAS**, through multiple public meetings, the Public Safety Committee, Fort Bragg Police Department, and City staff have received input from citizens and discussed various options for permitting cannabis businesses; and

**WHEREAS**, on November 21, 2019, the City Council of the City of Fort Bragg (“City Council”) adopted Ordinance 953-2019 which established rules and regulations by which cannabis dispensary businesses may be permitted; and

**WHEREAS**, on January 25, 2021, the City Council conducted a public hearing and considered the appeal of the Fort Bragg Planning Commission denial of Minor Use Permit 4-20 (MUP 4-20) on the proposed Sunshine Holistic Microbusiness at 144 N. Franklin Street. City Council failed to take action by a majority vote, which resulted in upholding the Planning Commission’s determination that growing, manufacturing, processing of cannabis is not a permitted use in the Central Business District and that uses (growing, manufacturing, processing of cannabis) are not accessory/subordinate to the primary use of retail; and

**WHEREAS**, on September 1, 2021, the City Council conducted a public hearing and considered the appeal of the Fort Bragg Planning Commission denial on Minor Use Permit 1-21 (MUP 1-21) on the proposed Sunshine Holistic Cannabis Dispensary at 144 N. Franklin Street. City Council failed to take action by a majority vote, which resulted in upholding the Planning Commission’s determination that the proposed use was not compatible with the existing and future land uses of that neighborhood and that there was not sufficient evidence that the required noticing of the Minor Use Permit, the Minor Use Permit Administrative Hearing, and the Minor Use Permit Appeal were properly posted at the property; and

**WHEREAS**, during the appeals of both the Sunshine Holistic Minor Use Permit applications, the City Council, the Planning Commission and members of the public raised concerns about the impact of residential uses located in or near the Central Business District (CBD) where cannabis dispensaries are allowed with an approved Minor Use Permit; concerns over the number of dispensaries allowed in the CBD and whether a maximum number should be set to avoid overconcentration of cannabis dispensaries; and the proximity of cannabis dispensaries to facilities that could be defined as a “youth center” including the City Hall gymnasium located at 213 East Laurel Street; and

**WHEREAS**, Chapter 9.30 Cannabis Businesses of the Municipal Code does not define “youth center” nor does the Fort Bragg Inland Land and Use Development Code define “youth center” or address it; therefore, California State law requires a 600-foot separation between youth centers and cannabis storefronts unless the local jurisdiction specifies a different distance; and

**WHEREAS**, the City has not established a separation requirement between cannabis businesses and youth centers and has received a completed application for a cannabis dispensary located at 362 N. Franklin Street which is directly across the street from the City Hall gymnasium located at 213 East Laurel Street and within the 600-foot buffer required by California State law; and

**WHEREAS**, City staff have determined that seventy-nine percent (79%) of the total hours the City Hall gymnasium located at 213 East Laurel Street was rented for activities associated with youth activities but only nineteen percent (19%) of hours for City Hall including the gymnasium are associated with youth activities. A determination by the City Council that the City

hall gymnasium is a “youth center” will impact the City’s ability to permit cannabis dispensary businesses in the Central Business District and uses may be in conflict with a contemplated general plan, specific plan or zoning proposal that the City Council is considering related to cannabis businesses; and

**WHEREAS**, on September 27, 2021, the City Council adopted Urgency Ordinance 972-2021 to impose a moratorium for 45 days, in order to study whether it would be appropriate to amend how the City regulates cannabis dispensaries; and

**WHEREAS**, on November 8, 2021, the City Council adopted Urgency Ordinance 975-2021 to extend the moratorium for four months to amend Title 18 Inland Land Use and Development Code to revise land use regulations for cannabis businesses.

**WHEREAS**, on February 17, 2022, the City circulated a draft Initial Study and Negative Declaration for proposed changes to the Municipal Code Chapter 9.30 and Title 18, the Inland Land Use and Development Code, to amend the regulations for cannabis businesses.

**WHEREAS**, on February 23, 2022, March 9, 2022, and March 23, 2022, the Planning Commission held public hearings on the proposed amendments to Title 18 Inland Land Use and Development Code to amend the regulations for cannabis businesses.

**WHEREAS**, on February 28, 2022, the City Council adopted Urgency Ordinance 976-2022 to extend the moratorium on cannabis businesses for 45 days through April 14, 2022.

**WHEREAS**, on March 28, 2022, the City Council received a staff report and provided direction to staff on amendments to Title 18 Inland Land Use Development Code.

**WHEREAS**, there are two pending applications for cannabis dispensaries in the City and one application that was approved on April 18, 2022 that is eligible for appeal through April 27, 2022 and, if not appealed will be final on April 28, 2022.

**WHEREAS**, the City opened a public hearing on the proposed ordinance to amend the Inland Land Use and Development Code to regulate cannabis businesses on April 11, 2022 and the hearing was continued to May 23, 2022; and

**WHEREAS**, approval of any cannabis dispensary in the Central Business District may conflict with the proposed amendments to the city’s zoning code.

**NOW, THEREFORE, the City Council ordains as follows:**

**Section 1. Findings.** The City Council hereby finds as follows:

1. All the recitals, facts, findings, and conclusions set forth above in this Ordinance are true and correct.
2. This ordinance is not a “project” under the California Environmental Quality Act (CEQA) and is therefore exempt from CEQA review. CEQA defines a “project” as including an activity directly undertaken by a public agency that “may cause either a direct physical change in the

environment, or a reasonably foreseeable indirect physical change in the environment.” (Pub. Res. Code, §21065.) The proposed ordinance will not do either.

This ordinance concerns activities intended to preserve the status quo while the City considers further regulation relating to cannabis dispensaries in the Central Business District and does not constitute a “project” as defined by CEQA and will not result in a direct or reasonably foreseeable indirect physical change in the environment pending the review of overconcentration issues (Pub. Res. Code, §21065, CEQA Guidelines §§15060(c)(2)(3); 15061(b)(3); 15064(d)(3); and 15378(a).).

3. The establishment and expansion of cannabis dispensaries in the Inland Zoning Area has created a current and immediate threat to public health, safety, and welfare. Cannabis dispensaries require additional standards to protect public health and safety.
4. The City intends to adopt an ordinance to amend how the City regulates cannabis dispensaries in the Inland Zoning Area.
5. This ordinance is intended to protect the public health, safety, and welfare by prohibiting the establishment of any new cannabis dispensary use with a MUP or expansion or modification of those uses while the City enacts new land use regulations. The continued establishment, expansion, and modification of these uses during this process would defeat the City Council’s objectives in adopting new regulations governing these uses in the area.
6. This ordinance promotes the public health, safety, convenience, and welfare of the City.

**Section 2: Moratorium.** The City of Fort Bragg declares a moratorium on the approval of applications to establish a cannabis dispensary on any parcel, which is, in whole or in part, in the Inland Zoning Area. However, the City will continue to accept and process applications for such businesses.

**Section 3: Already-approved projects.** This ordinance does not apply to projects for which the City has approved entitlements as of the effective date of this ordinance.

**Section 4: Inconsistencies.** Any provision of the City of Fort Bragg Municipal Code thereto inconsistent with the provisions of this Ordinance, to the extent of such inconsistencies and no further is hereby repealed or modified to that extent necessary to affect the provisions of this Ordinance.

**Section 5: Severability.** If any chapter, article, section, subsection, subdivision, sentence, clause, phrase, word, or portion of this Ordinance, or the application thereof to any person, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portion of this Ordinance or its application to other persons. The City Council hereby declares that it would have adopted this Ordinance and each chapter, article, section, subsection, subdivision, sentence, clause, phrase, word, or portion thereof, irrespective of the fact that any one or more subsections, subdivisions, sentences, clauses, phrases, or portions of the application thereof to any person, be declared invalid or unconstitutional. No portion of this Ordinance shall supersede any local, state, or federal law, regulation, or codes dealing with life safety factors.

**Section 6: Uncodified Ordinance.** This Ordinance is to be uncodified.

**Section 7: Immediately Effective.** This Ordinance shall become effective immediately and shall remain in effect for a period of 45 days unless extended or prematurely terminated.

**Section 8: Written Report.** Pursuant to Government Code Section 65858(d), ten days prior to the expiration of this Ordinance or any extension of such, the City Manager shall issue a written report on behalf of the City Council describing the measures taken to date to alleviate the condition, which led to the adoption of this Ordinance.

**Section 9: Certification.** The City Clerk shall certify to the adoption of this Ordinance and shall cause the same to be posted or published in the manner as required by law.

The foregoing Urgency Ordinance was introduced by Councilmember \_\_\_\_\_ and adopted by no less than the required 4/5 vote at a regular meeting of the City Council of the City of Fort Bragg held on April 25, 2022 by the following vote:

- AYES:
- NOES:
- ABSENT:
- ABSTAIN:
- RECUSED:

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**BERNIE NORVELL**  
Mayor

ATTEST:

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June Lemos, MMC  
City Clerk

PUBLISH: May 5, 2022  
EFFECTIVE DATE: April 25, 2022

**From:** [Jacob Patterson](#)  
**To:** [Lemos, June](#)  
**Cc:** [Spaur, David](#); [sarah mccormick](#); [Smith, John](#)  
**Subject:** Public Comment -- 4/25/22 CC mtg., Item Nos. 5E, 5G, and 8D  
**Date:** Monday, April 25, 2022 1:05:39 PM

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City Council,

It might be a little odd connecting consent calendar Items 5E and 5G for the purposes of public comment but I am doing so because the City may be focusing on the wrong things in how we are allocating scarce staff time and financial resources compared to the actual pressing problems facing the community. The Covid-19 pandemic is serious but I think the more pressing "emergency" facing the community is your lack of collective judgment and prioritization of efforts that we can actually address on a local level.

Please see the below excerpt from yesterday's Mendocino County Today in the AVA, which sums up the situation nicely, IMO. As a City, we appear to be wasting a lot of time, money, and energy on relatively low-impact and low-utility efforts that cater to local special interests and fun pet projects of particular councilmembers, staff (or former staff) rather than tackling many of the real problems facing our community, some of which are discussed in the AVA excerpt below. I recognize that some of the listed issues are not clearly within the direct control of the City or City Council but we are spending a lot of time and money on relatively unimportant projects like investing in public money to develop the ocean water infrastructure for the Noyo Center's potential--emphasis on potential--future aquarium and research center and allegedly for a blue economy business park that is not even permitted by the current zoning (except aquaculture) and isn't likely to ever come to fruition despite all of the best intentions of the supporters of such concepts. Why are we investing so many local public resources with a single well-connected local non-profit but effectively ignoring so many other more pressing community needs and services that have a greater likelihood of tangible economic benefits or much-needed local services? Where is the urgency dealing with facilitating future healthcare services on the coast as we continue to face diminishing access to such services? What is happening with the Community Land Trust and how is it actually moving forward to developing more local housing opportunities? What are we doing to address the numerous housing-related programs we committed to consider when you adopted the current housing element? Where is the pilot safe parking program for mobile homeless living in vehicles and RVs that won't be covered by the tiny home ordinance--all the more pressing because County code enforcement efforts are apparently leading to eviction notices for at least some of the long-term residents of Wildwood Campground? (Where do you think those people are likely to end up along with their trailers and RVs?...)

To address the matters at hand on the agenda tonight, why are we spending so much time on cannabis-related issues when the legal cannabis industry is in such decline and we likely missed the boat on getting our regulations in order? There are rapidly diminishing returns at this point and facilitating more commercial cannabis activity in town is less likely to generate meaningful economic development and the free-for-all that is being pushed by the Council majority for the CBD is likely to harm our other economic development opportunities for our downtown core; meanwhile we are effectively ignoring the industrial areas north of town where such development has already been proposed and is the most likely area for future economic development related to commercial cannabis, particularly with the pending end of the sunset period for the Mitchell Creek and Simpson Lane County neighborhood in June 2022. What is the purpose of even adopting a new moratorium for pending applications in the

CBD when the only applications we have in the CBD are not approvable under the current code so we don't need to worry about them getting approved prior to the new regulations potentially going into effect mid-July?

On a more granular note, why was the report in 5G not prepared and presented to the City Council prior to the expiration of the prior moratorium as is required? Can we do nothing correctly according to legally-required procedures? When will you recognize that there is a problem with how the City is doing things and try to make some changes to get us on the right procedural track? When will the City actually hire qualified people to perform these functions rather than sometimes hiring apparently under-qualified people who may function as relatively expensive interns who we then pay to develop many of the basic qualifications for their jobs they likely should have already possessed in order to be hired in the first place and using public funds to do so? What is going on and why are none of you doing enough to right the ship (at least in the opinion of people like me who pay attention to details and City projects and activities overall rather than just lookign at each project and effort in a silo without taking a more broad perspective)? I wouldn't be surprised if the new city manager gets to town and doesn't even unpack before she/he/they go running for the hills because of all of the issues and dysfunction apparent in how the City is being run not the least of which appears to be an illogical organizational structure and inefficient allocation of staff positions within City Hall that appears to be structure to cater to the needs and wants of the current staff rather than what actually makes sense to serve and provide services to the community and the public-at-large. Please consider these topics as you decide how to proceed with the agenda items tonight and in general as the City moves forward with budget development and various projects and efforts.

Please note that I have no idea who wrote into the AVA under "name withheld" and it wasn't me; I first became a local resident by being born at the hospital a decade prior to the author's arrival.

Regards,

--Jacob



## FORT BRAGG'S DIMINISHING CORE SERVICES

Editor,

Over the past 30+ years...since I moved to Fort Bragg in 1986. I've loved this town and all that it represented (past tense).

However, as of late, what seems like almost overnight, Fort Bragg has become a town in crisis!

All the institutions that we came to depend on and respect are for all intents and purposes GONE! I raised my son here in great schools, acquired my AA at the local JC, received caring and supportive medical/emergency services for my rescues, was on the receiving end of dedicated and devoted medical health professionals and more.

But, these and too many other institutions that I've come to depend on are no longer available to the residents of this community.

The Junior College is a ghost town, the hospital struggles to entice doctors to the Coast. Emergency and even regular veterinary care services are all but non-existent! We are forced to seek services elsewhere, off the Coast, and for those with limited resources, they do without.

Fort Bragg City Council members, I ask you: What is it, exactly, that you are doing to address the absence of essential, core services of this community, services you were elected to provide?

What exactly have or are you currently doing/planning to address the absence of the most basic needs of this community?

What are you doing or plan to do to put an end to the ever-diminishing services that once were available to this community?

It's a long time since 1986, but I would ask you, Council members, are we, here on the Coast, here in Fort Bragg, better off now, than we were 10 years ago? 20 years ago?

That answer is abundantly clear: NO! Not even close!

And now, you talk about dissolving the Mendocino Coast Health Care District? Why? Because you've already accomplished so much in providing for the needs of this community? My guess? Pure arrogance? Another agenda? Your inbox is overflowing. Start doing what you were elected to do!

Name Withheld

Fort Bragg

